



City of Fairfield

FAIRFIELD TRAIN STATION SPECIFIC PLAN

July 2011





City of Fairfield

FAIRFIELD TRAIN STATION SPECIFIC PLAN

City Council Approval - July 26, 2011

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1.0 GUIDING PRINCIPLES/ PROJECT OBJECTIVE



1.0 GUIDING PRINCIPLES/ PROJECT OBJECTIVES

July 26, 2011



1.1 Community Vision

The Fairfield Train Station Specific Plan represents a new era of transit-oriented design in Solano County, creating an aspirational community founded on principles of traditional neighborhood planning combined with access to transportation and the workplace that is unparalleled in Solano County. It will provide diverse choices in housing, jobs, and recreation and a place for families to live and work. Its boulevards, pathways, and green spaces will weave together highly crafted neighborhoods, a vibrant Town Center and new employment districts. It incorporates 21st century concepts in sustainability and environmental responsibility including allowing residents direct access to the open spaces that surround them.

The Fairfield Train Station Specific Plan (FTSSP) is a community formed at a crossroads. The plan encompasses existing development that is bordered by two cities and an Air Force Base, and is strategically located along a rail line. It is also formed at a time when the country, state, and region are at an economic crossroads; working out of a serious market and financial correction.

The vision statement above expresses the primary goal that the FTSSP will guide the formation of a new type of live/work/play environment in Fairfield and Solano County; one with village scale and charm, but with the sustainability and lifestyle that reflect the demands and complexity of life in contemporary northern California.

The FTSSP attempts to create a physical form for the community that captures the romance and charm of the “golden years” of town building that occurred between the turn of the 19th century and the Second World War. During this era, small-town America was at its zenith, with quaint main streets, town squares, civic boulevards, central parks, and streets with front porches and overhanging shade trees. These qualities created some of the most sought after neighborhoods in the country with many maintaining their status and values over the last century.



But many of these attributes have been lost during the more recent period of rapid suburban growth. Current development often reflects a more mass-produced approach to delivering homes and communities in order to achieve the high volumes and affordability the market has demanded. In the FTSSP we seek the middle road, providing aspects of charm and nuance from the traditional small town while acknowledging the realities of contemporary economies-of-scale and post-recession market pressures.

A plan that anticipates the lifestyle shifts and daily pressures of working and/or raising a family in the first part of the 21st century needs to be highly integrated and infused with support services, convenience, connectivity, security, and choice. This means that shopping, dining, education, recreation, and open space need to be ingrained in the community fabric rather than isolated, requiring greater connectivity between neighborhoods and the things around them. These needs are manifest in the FTSSP by a creating a Town Center district of higher density housing, multiple services, and a network of boulevards, walkable blocks and small parks. To further support this desire for connectivity, greenways, trails, boulevards and other pedestrian-friendly elements tie the overall village together from north to south and east to west.

Sustainable transportation is another key element of the plan. The advent of the new train station located at the intersection of Peabody and Vanden Roads provides a unique opportunity to address both the need for more mixed-use living environments for various market segments and to reduce greenhouse gas emissions in light of global warming and climate change. The FTSSP attempts to maximize the value of the Train Station by creating strong linkages to the core of the Town Center and the Lake District with a sequence of highly walkable civic spaces and parks. This planned linkage allows ideal walking and biking access from all of the high- and mid-density neighborhoods to the Train Station and Town Center areas. The effect is a linear activity and density spine that extends the transit-oriented aspect of the district as far as possible into the plan.

It is a goal of the FTSSP to create a model for contemporary new community design in the region. This cannot be achieved without a concerted effort at increased sustainability. An important aspect of the planning process has been using principles of whole systems and integrated design to reduce green house gases by optimizing a low carbon generating urban form. The Town Center and the Village Core within the Lake District, which allow a large portion of the project population to be within walking distance of daily services, are a result of applying low carbon principles to the plan. The linkage to and expansion of the rail station transit opportunity is a key sustainable planning concept. In addition, the FTSSP includes a number of water, building energy, ecological services, public realm energy, and additional transportation efficiency measures. In an era of increasing concern about the environment, the threat of climate change and

global warming, it is the responsibility of any new development to utilize urban forms, mobility systems, and building practices that reduce energy consumption, vehicle miles travelled, and related emissions.

These strategies, and others to be described in the body of the FTSSP, lead to a number of key development concepts, including:

- A Transit Oriented Community – a model of walkable, multi-modal, mixed-use living
- A Sustainable Community – demonstrating increased resource efficiency within a suburban setting
- A Diverse Community – with a range of housing opportunities across the economic spectrum
- A Working Community – providing jobs as well as housing for land use and economic balance
- A Multi-Modal Community – providing a Town Center and secondary centers of services and activities
- A Conservation Community – conserving major high-value open space areas in perpetuity
- A Recreation Community – incorporating regionally unprecedented recreation and sports amenities
- An Economically Sustainable Community – finding the balance to achieve economic value for all parties during a time of national marketplace restructuring

The FTSSP community will be a place where pedestrians, bicyclists, train commuters, and vehicles coalesce along tree-lined streets that lead to a wide array of residences, businesses, and public and recreational spaces. Shops, restaurants, and businesses will transition to apartments and condominiums in the Town Center and then to lakefront town and single-family homes in the Lake District. Open space and multi-modal pathways will link charming and varied neighborhoods with the whole surrounded by an open space setting of conservation areas and ecological preserves. Timeless architectural styles, tree-lined streets, small neighborhood parks, and specially crafted people-gathering places reinforce the traditional neighborhood planning character and help set the FTSSP apart from other communities.

Figure 1-1 shows the Concept Plan for the FTSSP community.



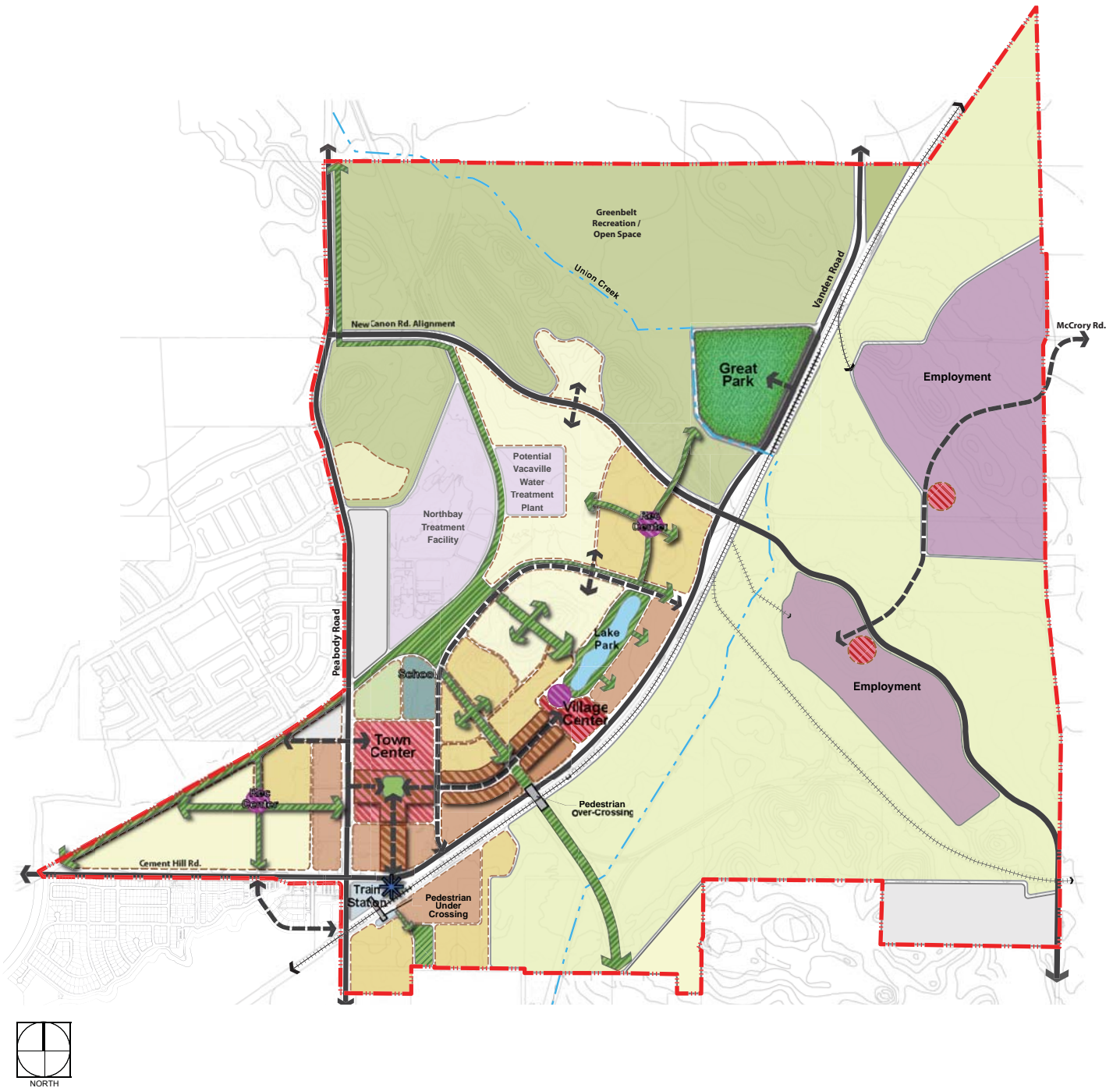


Figure 1-1: Concept Plan

1.2 Guiding Principles, Objectives, and Policies

The FTSSP community concept has evolved from a close working relationship between the City of Fairfield, the Fairfield City Council, and local land owners to identify their aspirations for the area and capture them in a workable set of Project Objectives. These Project Objectives have been adopted by the Fairfield City Council to assist in crafting the Specific Plan and preparing the FTSSP document. The Fairfield City Council has been instrumental in the initial visioning of the FTSSP Project. These objectives have formed the community vision and served as a guide against which all future development within the Specific Plan boundary will be measured.

The FTSSP has been designated as a Priority Development Area (PDA) by the Association of Bay Area Governments (ABAG). A PDA generally refers to an infill development opportunity within an existing community that is planned near existing or future fixed transit and includes a plan for more housing. The City's Guiding Principles for the Specific Plan are consistent with ABAG's planning goals.

1.2.1 Transit-Oriented Development

Project Objective: The FTSSP shall be a transit-oriented community. Its design shall create a strong sense of place that is unique and comparable to the best "greenfield" communities in the United States.

Key Concepts:

- Provide a diverse mix of housing, shopping, employment, cultural, and recreational opportunities within walking distance to reduce the number of required vehicle trips while encouraging alternative modes of travel.
- Provide multiple modes of transportation including train, bus, park and ride, bicycle, and walking to further reduce single occupancy trips both internal and external to the Project.

1.2.2 Train Station

Project Objective: The land use and circulation plans will be designed to support and enhance use of the planned Train Station. The density and intensity of development shall be greater in proximity to the station.

Key Concepts:

- Provide clear and convenient linkage between the station, parking, residential, and business uses.
- Locate key amenities and destinations in the vicinity of the Train Station to encourage the use of public transit.



- Ensure sufficient density within a half-mile radius of the station to maximize the number of people living and working within walking distance of the station.

1.2.3 Land Use

Project Objective: The land plan will take advantage of the Project area and location in providing a mix of land uses, including a broad range of housing types and densities, and public, commercial, office, and industrial uses.

Key Concepts:

- Develop a sustainable urban form by transitioning from higher densities in the Town Center, to medium densities within the Lake District to lower density neighborhoods on the community edge.
- Provide retail and other commercial services, a school, a library, parks, and community services at a sufficient level to meet the needs of the community.
- Create view corridors, landmarks, special civic streets, and town features to articulate the visual landscape and create a visually pleasant and attractive community.

1.2.4 Circulation

Project Objective: The circulation system shall include vehicular, transit, pedestrian, and bicycle components, including extension of the Linear Park Trail.

Key Concepts:

- Create a circulation infrastructure system that accommodates a range of transportation modes including pedestrian pathways and bicycle paths to reduce dependence on the automobile and single occupancy vehicle trips.
- Develop internal trail linkages between the citywide Linear Park Trail and regional trail system, including access to key adjacent destinations and the surrounding open space and natural preserves.

1.2.5 Travis Air Force Base

Project Objective: Development shall be compatible with the continued operations of Travis Air Force Base (AFB) and shall protect its viability to accommodate future expansions and changes in mission.

Key Concepts:

- Create new and improved roadway linkages to Travis AFB that allow convenient access and support its operation.
- Propose land uses within proximity of the base that are consistent with noise and overflight criteria.



- Provide housing choices that are economically and physically accessible to service people and civilian employees at the base.

1.2.6 Municipal Services

Project Objective: A Community Facility District will be established so that tax revenue from new residential development is equal to or greater than the cost of municipal services. This municipal service demand includes public safety (police protection and fire protection) park maintenance, street maintenance, and recreational programs.

Key Concepts:

- Ensure the FTSSP will not diminish service for remainder of City through adoption of a Community Facility District (CFD) and phasing.
- Adopt CFD to ensure the FTSSP generates revenue equal to cost of City services.
- Ensure the FTSSP has adequate City services including:
 - police protection
 - fire protection
 - park maintenance and recreational programs
 - street maintenance
- Promote Project self-sufficiency and sustainability with benefits to the wider community.



1.2.7 Municipal Improvements and Facilities

Project Objective: New development will be required to provide all municipal facilities and improvements needed to achieve and maintain adequate levels of service. Types of improvements include road, water and sewer infrastructure, drainage improvements, and parks, and may include a new fire station. New development will pay for these improvements through a combination of impact fees, exactions and areawide financing district(s).

Key Concepts:

- Ensure municipal improvements and facilities in the FTSSP are equivalent to or better than those found elsewhere in the City.
- Implement a public facility capital improvements financing plan that pays for all public facilities required by the community with revenues generated within the Project and that results in no new costs or tax burden on existing City residents.

1.2.8 Public Amenities

Project Objective: Public amenities will be included which benefit the planned community, as well as the City as a whole. Examples of such

amenities include, but are not limited to, a community park, a lake, and a library.

Key Concepts:

- New development in FTSSP is required to construct and maintain:
 - A public Lake Park with associated trails and day use activities that serves the local as well as citywide residents.
 - A public Great Park with associated sports fields, water features, day use areas, parking, and trails.
 - Private parks and recreational amenities as needed to meet the recreational and wellness needs of the community, including recreation centers and pocket parks.
- FTSSP will designate sites for an elementary school and public library and contribute to site acquisition and construction through payment of impact fees to Travis Unified School District and Solano County, respectively.

1.2.9 Economic Development

Project Objective: The Project shall provide opportunities for new jobs within industrial development east of the railroad, and retail and office employment west of the railroad.

Key Concepts:

- Capture the unmet demand for industrial and manufacturing in the region by developing a new business park offering a flexible array of business sites.
- Exploit the rail access on the site by providing one or more spurs to parcels within the Project and facilitating rail served industry.
- Create permanent employment opportunities by attracting high-quality employers to the FTSSP through transportation improvements, accessible housing, and a supportive business climate.
- Collaborate with community stakeholders to design and implement a business plan to capture jobs created by businesses in emerging and high-growth sectors.
- Create an environment that enhances business creation through expedited permit processing.

1.2.10 Sustainable Development

Project Objective: Sustainability will be promoted through such design features as compact development, mixed land uses, higher densities, transit and nonmotor transportation modes, energy conservation, and water conservation.



Key Concepts:

- Create an urban form that organizes land uses around mixed-use services and activity cores so as many people as possible are close to daily services.
- Conserve high-value habitat and sensitive and threatened species by creating open space preserves and conservancy areas.
- Establish strategies to respond to climate change by decreasing greenhouse gas emissions by reducing vehicle trips and building energy efficiency within the Project.
- Reduce potable water demand with reduced irrigation demand and low water fixtures within buildings.

1.2.11 Redevelopment

Project Objective: The Project will facilitate the redevelopment of existing industrial uses along Peabody Road to residential, commercial, and mixed-use development. The proposed industrial land east of the railroad will be designed to provide opportunities for possible relocation of existing industrial uses.

Key Concepts:

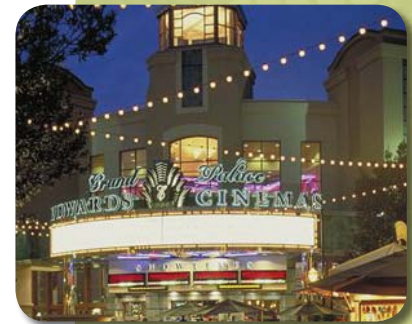
- Promote the timely transition of industrial properties adjacent to Peabody Road, Cement Hill Road and Vanden Road to uses consistent with the Specific Plan.
- Adopt FTSSP land use regulations that allow existing industrial uses and businesses within FTSSP to relocate to the employment area east of Vanden Road.
- Develop a phasing strategy that provides flexibility in timing for industrial relocation without sacrificing the integrity of the FTSSP vision.

1.2.12 Open Space

Project Objective: A robust open space system will be established to preserve key habitat areas and to provide public access and public recreational amenities in the portion of the Greenbelt within the Specific Plan area.

Key Concepts:

- Evaluate and reconfigure the Greenbelt as needed to improve configuration, increase size if possible, and integrate with the urban form of the Project.
- Incorporate previously identified and recommended preserves and conservation areas into the plan.





- Complete surveys for all sensitive and threatened species including demarcation of appropriate areas for preservation.
- Create a full delineation of wetlands and develop a protection and mitigation plan in collaboration with regulatory agencies.
- Create a hierarchy of parks, including neighborhood parks, pocket parks, and paseos to provide open space relief and recreational opportunities for the community and Fairfield residents.

1.2.13 Interagency Coordination and Cooperation

Project Objective: The City commits to work in coordination and cooperation with other public agencies to balance Project objectives with those of other agencies. These agencies include Travis AFB, Solano Transportation Authority, Solano Water Authority, Solano County, Local Agency Formation Commission, City of Vacaville, Solano Irrigation District, and Travis Unified School District.

Key Concepts:

- Provide an ongoing process of communication with all regional stakeholders.
- Be sensitive to the objectives and needs of associated public agencies and incorporate into the plan to the degree practical.

1.3 Project Description

The vision and Project objectives above have guided the crafting of a community framework based on sustainable urban form, traditional neighborhood planning principles, transit-oriented design, and an integrated open space concept (Figure 1-2, Land Use Plan).

The plan area encompasses 2,972 acres of land with the largest proportion of land area committed to Greenbelt open space and habitat conservation open space (approximately 60 percent), the second largest to residential uses (17 percent), and the third largest to industrial uses (approximately 10 percent). Key characteristics include:

- Construction of a variety of housing types with up to 6,800 dwelling units on 504 residential acres,
- Provision of a minimum of 3,000 dwelling units within a half-mile radius of the Train Station,
- Development of new industrial businesses on approximately 286 industrial acres,
- New stores and businesses on up to 47 commercial and mixed-use acres,
- An elementary school and a library on up to 12 acres,
- Park land totalling at least 156 acres, and

- Resource conservation and mitigation resulting in 869 acres of conserved open space.

The following is a brief description of the primary land use elements around which the FTSSP community is designed.

1.3.1 Community Form and Organization Elements

Transit-Oriented Town Center

The Town Center is planned as the walkable mixed-use hub of social, entertainment, shopping, and business activity for the FTSSP. The transit-oriented aspect derives from its proximity to the proposed Fairfield-Vacaville commuter Train Station, providing the opportunity for those residing in the Town Center to conveniently walk to the station and gain non-auto access to major employment, shopping, entertainment, and cultural destinations within the region.

The proposed primary anchor is a full-sized food market/grocery store located with full visibility and access from Peabody Road. Combined with associated in-line retail, food and beverage, and one or more retail/commercial pads, a full-service neighborhood commercial center is constituted serving the Specific Plan area as well as neighborhoods to the south and west.

A portion of the retail shops is to be configured as a traditional Main Street. Oriented perpendicular to Peabody Road and served by a right-in, right-out intersection, the one-block-long shopping street will encourage pedestrian activity and instill a human scale and visually engaging retail and dining experience. Mixed-use configurations of office or apartments over ground floor retail will be encouraged. It is important that two-story massing be achieved to ensure the appropriate scale and proportion for the street. Either angle or parallel parking will be allowed on the street to serve the convenience function of the shops.

Near the terminus of the Main Street, a Town Square is planned that would provide a place for community events, concerts, art fairs, and possibly a farmers market. The area around the square, behind the grocery store, and extending down to the Train Station will be zoned for mixed-use to encourage the location of service commercial uses on the ground floor. The primary use in the Town Center other than neighborhood retail is high-density residential in order to provide as many residences as possible within easy walking distance to both services and transit.

An important component of the urban design structure of the plan is the Civic Boulevard that links the Town Square to the Lake Park. This is envisioned as a street with a visually striking double row of trees along both sides under which a broad walkway would provide a pleasant shaded walking experience between the two destinations. Housing along the edges shall be rear-loaded so the frontage along the street is characterized by porches, stoops, and detailed architecture



creating an engaging and sophisticated backdrop. The housing in this district is proposed as medium to high density, to locate a maximum population within reach of the services and amenities of the Lake Park and Town Center, and form a quasi-urban neighborhood, bustling with activity, neighbor-to-neighbor interaction, and eyes-on-the-street security.

Lake District

The Lake District is envisioned as a one-of-a-kind amenity in the City which will draw hundreds of users a day from surrounding neighborhoods as well as the City-at-large. Planned as a mixed-use district surrounding an approximately 11-acre manmade Lake, it will be accessible to everyone in the community and offer a wide variety of neighborhood and recreational activities including walking, picnicking, day use areas, and small, nonpowered craft boating.

The design of the Lake is unlike most residential lakes in the region in that it does not allow homes to be backed directly up to the water's edge and block pedestrian access. It is conceived not as just a lake, but as a park with a lake within. A trail loops around the entire perimeter of the lake allowing walking, skating, baby strolling, and jogging on approximately 11 acres. The trail is proposed to be articulated with seating areas, a par course, fishing outcroppings, meadows, plazas, and other elements that choreograph the experience to engage, entertain and encourage healthy walking activity.

Also unlike most lake projects, rather than locate the lowest density and most expensive housing around the water, here higher and mid-density housing is proposed to allow the largest number of residents to enjoy the views and have walking access to the activities within the park.

The center of activity for the Lake Park occurs at the south end where a small village retail center, the Village Core, is oriented around a waterfront plaza. A promenade wraps around this southern portion of the Lake creating a setting that invites visitors to enjoy the views of the lake while sitting in a cafe or exploring local shops. Residents will have the ability to live within view of the Lake, meet their daily service needs, dine, exercise within the Lake Park, and end the day listening to a concert in the meadow; all within walking distance of their homes.

Residential Neighborhoods

Neighborhoods within the FTSSP will range from those with sizable residential lots along curvilinear streets adjacent to the open space preserve to eclectic brownstone flats just down the street from coffee shops and restaurants. The Specific Plan encourages a broad variety of housing sizes and price points as well as a mix of neighborhood designs.

Higher density neighborhoods in the Lake District and Town Center District will cater to the more urbane resident, alternative families,





young couples, and empty nesters; providing them a walkable, secure, and high-service environment in which to live, work, and play.

Mid-density neighborhoods will support young and growing families and allow single-family detached living at the most affordable prices. This neighborhood type is probably the most attractive to the marketplace due to the blend of price and lifestyle and will make up a large portion of the community. The most innovative single-family housing types will occur within this category with cluster housing, alley-loaded, and other small-lot homes. Due to the smaller lot sizes it will be important to provide an integrated park and trail system to the school, Lake Park, and Town Center.

Lower density residential neighborhoods will be designed for growing and mature families with tree-lined streets, pocket parks, and the occasional cul-de-sac. Although more on the outskirts of the community, these residents are designed with larger yards, views, and convenient access to the surrounding open space.

In all neighborhood types, the goal is to re-establish the neighborhood as a social unit with maximum opportunity for neighbor-to-neighbor interaction, eyes-on-the-street surveillance, and for most dwellings to be no more than two blocks from a park or other open space. In general, neighborhood types will vary by the mix of housing type, block size, and orientation to amenities and open space. By creating variation in neighborhood form and design, a more authentic town character will result, which will better support long-term social and economic sustainability, as well as expand choice for the region's diverse population.

Industrial and Business Park

Approximately 300 acres of land is designated for business park and industrial use carefully configured to avoid ecological and wetland impacts. The rail service bisecting the Project area also provides the opportunity for rail spur extensions to either of the industrial park zones. The flexibility of land use designation, lack of direct adjacency to residential areas, and rail access create a unique opportunity to meet unmet regional demand for light and medium manufacturing.

In addition to industrial, these districts may include other employment types including incubator new business, multi-tenant commercial, office, and corporate campus sites that integrate management, manufacturing, and distribution. To protect view sheds from Vanden Road, the more intensive industrial uses will be located, through the use of guidelines, to the most eastern portions of the districts.

Consistent with the goals of reducing green house gas emissions by limiting vehicle miles travelled (VMT), each of the two industrial districts are encouraged to include a modest service core of business-oriented services (retail, medical, food service). These small activity cores will further reduce unnecessary trips to meet daily worker and business needs.

Open Space

Thirty percent of the Project area is devoted to the preservation and conservation of natural lands. A total of 862 acres of land (habitat conservation, mitigation bank, and vernal pool conservation area) is committed to natural lands preservation with an additional 824 acres of open space in the community as parks, paseos, and recreation network. Key components of the open space system include the 593-acre habitat conservation, the 203-acre mitigation bank, the 50-acre Great Park, and the 22-acre Lake Park. These key components will ensure the conservation of open space lands within that portion of the Vacaville-Fairfield-Solano Greenbelt within FTSSP boundary. The Specific Plan proposes to reconfigure the Greenbelt boundary to better align with ecological conservation priorities and appropriate land use patterns.





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2.0 PLAN SETTING



2.0 PLAN SETTING

July 26, 2011



2.1 Site Opportunities

Strategically located midway between the Bay Area and Sacramento, the Specific Plan area is well positioned in one of the most dynamic growth corridors in the state. Community development along the I-80 corridor has extended farther from job centers in the inner-Bay Area. Communities in Vallejo, Hercules, and Cordelia are nearing build-out. Few new large opportunity sites exist in western Solano County and Northern Contra Costa County which can accommodate new master plan communities. Rather, communities such as Benecia, Vallejo, Richmond, and Hercules are planned to accommodate future housing needs on smaller infill development centers and in their urban cores. The limited supply of available large development sites west of Fairfield combined with planned transportation improvements on the Capitol Corridor commuter rail line pull development forces to Northeast Fairfield. This presents a near-term opportunity for Northeast Fairfield to accommodate regional demand for master planned communities.

As growth continues to advance into outlying areas of the Bay Area, concerted efforts have, and are being made, to control its form; guiding it into transportation corridors that promote movement between housing and job centers by utilizing commuter rail and other sustainable modes. Senate Bill (SB375) is such an effort, attempting to restructure outlying growth in a manner that provides low carbon access to jobs, services and transit. The Fairfield Train Station site is a prime example of regional infill within such a transportation corridor. Few opportunities exist of this scale in the region where significant new housing at suburban and quasi-urban densities can occur with such direct access to commuter rail and proximate employment.

The following are other key attributes of the site location that support the Project as an example of sustainable development in a suburban setting:

- Direct adjacency of the proposed Fairfield-Vacaville Train Station, which will provide access to multiple stations within the Capitol



Corridor, many with job centers accessible to Project residents and housing areas that will become more transit accessible to proposed Project employment

- Presence of industrial land uses, some in transition, that provide an opportunity for redevelopment and relocation to more suitable business park locations
- Presence of significant natural open space resources including the Travis Reserve that can be incorporated into the master plan in a manner resulting in long-term preservation and stewardship
- Proximity of the Vacaville-Fairfield-Solano Greenbelt allowing the Project to refine the Greenbelt configuration consistent with preservation of the highest value natural resources and habitat while converting large portions of the Greenbelt from a preserve-in-concept to one grounded in zoning and long-term conservation
- Proximity of Travis AFB with over-flight hazard and noise zones that promote employment uses within the Project boundaries and in proximity to the base and Project-related housing
- Expanded arterial roadway access via proposed improvements on Vanden Road, Peabody Road, and Manuel Campos Parkway, which will allow more direct access from Fairfield and Vacaville to the new job center proposed within the Project, reducing the need for existing residents to travel outbound of the area for employment
- The location of the North Bay Water Treatment Plan within the study area allowing the provision of potable water resources for the community
- Large ownership pattern over much of the Specific Plan area allowing a coherent master planned concept with citywide amenities to be developed in a timely and economically sustainable manner

These site location attributes provide an unusual opportunity for a new expansion of the City; one that can meet the emerging demands related to transportation, balanced growth, resource preservation, and climate change.

2.2 Project Location

The Fairfield Train Station Specific Plan (FTSSP) is located in the northeastern portion of the City of Fairfield west of North Gate Road, south of the City of Vacaville's city limits and north and west of Travis AFB. The Specific Plan area encompasses approximately 2,972 acres.

The regional location of the Specific Plan is shown in Figure 2-1 and the Specific Plan Boundary is shown in Figure 2-2.

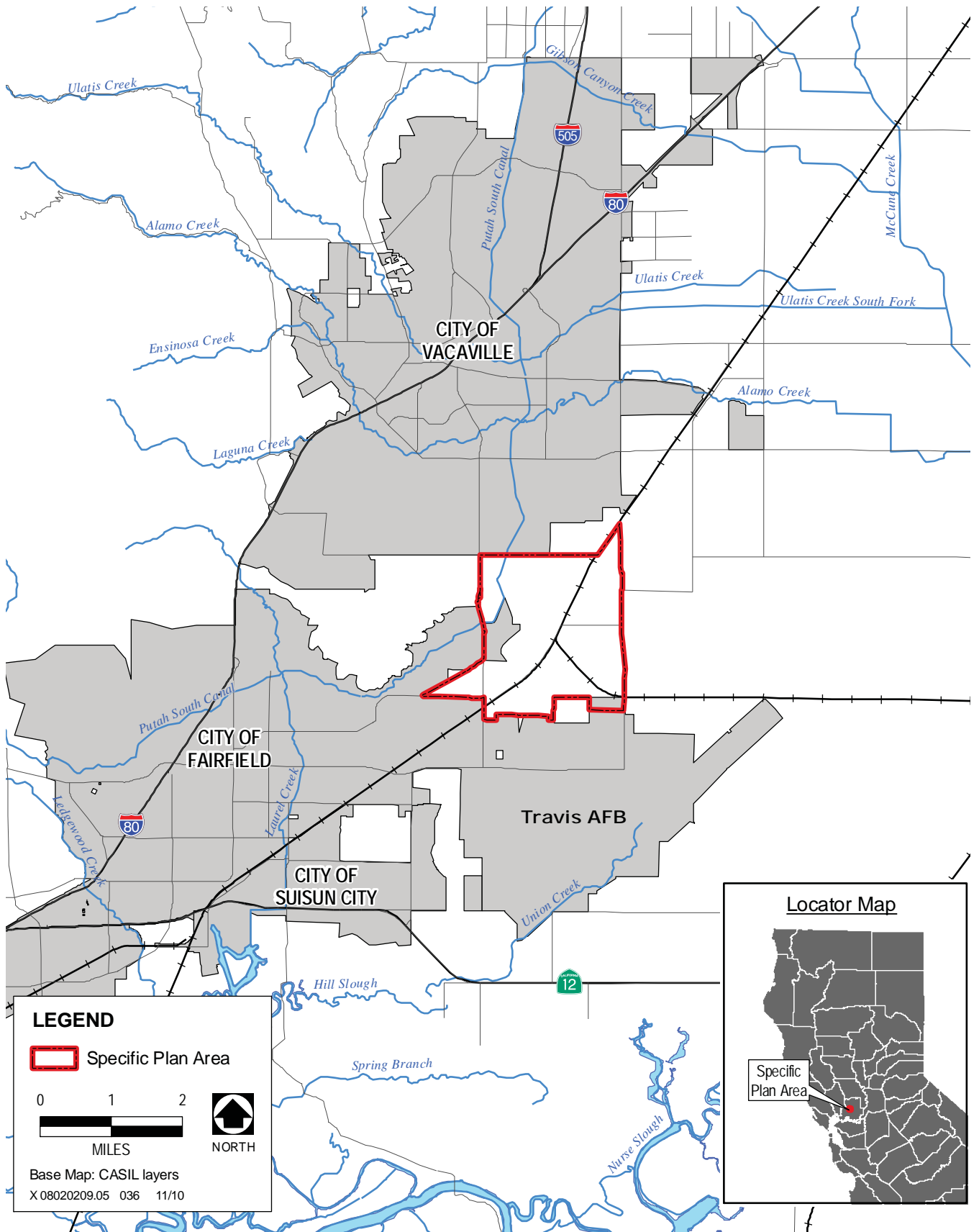


Figure 2-1: Regional Location

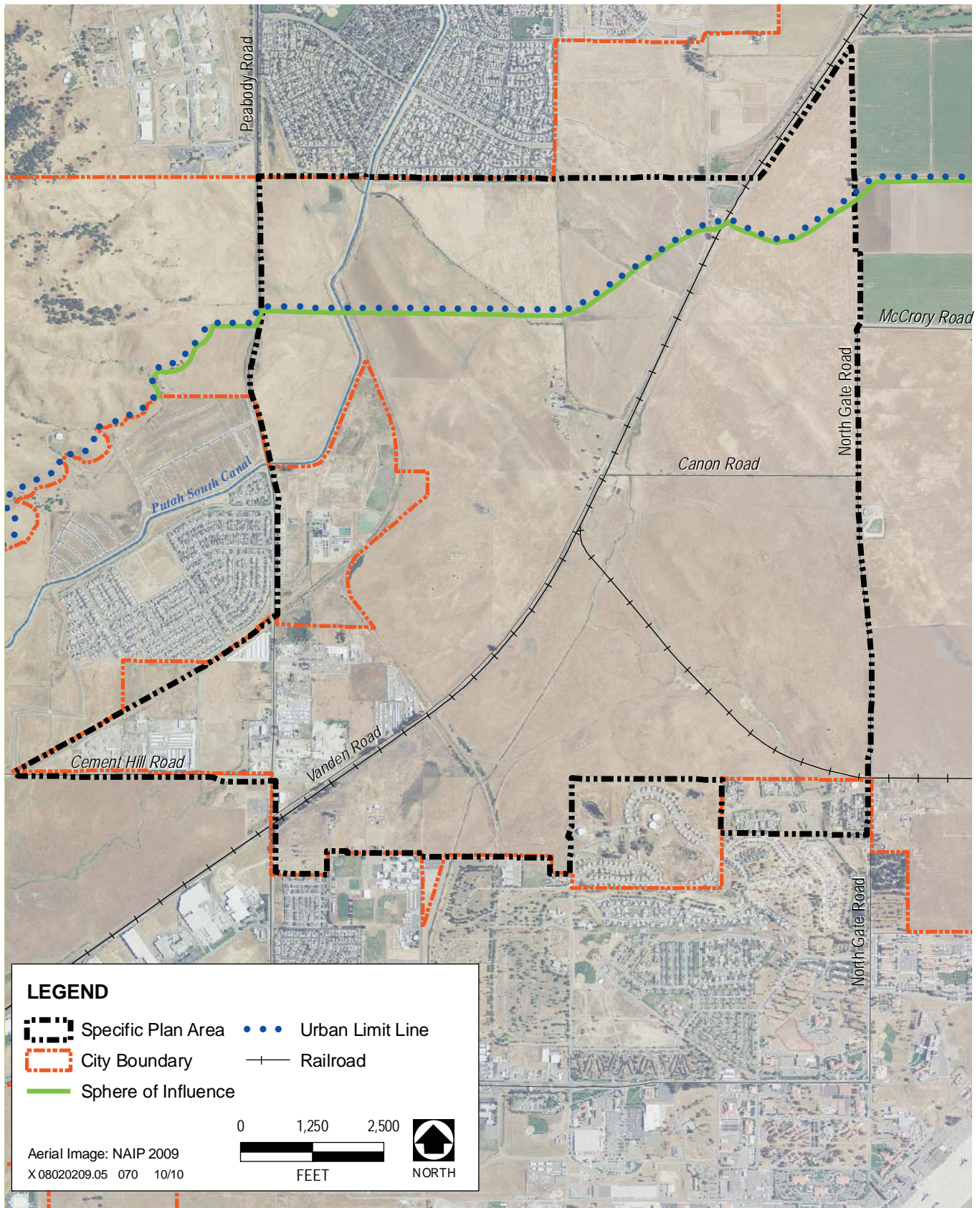


Figure 2-2: Specific Plan Boundary

2.2.1 Specific Plan Area

The Specific Plan area is located within the City of Fairfield's planning area but extends outside of the current City limits and Sphere of Influence (SOI). All proposed urban development areas within the Specific Plan are within the City's current SOI and General Plan voter-approved Urban Limit Line. Proposed open space areas in the northern portion of the Specific Plan area extend outside the City's current SOI and Urban Limit Line.

A variety of land uses currently exist on-site. The northern portion of the Project site is used for agricultural and cattle grazing purposes. This area has roads, agricultural irrigation structures (i.e., canals, water storage tanks, and ponds), rural housing, and agricultural buildings. The southern portion of the Project site contains a portion of the Travis Reserve, which is designated as open space in part to ensure against incompatible uses near Travis AFB. Areas in the southern portion of the Specific Plan area also have a small number of rural residences.

Southwestern portions of the Specific Plan area are developed with industrial and related uses, including numerous businesses (e.g., Recycling Zone, Cement Hill Ready Mix, Allied Supply, Syar Industries, Self Storage Unlimited). Roughly 900,000 to 950,000 square feet of light industrial, industrial, and similar uses exist in this portion of the site. The southeastern extremity of the Specific Plan area is developed with approximately 300 dwelling units formerly associated with Travis AFB. Western portions of the Specific Plan area house the North Bay Regional Water Treatment Plant, which is jointly operated by the cities of Fairfield and Vacaville. Single-family attached homes were recently constructed to the west of the North Bay Regional Water Treatment Plant within the Specific Plan area. With the exception of the above-mentioned land uses, the remainder of the Project site is generally used for grazing or is fallow.

Interstate 80 (I-80) and State Route 12 (SR-12) are the major roadways that provide circulation to the Project area. I-80 is a major east-west freeway originating in the San Francisco Bay Area to the west, continuing east toward Sacramento, and terminating in New Jersey. SR-12 is an east-west state highway, also called Rio Vista Road in the Project vicinity, which extends from SR-49 in San Andreas to a junction with SR-1 near Bodega Bay in Sonoma County. Regional roads such as Peabody Road and Vanden Road serve as important circulation routes for the Project area. The public transit system in Fairfield includes both bus and rail passenger components, providing local and regional connections to Fairfield residents.

2.3 Site Conditions

2.3.1 Climate and Topography

The Specific Plan area has a mild Mediterranean climate characteristic of the region, with hot and dry summers and cool and wet winters. In





summer, when the high-pressure cell is strongest and farthest north, fog forms in the morning, and temperatures are mild. In winter, when the high-pressure cell is weakest and farthest south, occasional rainstorms occur. Regional wind flows in the area range from mild to moderate, with the wind direction in Fairfield in the summer being very consistent and generally parallel to Vanden Road alignment. Average wind speeds for Travis AFB range from 8 miles per hour (mph) in the winter to 17 mph in the summer.

The average annual precipitation for Fairfield is 22.7 inches, with most occurring between the winter rainy season, particularly December through March. The average maximum temperature is 73.5 degrees Fahrenheit (F), and the average minimum temperature is 47.4 degrees F. Summer high temperatures can on occasion exceed 100 degrees F.

The Project site is located along the western margin of the Sacramento Valley, adjacent to the foothills of the Coast Range. The most prominent topographic features of Solano County in the Specific Plan area are the Vaca Mountains to the west and Cement Hill, 891 feet above mean sea level (MSL), approximately one-half mile to the northwest. The Project site is characterized by gently rolling hills with a gradual decrease in elevation of approximately 70 feet from northeast to southwest. Elevations at the Project site range from 100 feet above MSL in the northeast to 60 feet above MSL in the southwest. Several knolls are present in the central portion of the Project site with heights ranging from 130 to 180 feet above MSL.

2.3.2 Grazing Lands

Currently, approximately 60 percent of the Specific Plan area is open space/parks. The northern portion of the Specific Plan area is used for agricultural and cattle grazing purposes. This area has roads, agricultural irrigation structures (i.e., canals, water storage tanks and ponds), rural housing, and agricultural buildings. According to the Solano County Important Farmland Map, approximately 2,645 acres of the Project site are designated as Grazing Land, while 316 acres are classified as Urban and Built-Up Land, and 11 acres are considered Other Land. Most lands adjacent to the Greenbelt proposed for development under the Specific Plan are used for grazing rather than cultivated lands.

2.3.3 Williamson Act Contracts

The Williamson Act was enacted in 1965 to preserve agricultural and open space land by providing lower property taxes for individual landowners and payments of subventions to counties for some of the losses of property tax revenues. There are approximately 873 acres of land under active Williamson Act contracts within the Project and in its immediate vicinity, of which approximately 199 acres fall within the Specific Plan area. Active Williamson Act contracts within the Project include Contracts #56 and #508 and small portions of Contracts #45

and #55. Contract #508 was protested by the City at the time of the contract's proposed establishment, through adoption of City of Fairfield Resolution 70-3. The City may exercise its right not to succeed to the contract for any land being annexed that was within a mile of the City's boundary when the contract was executed without financial penalty.

2.3.4 Biological Resources

All of the Specific Plan area has been subjected to some level of agricultural disturbance, and much of the site has been extensively levelled for agricultural use, which has resulted in substantial disturbance and alteration of natural habitats on the site, including historic vernal pools and vernal pool grassland. The predominant plant community in the Specific Plan area is nonnative annual grassland, but wetland and vernal pool grassland and plant communities are present within vernal pools and swales, drainage channels, and other depressions on-site. Disturbed vernal pools and other seasonal wetlands are scattered throughout the Specific Plan area but occur in higher concentrations east of Vanden Road. In some instances, the vernal pool basins remain intact but the vegetation has been converted from vernal pool endemic plant communities to communities dominated by nonnative annual grasses, such as Italian ryegrass and Mediterranean barley. In other cases, the surface topography has been altered sufficiently to result in hydrological changes, such as shortened hydroperiod. In the most extreme cases, vernal pool basins have been completely eliminated through grading.

The Specific Plan area supports an abundant and diverse fauna since the large and contiguous open spaces are important to native wildlife species associated with grassland and seasonal wetland habitats. A total of 21 special status plant species have been documented to potentially occur in the Specific Plan area. Alkali milk-vetch, heartscale, brittlescale, San Joaquin sparscale, papoose tarplant, Carquinez goldenbush, and Baker's navarretia are considered to have high potential for occurrence because suitable habitat conditions are present and there are known occurrences in the immediate vicinity of the Specific Plan area.

The Specific Plan area provides critical habitat for several endangered species, as well as habitat for raptors that prefer large tracks of open grassland for foraging. The combination of open grassland with available seasonal and perennial water sources is attractive to many of the common wildlife species found in Solano County, as well as a few special-status wildlife species. Numerous common wildlife species can be observed within the annual grassland habitat on-site including a wide variety of songbirds; birds of prey; reptile species; and mammals including coyote, fox, raccoon, skunk, and possum.

In addition to the grassland species, several wildlife species can be found in aquatic habitats of Union Creek and the irrigation ditches,





ponds, and vernal pools on the Specific Plan area including bird, turtle, and frog species.

Special-status wildlife species have been observed in the Specific Plan area. According to database searches and field surveys, the presence of vernal pool fairy shrimp, California tiger salamander, western pond turtle, burrowing owl, Swainson's hawk, and loggerhead shrike have been documented in the Specific Plan area.

It is the relatively rich array of species and habitat associated with the wetland areas and their interface with dryer upland zones that have led to over half of the study area being committed to open space and a reconfiguration of the Greenbelt to best serve their conservation.

2.3.5 Hydrology

The Specific Plan area is within the Suisun Hydrologic Unit within the San Francisco Bay Regional Water Quality Control Board (RWQCB), with a portion of the site located in the Central Valley RWQCB. Union Creek flows south through the Specific Plan area to Suisun Marsh and Bay, the receiving waters for the creek. The majority of Specific Plan area is within this watershed. However, some of the west-central portions of the site are part of the McCoy Creek and McCoy Basin watershed. The southeastern portion of Noonan Ranch drains to the Denverton Creek watershed and the northeastern portion of Noonan North is part of the Barker Slough watershed.

The northwest portion of the Specific Plan area is traversed by the Putah South Canal. A segment of a Solano Irrigation District canal traverses the northeast corner of the Specific Plan area.

There are four groundwater basins within Solano County: the Napa-Sonoma Lowlands subbasin within the Napa-Sonoma Valley basin, the Suisun-Fairfield Valley basin, and the Solano and Yolo Valley subbasins within the Sacramento Valley basin. The northeast portion of the Specific Plan area overlies the Solano basin, the northwest corner overlies an undefined aquifer, and the remainder of the Specific Plan area overlies the Suisun-Fairfield basin.

2.4 Existing and Surrounding Land Uses

Vacaville city limits are located adjacent to, and north of the Specific Plan area. Portions of the City of Vacaville north of the Specific Plan area are either developed with residential uses (Gonsalves Lockie [Foxboro] Policy Plan, to the west), or are planned for residential development. These areas of south Vacaville are predominantly low-density neighborhoods of single-family detached housing.

Lands to the east of the Project site are large ownerships used for agricultural purposes. Much of the area to the east has overlays of land

use restriction related to Travis AFB. These restrictions preclude residential use due to noise and other land use issues.

Travis AFB is located adjacent to the southern boundary of the Project site, east of Vanden Road. The land use along this boundary is primarily residential related to base housing. There is also an elementary school along the southern boundary.

Lands to the west of the Specific Plan have wetlands and other natural open space, as well as several tracts of low-density residential development with taller hills and steep topography providing ridgeline views farther to the west.

2.5 Historic Context

2.5.1 Regional Overview

Mexico ceded California to the United States under the Treaty of Guadalupe Hidalgo, in 1848 (Hoover et al. 2002). John Marshall discovered gold in the tailrace of his mill at Coloma, just days prior to the treaty. The discovery of gold in California incited a massive influx of settlers, including gold seekers and immigrants from the United States and the rest of the world. The influx rapidly changed the demographics and cultural landscape of California and increased interaction with, and destruction of aboriginal populations, including those in the Central Valley.

During the Gold Rush, ranchers and farmers quickly discovered that selling their crops to miners was extremely profitable. In the late 1840s and 1850s, former gold seekers and pioneers began settling Solano County, where they raised livestock and cultivated fruit orchards, vineyards, wheat, barley, and oats. Produce and livestock were transported overland by wagons to docks located at the numerous sloughs throughout the county, where they were then shipped to market. As a result of this trade, economic development, and arrival of the railroad, 12 townships were established in Solano County between 1850 and 1871.

The construction of two rail networks further spawned economic growth in the region. In 1868, the completion of the California Pacific Railroad through Solano County, and within the Specific Plan area, allowed the shipment of goods to east coast markets, significantly bolstering economic development, agricultural production, and population growth. Solano County's most prevalent economic activities continue to be agriculture and livestock. A wide variety of vegetables, fruit, and nuts are grown, with walnuts being the most recent crop that has gained in economic importance. Solano County is one of the top five California producers of corn, lamb, sheep, and Sudan grass hay.

The Specific Plan area is situated in the northeast portion of Fairfield. Approximately 1 mile west of the Project area, now incorporated into

History

Prior to 1810 - *The earliest native inhabitants settle in the Rockville and Green Valley areas - earliest traces of Indian settlements in Northern California.*

1810 - *Gabriel Moraga - The first Spanish attack against the local Suisun Indians.*

1835 - *A major battle between General Mariano Guadalupe Vallejo (Mexican government) and Chief Sem Yeto.*

1837 - *Chief Solano applies to the Mexican governor for a land grant for the Suisun tribe.*

1842 - *Chief Solano sells his grant to Vallejo for \$1,000.*

1850 - *Chief Solano and the remaining Suisun tribe move to the Napa area.*

1856 - *Captain Robert H. Waterman lays out the townsite of Fairfield, named after his hometown in Connecticut.*

1858 - *County seat moves to Fairfield from Benicia.*

1860 - *The first county buildings area constructed.*

1903 - *Fairfield is formally incorporated as a city.*

1942 - *US Air Force builds a major base to the east of Fairfield. Travis Air Force Base is annexed to Fairfield on March 30, 1966.*

2003 - *Fairfield celebrates its 100th anniversary - population 100,200.*

2009 - *One of the most desirable urban growth centers in Bay Area - population 106,502.*

2010 - *Fairfield Train Station Specific Plan.*



Gen. Mariano Guadalupe Vallejo



Capt. R.H. Waterman

Fairfield, was the historic town and factory of Cement. Each of these communities is described in the paragraphs that follow.

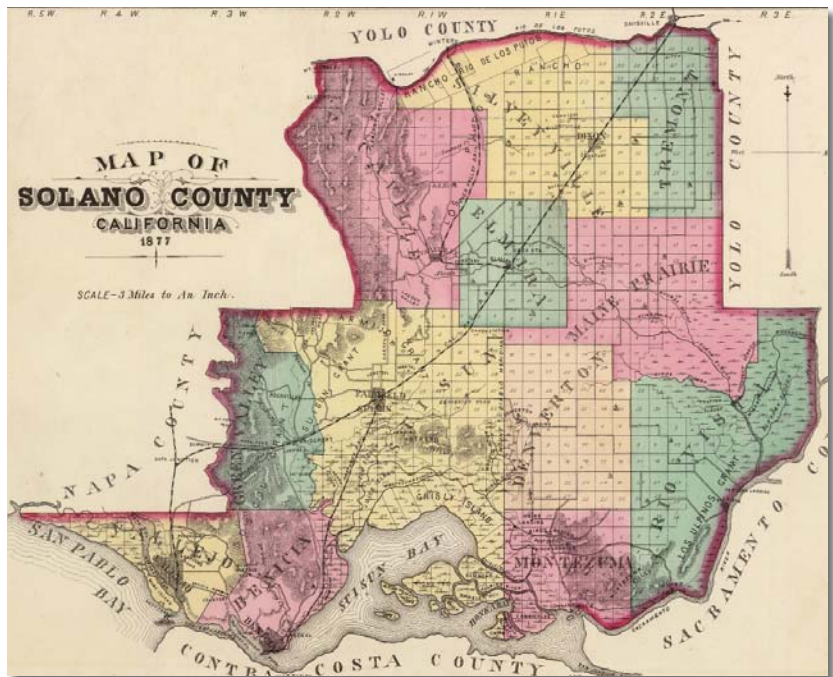
Fairfield

Fairfield is located along the growing “Capitol Corridor” between San Francisco and Sacramento on I-80. Its central location makes it accessible to Bay Area employment and cultural assets, the state capital, and major recreational areas such as the delta regions and the Sierra Nevada.

History

Fairfield is situated on land petitioned by José Francisco Armijo in 1839. The land was granted to Armijo by Governor Pio Pico in 1840 as Rancho Tolenas. The land passed to Armijo’s son, Antonio, in 1850 upon his death and was subsequently acquired by Captain R.H. Waterman in 1858. A clipper ship captain who had sailed around the world five times, Waterman decided to settle in Suisun Valley with his wife, Cordelia (for whom the Cordelia area of Fairfield is named). Waterman offered 16 acres to Solano County for a new, more centrally located county seat. Solano County voters accepted the offer, and the county seat was relocated from Benicia. Waterman laid out the town site of Fairfield, which he named after his hometown in Connecticut. The first county buildings were constructed (a brick courthouse and jail were built for \$15,400). In 1903 Fairfield formally incorporated as a city.

In 1942 ,the United States Air Force decided to build a major base installation on a tract of land located to the east of Fairfield, giving a tremendous boost to the local economy. Travis AFB became one of the



major departure points for military units heading for combat in Vietnam. The base was annexed to Fairfield on March 30, 1966.

In 2003 Fairfield celebrated its 100th anniversary as a city in Solano County. The City buried a time capsule at the City Hall complex, to be opened in 2103. Its population at that time was 100,200 and it was the 60th largest city in the state.

The City's location, natural amenities, and abundant land sites are some of the attributes that have made Fairfield a desirable place to live for those seeking a less urban experience than San Francisco or who are a part of the long-time agricultural economy of the area demonstrated by the population growth from 3,100 people in 1950 to 106,502 in 2009.

Vacaville

In 1850, Don Manuel Vaca deeded 9 square miles of Rancho Los Puntos to William McDaniel to lay out a town that was to be named after him. The fruit industry around the area of present Vacaville began in the late 1850s when Ansel Putman and John Dolan, local nursery owners, along with William and Simpson Thomas constructed a road from Pleasants Valley to Suisun City. This roadway, which was later known as Pleasants Valley Road, provided access for the shipment of fragile fruit from the Vaca, Pleasant, and Laguna Valleys to major markets. This important transportation route spurred the development of land for commercial fruit and vegetable farming in the area. The construction of two major rail lines by 1870 broadened the market even further by providing access of shipment overland across the country. By the 1890s, Vaca Valley and the foothills to the west were covered with orchards encompassing almost all of the available non-irrigated land.

Cement

Cement was a Pacific Portland Cement Company town and factory built on Cement Hill in 1902. The town, just northeast of Fairfield, was connected by a short rail-line to the California Pacific Railroad. Sanborn Insurance maps prepared from 1912 and 1925 depict extensive industrial and residential facilities to extract raw materials for cement production, as well as to house and care for the large workforce necessary for the company town's success. Tufa deposits (i.e., clay used to make cement) were exhausted in 1927, and the town of Cement was abandoned. Today, the former town and its structures are part of a working ranch.





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3.0 SPECIFIC PLAN PROCESS



3.0 SPECIFIC PLAN PROCESS

July 26, 2011



3.1 Specific Plan Consistency

The Fairfield Train Station Specific Plan (FTSSP) is a land use, policy, and regulatory document designed to guide the development of the proposed Project and modify some of the City of Fairfield Zoning Code (Fairfield Municipal Code, Chapter 25, Article I) standards in order to implement Project-specific form, design, and neighborhood planning within the Project boundary. The following sections describe how the Specific Plan relates to and is consistent with other policies governing land use in the City.

3.2 Specific Plan Authorization

3.2.1 Specific Plan Purpose and Intent

The Specific Plan serves as both a policy and regulatory document for the development of the FTSSP community. This Specific Plan provides guidance to the City, Project developers and builders in designing a place that creates a transit-oriented community with multiple planning areas, a Lake District and Town Center District, and a diversity of residential and nonresidential uses catering to the needs of the residents within the Community and the City as a whole.

It is intended that the Specific Plan find a balance between guiding and regulating development in the Project area so that levels of performance and quality are defined to achieve exemplary built form. But it is a goal to ensure that builders and developers are also allowed flexibility to respond to the financial and market conditions of the time, whether the condition is a recovering or normalized economy.

3.2.2 State Authority

California Government Code, Title 7, Division 1, Chapter 3, Article 8, Sections 65450 through 65457 authorizes preparation and adoption of



specific plans to implement a jurisdiction's General Plan. State law specifies that a specific plan must address the following information either in text and/or graphics:

- The distribution, location, and extent of the uses of land, including open space, within the area covered by the plan;
- The proposed distribution, location, extent, and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the area covered by the plan and needed to support the land uses described in the plan;
- Standards and criteria by which development will proceed, and standards for the conservation, development, and utilization of natural resources where applicable;
- A program of implementation measures including land development regulations, capital improvement programs, public works projects, and financing measures necessary to carry out the above items; and
- A statement of the relationship of the Specific Plan to the General Plan.

The FTSSP establishes a development framework for land use, sustainability, resource management, circulation, public services, utilities, design, and implementation. Within this framework, all subsequent development projects and related activities within the Project boundary must be consistent and in conformance with this Specific Plan.

3.2.3 Plan Adoption

The Specific Plan is both a planning and regulatory document for the development and implementation of the FTSSP community. Any issues that are not specifically addressed by this Specific Plan shall be subject to the requirements and procedures of the City of Fairfield Zoning Code. In instances where there is a conflict between this document and the City of Fairfield Zoning Code, the Specific Plan shall prevail.

3.2.4 CEQA Compliance

In accordance with the California Environmental Quality Act (CEQA), the City prepared a Notice of Preparation (NOP) that identified potential significant environmental impacts associated with the FTSSP. Subsequently, an Environmental Impact Report (EIR) has been prepared to analyze potentially significant environmental impacts created by the Specific Plan and ensure its development is consistent with the Specific Plan policies as determined in Section 15162 of the CEQA Guidelines. Upon receipt of public comments and finalizing the

document, the EIR will be certified and the Mitigation Monitoring and Reporting Program (MMRP) will be adopted.

3.3 Specific Plan City Entitlement Process

The Project's entitlement process involves a General Plan Amendment (GPA), Zone Change, Development Agreement, annexation, and Subdivision Maps. The General Plan Amendment, Zone Change (amending the zone districts), and Development Agreement will be processed concurrently, followed by subdivision maps.

3.3.1 General Plan Amendment

The City is processing a General Plan Amendment concurrently with the Specific Plan to ensure consistency between the two documents. The proposed Amendment to the Land Use Diagram of the General Plan is shown in Figure 3-1, Proposed General Plan Amendment. The text of the General Plan Amendment is provided in Appendix B.

3.3.2 Fairfield Zoning Code Amendment

The City is processing an Amendment to the Zoning Ordinance concurrently with the Specific Plan. The Amendment will include changes to the Zoning Map and changes to the text of the Zoning Ordinance. See Figure 3-2, Zoning Map Amendments. The scope of the proposed Zoning Ordinance Amendment includes the following:

- Adoption of a new Train Station Specific Plan Planned Development Overlay Zone District (TS-PD)
- Change Zoning Map to prezone and rezone the Specific Plan area to the Train Station Specific Plan Planned Development Overlay Zone District (TS-PD)

Except as may be modified by the Train Station Specific Plan Planned Development Overlay Zone District, development and land uses within the Specific Plan area shall comply with the standard provisions of the City of Fairfield Zoning Code and the development standards of this document.

3.3.3 Environmental Impact Report

The Project EIR is required to analyze the potential environmental impacts of the Project and includes measures to mitigate said impacts to below a level of "less than significant." Specific areas of investigation included aesthetics, air quality, noise, land use, population and housing, biology, hydrology/water quality/drainage, geology and soils, utilities and public services, and traffic and circulation impacts. As the Lead Agency, the City of Fairfield prepared the Project EIR reflecting its independent judgment and analysis.













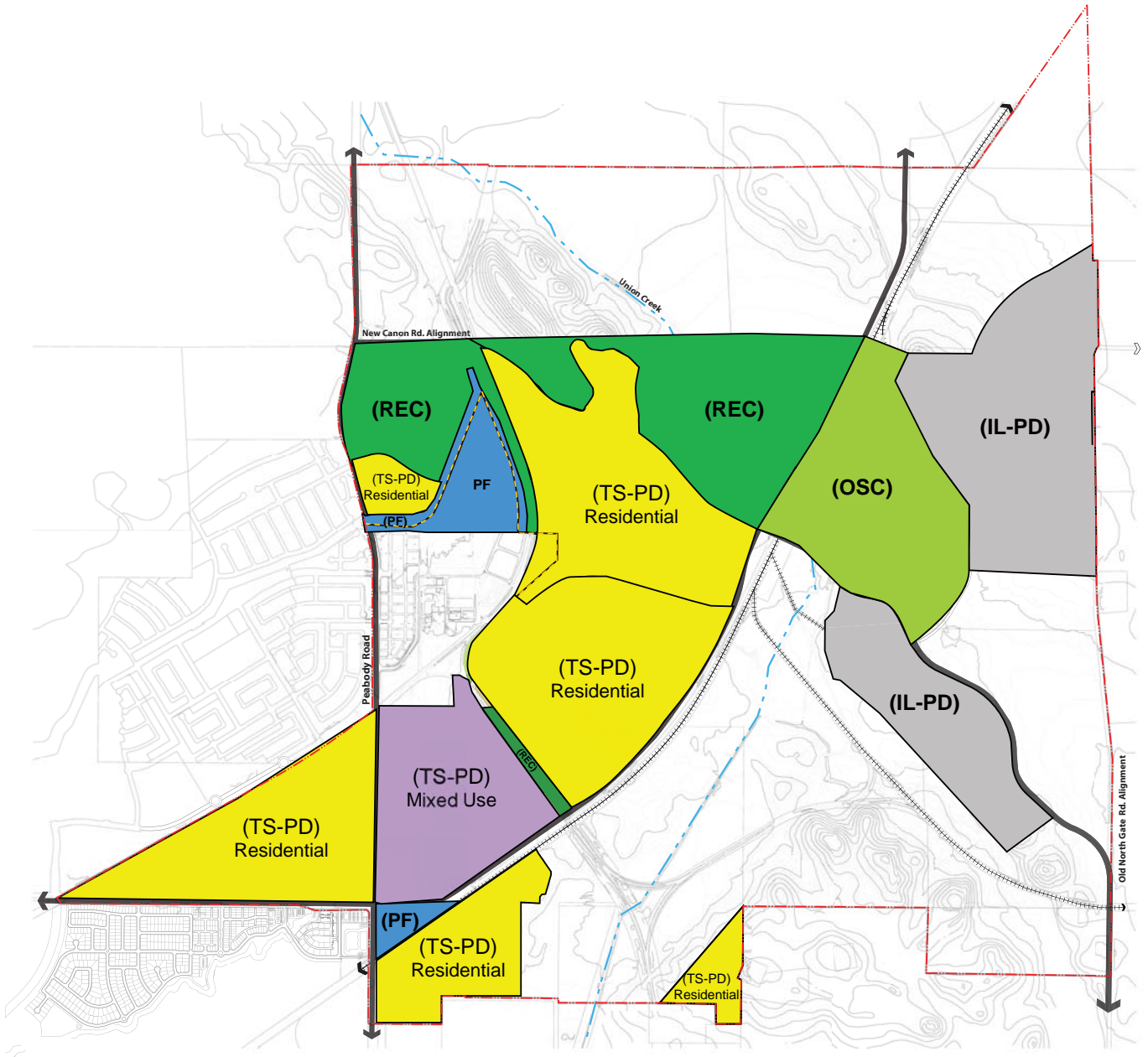
Legend	
	OSR - Recreation
	OSC - Conservation
	RVL - Very Low Density
	FTSSP
	IL - Limited
	IG - General
	PF - Public Facilities
	Urban Limit Line



Figure 3-1: Proposed General Plan Amendment



Legend	
	Recreation
	Public Facility
	Limited Industrial Planned Development
	Open Space Conservation
	Train Station Specific Plan Planned Development District - Residential
	Train Station Specific Plan Planned Development District - Mixed Use

Note: Rezoning is identified by parentheses: ()

Figure 3-2: Zoning Map Amendments



3.3.4 Development Agreement

One or more Development Agreements may be prepared and executed between the City of Fairfield and the land owners in conjunction with the Specific Plan. If adopted, the Development Agreement(s) will obligate the developers to implement public benefits and the City will vest entitlement approvals. Future Project proponents may enter into future development agreements in conformance with this Specific Plan.

3.3.5 Annexation

The Specific Plan proposes annexation to the City of Fairfield of those portions of the Specific Plan area which are designated for urban development or which are largely surrounded by proposed urban development. The boundary of the proposed annexation area is shown in Figure 3-3, Proposed Annexation Boundary. The Solano Local Agency Formation Commission (LAFCO) will consider various reorganization actions associated with annexation application. These actions include:

- Annexation to the City of Fairfield
- De-annexation from Solano Irrigation District those areas designated for residential development
- Annexation to Solano Irrigation District those areas designated for industrial development
- De-annexation from Vacaville Fire District
- Annexation to the Suisun Fairfield Rockville Cemetery District and the Vacaville Elmira Cemetery District, based on their respective Spheres of Influence

According to City of Fairfield Resolution No. 2010-91 (adopted April 20, 2010), the City acknowledges its desire that the Specific Plan area be incorporated into the City.

3.3.6 Subdivision Maps

The Project will require approval of a number of subdivision maps in order to subdivide the site into development parcels. The Project must comply with the state requirements on subdivisions found in the Subdivision Map Act and must meet the standards and laws identified in Government Code Section 66410.

3.4 Relationship to Other Plans

Several City plans and documents provide policy guidance on the development of the Project. The following is a list of pertinent plans.

3.4.1 City of Fairfield General Plan

The Specific Plan serves as a planning guide to implement the City of Fairfield General Plan within the plan area. The General Plan is being amended concurrently with adoption of this Specific Plan and, upon

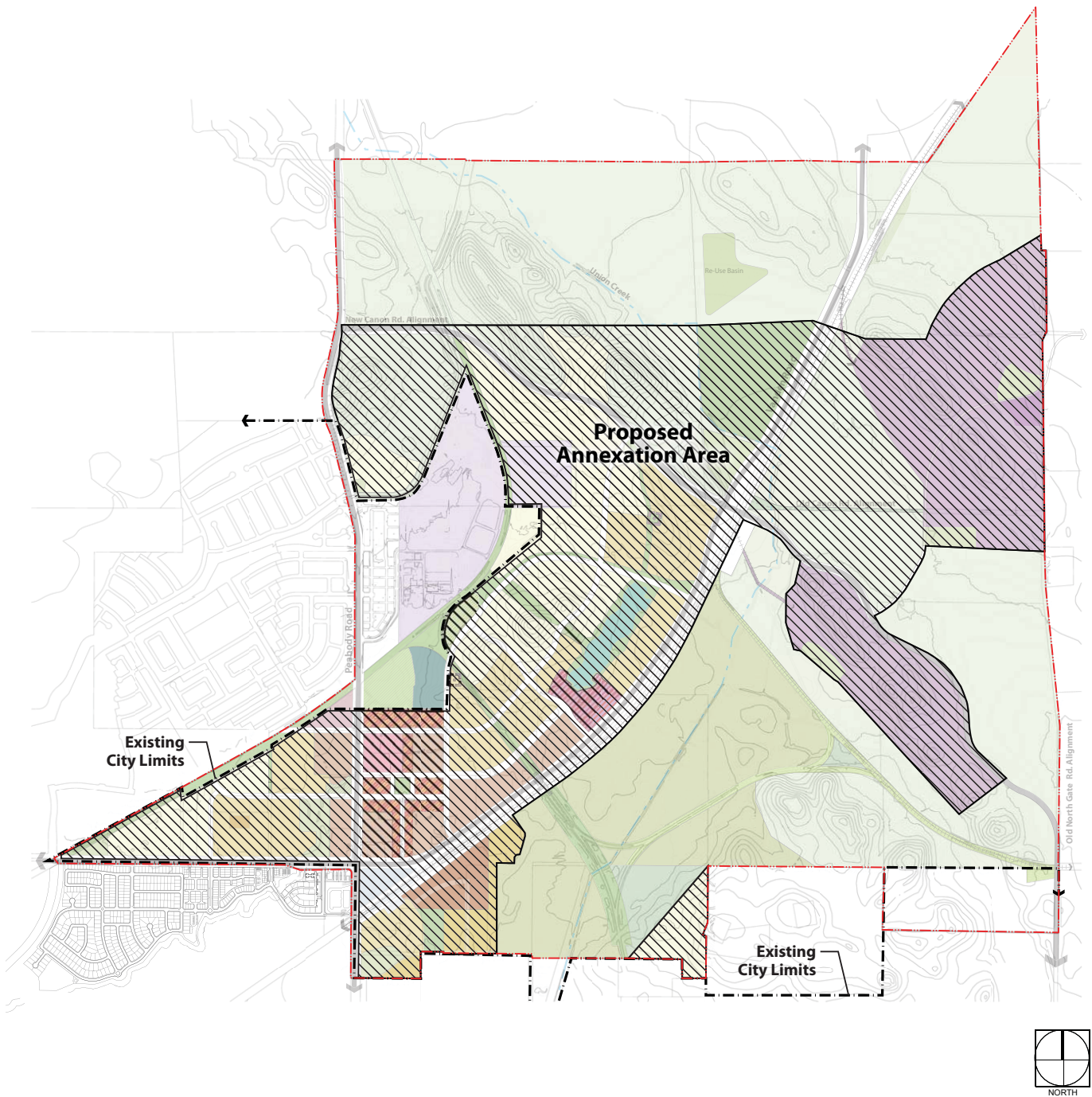


Figure 3-3: Proposed Annexation Boundary



adoption, brings the Project into compliance with all applicable goals and programs contained within the General Plan, and effectively implements the policies of the General Plan and the applicable zoning requirements and guidelines (General Plan Amendment text is included in Appendix B). A General Plan Consistency Analysis has been prepared and is included as Appendix A to this Specific Plan document. Figure 3-4 displays the City's existing General Plan Land Use Map.

3.4.2 City of Fairfield Zoning Code

The City of Fairfield Zoning Code is intended to provide the legislative framework enhancing and implementing the goals, policies, principles, and standards of the City of Fairfield General Plan. In conjunction with the adoption of this Specific Plan, a new Train Station Planned Development Overlay Zone District will be adopted which will include regulations for the Project development. Figure 3-5 displays the City's existing Zoning Map.

3.4.3 Train Station Development Plan

The Fairfield-Vacaville Train Station Project has been approved by the city of Fairfield and Solano Transportation Authority as part of Capitol Corridor system. Funding has been appropriated for \$40 million of improvements. The proposed improvements include a six-lane overcrossing of railroad, railroad track modification, and a platform for boarding. The Fairfield-Vacaville Train Station is part of the "Capitol Corridor," a 168-mile passenger train route that runs from the San Francisco Bay area to Sacramento and is operated by Amtrak.

3.4.4 Travis Airport Land Use Compatibility Plan

Fairfield Train Station Specific Plan (FTSSP) is located immediately adjacent to the Travis Air Force Base (Travis AFB). Development within the Travis AFB Area of Influence Boundaries directly impacts the operation of adjacent runways. Similarly operations at the base may have noise and safety impacts on development within these areas. The Travis AFB Land Use Compatibility Plan (Travis LUCP), as adopted in 2002 and administrated by the County Airport Land Use Commission (ALUC), regulates land use and development of compatible uses around Travis AFB to minimize public exposure to excessive noise and safety hazards of airport operations.

FTSSP land east of the railroad line, specifically PA3, PA6, PA8 and PA9, shares boundary lines with Travis AFB and is designated in the Travis LUCP as Compatibility Zone C. Travis LUCP addresses restrictions on non-residential development according to the number of people per acre and established noise sensitivity of different land uses and activities. Zone C prohibits children's schools, day care centers, libraries, hospitals, nursing homes, and hazards to flight (physical (e.g., tall objects), visual, and electronic forms of interference with the safety

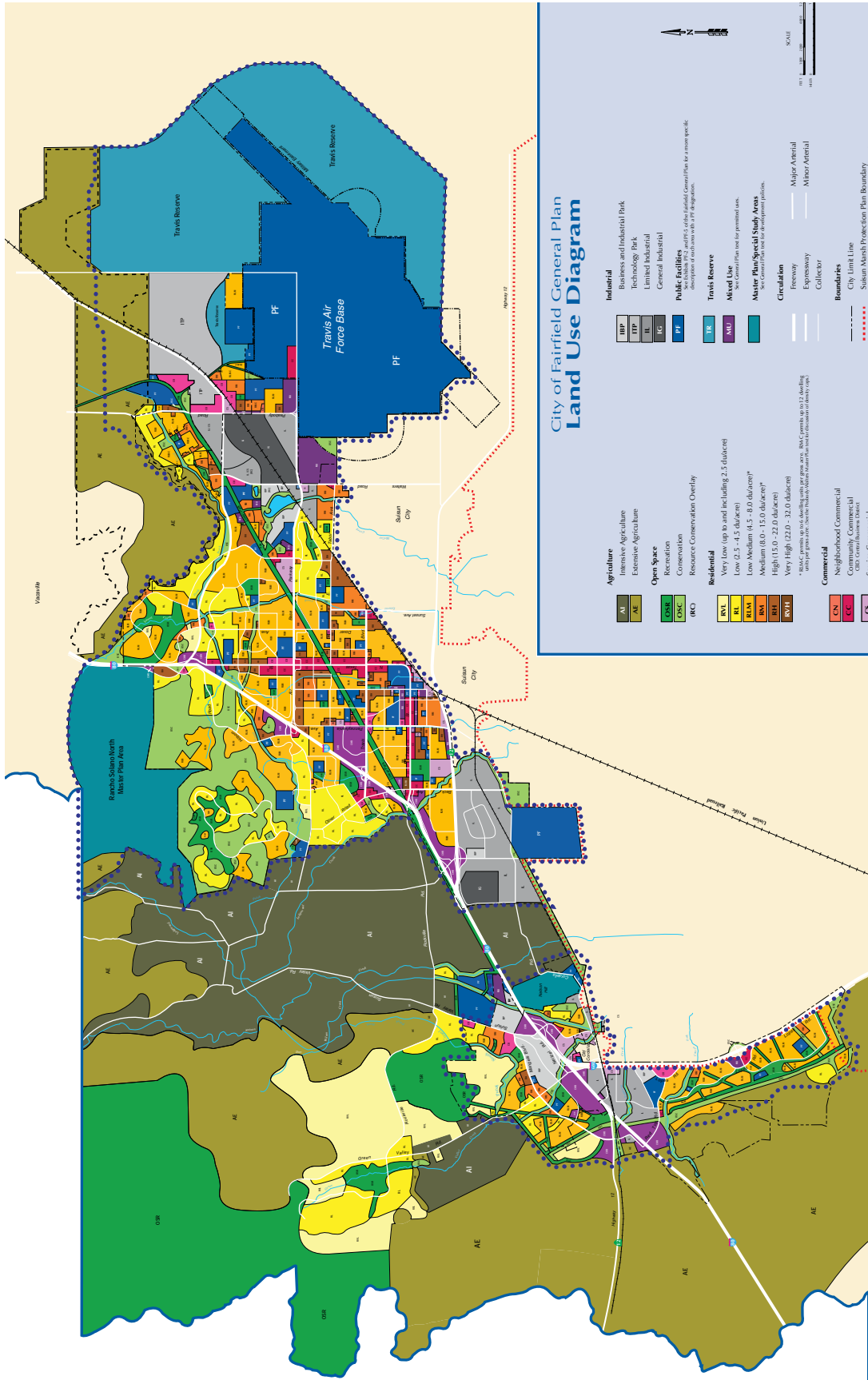


Figure 3-4: Fairfield General Plan Land Use Map

of aircraft operations.) Land use development that may increase bird attraction is also prohibited.

FTSSP land west of the rail line is designated Zone D by the Travis LUCP. The only applicable Travis LUCP standard is the prohibition of hazards to flight which requires limitation of the height of structures. See section 4.5.1 General Land Uses for policies associated with Travis LUCP compatibility.

Travis LUCP Compatibility

The FTSSP Land Use Plan, Figure 1-2, sufficiently addresses the policies and limitations of the Travis LUCP by:

- effectively reducing development intensity in Zone C
- focusing industrial development within Zone C
- limiting development height
- prohibiting reflective glass and other hazards to flight

Zone C

Residential development in Zone C is limited to the dwelling unit intensity dictated by zoning at the time the Travis LUCP was adopted. The large dedication of conservation/open space acreage of PA8 effectively reduces the number of dwelling units proposed from levels permitted in the City of Fairfield land use plan. Proposed MR and HR dwelling units in PA3 are within acceptable unit count limits.

Per the FTSSP Specific Plan, building heights within Zone C do not exceed 100 feet, thus will not cause hazards to flight. FTSSP building heights west of the railroad will be well within the Travis LUCP parameters, ranging from 45 feet for residential to 50 feet for non-residential/industrial buildings.

Zone D

Per Travis LUCP, Zone D west of the railroad only requires the limitation of buildings and structures to be less than 200 feet in height. Building and structure height shall not exceed 65 feet at any time within the FTSSP project boundary.

3.5 Discretionary Actions

Project implementation includes the following discretionary actions by the city of Fairfield and other public agencies.

3.5.1 City of Fairfield

- Approval of various amendments to the General Plan to ensure consistency between the General Plan and this Specific Plan
- Approval and adoption of the FTSSP





- Approval of a Development Agreement between the city of Fairfield and Canon Station LLC, as well as potential approval of other development agreements with other property owners within the FTSSP boundary
- Approval of Master PUD Permit, parcel, tentative, and final maps for portions of this Specific Plan that are designated for development
- Approval of grading permits, including provisions to allow for the export of soils from the FTSSP area
- Approval of design review for buildings and site improvements, consistent with the FTSSP
- Approval of the conveyance of approximately 98 acres of surplus lands owned by the city of Fairfield to Canon Station LLC, in exchange for a 204-acre property and other financial considerations
- Approval of conservation easements for areas designated as “Open Space” by the FTSSP

3.5.2 Solano Irrigation District

- Approval to relocate a Solano Irrigation District canal located in the northern industrial area
- Approval to use raw water from Solano Irrigation District for landscape irrigation as well as operation of Project lake(s)

3.5.3 Solano Local Agency Formation Commission

- Annexation to the City of Fairfield of the portion of the FTSSP area within its SOI that is planned for urban development
- De-annexation from the Vacaville Fire District
- Annexation to Suisun Fairfield Rockville Cemetery District
- Annexation to Vacaville-Elmira Cemetery District

3.5.4 Solano Transportation Authority

- Modify its design for Jepson Parkway (Vanden Road) and Cement Hill Road through the FTSSP area
- Provide for a grade-separated railroad overcrossing near the intersection of Vanden Road and “New Canon Road”
- Change the design standard in Jepson Parkway Concept Plan to “urban/residential parkway” for that portion between the Walters Road extension and Leisure Town Road that abuts urban development

3.5.5 Vacaville-Fairfield-Solano Greenbelt Authority

- Amendment of Greenbelt boundary to (1) remove areas designated for development by the FTSSP, and (2) add areas to the Greenbelt such that there is a net increase in the area of the Greenbelt. City of Fairfield has proposed an amendment to the Greenbelt boundary, consistent with Specific Plan, as shown on Figure 3-6.

3.5.6 Solano Airport Land Use Commission

- At their April 2011 meeting, the Solano Airport Land Use Commission determined that the FTSSP is consistent with the Travis AFB Land Use Compatibility Plan (Section 3.4.4 of this Chapter).

3.5.7 Fairfield-Suisun Sewer District

- Construction of a 21-inch sewer trunk line from Peabody Road/ Vanden Road intersection to vicinity of the Walters Road and E. Tabor Avenue intersection
- Possible construction of other trunk lines in accordance with Sewer District practices

3.5.8 Travis School District

- Elementary school construction and site acquisition

3.5.9 California Public Utilities Commission

- Approval of a railroad overcrossing for a new arterial street, to replace an existing at-grade crossing for Canon Road
- Approval of one or more rail spur lines to serve the proposed industrial area
- Approval of a railroad overcrossing for a pedestrian/bicycle bridge

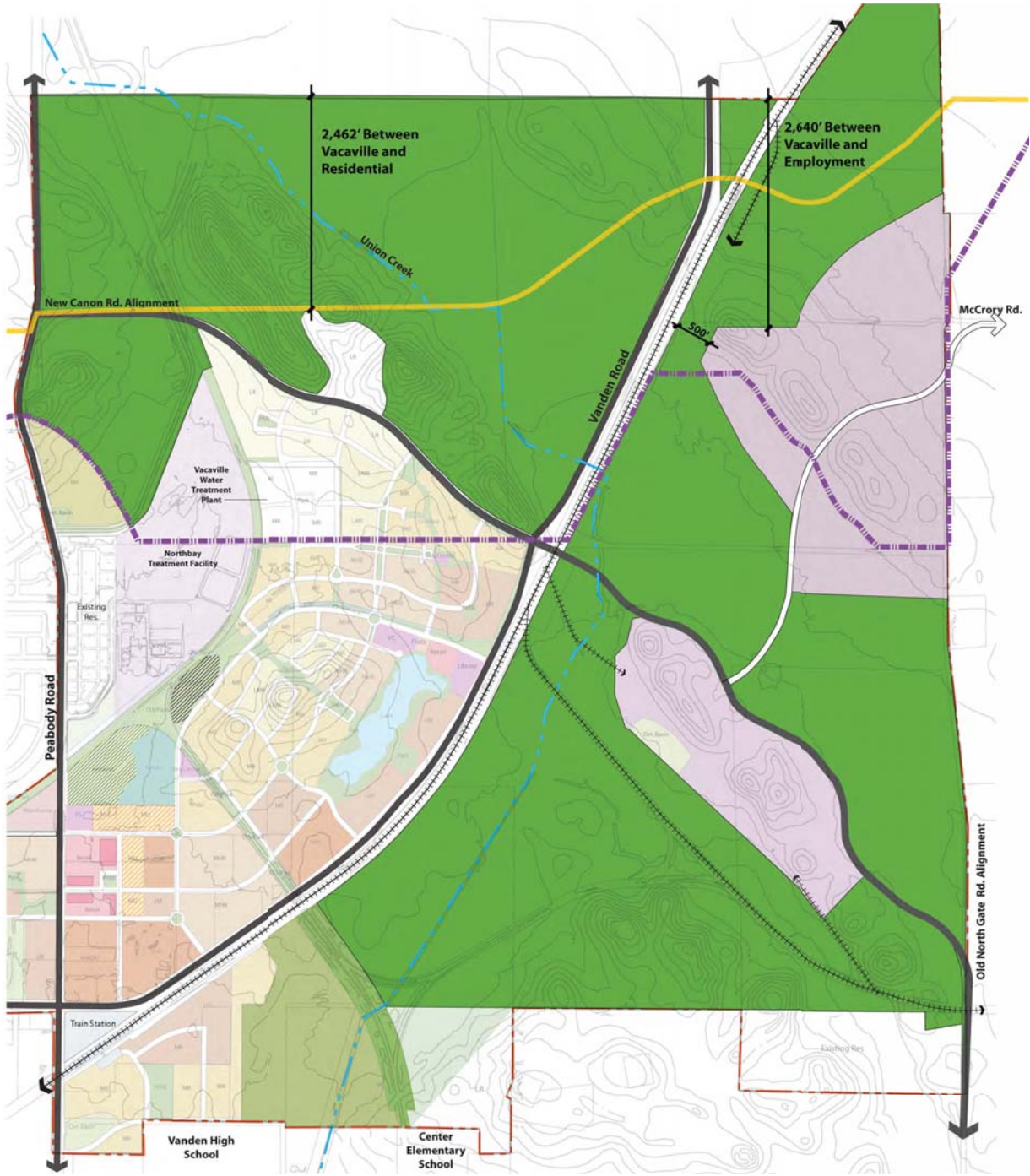
3.5.10 California Department of Water Resources

- Approval for road and utility crossings of the North Bay Aqueduct easement held by Department of Water Resources

3.5.11 Other State Agencies

- Approval of permits by state agencies such as Department of Fish and Game, State Regional Water Control Board, and Bay Area Air Quality Management District





Legend

- Adjusted Greenbelt (1,496 Acres)
- Greenbelt Boundary
- Sphere of Influence



Figure 3-6: Proposed Greenbelt Boundary Amendment

3.5.12 Federal Agencies

Approval of permits by federal agencies including the following:

- Incidental take permit for endangered species from U.S. Fish and Wildlife Service
- 404 Permit for fill of wetlands from U.S. Army Corps of Engineers
- Approval of bridge structure and utility easements over Putah South Canal by U.S. Bureau of Reclamation





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4.0 LAND USE & DEVELOPMENT REGULATIONS



4.0 LAND USE & DEVELOPMENT REGULATIONS

July 26, 2011

4.1 Overview

The Fairfield Train Station community is envisioned to be a multi-use, transit-oriented new community based on timeless urban design principles applied within the context of Fairfield's location and market setting.

The Guiding Principles outlined in Chapter 1.0 have led to a land use plan and urban design framework that provide a robust public realm, high level of internal connectivity, and diversity of land use. At the same time, the Specific Plan provides a level of flexibility to allow the community to evolve in step with inevitable changes and shifts in the marketplace and economic backdrop. It has been demonstrated since large-scale new communities were initiated in the 1960s that flexibility is essential to ultimate economic sustainability.

The FTSSP is proposed as a blend between a Transit-Oriented Development (higher densities and mixed-use) and a traditional neighborhood community (low- to mid density with a focus on schools and parks). As such, it has a wide range of housing types with a special effort to provide convenient walking, biking, and driving access between neighborhoods, the Town Center, and the Train Station. To reduce green house gas emissions and vehicle miles travelled, the land plan includes a variety of support land uses such as, commercial services, restaurants, child care, parks, schools, and other public facilities. The plan provides over 300 acres of land for employment uses to reduce the need for residents to drive long distances for work and to help balance the jobs/housing ratio citywide. An added benefit of the ecological resources found on the site is to allow a reconfiguration of the existing Greenbelt separating Vacaville from Fairfield. The reconfiguration of the Greenbelt, consistent with the Specific Plan, will not only increase its area but will provide the mechanism for permanent open space, directing urban development to appropriate locations. The following section describes the vision, land uses, and urban design components that make up the Specific Plan. In addition, the development standards that comprise the zoning for the FTSSP are included in this section.





Urban Design Goals:

1. Develop cores as the center of neighborhood activity
2. Graduate development in a density transect
3. Create a highly connective open space network
4. Incorporate plazas, squares and other people gathering places
5. Establish the Lake ParkTrail as a central open space network for FTSSP Community

4.2 Community Vision

The Fairfield Train Station Specific Plan represents a new era of transit-oriented design in Solano County. This Plan will create an aspirational community founded on principles of traditional neighborhood planning combined with access to transportation and the workplace that is unparalleled in Solano County. It provides diverse choices in housing, jobs, and recreation. It creates a place for families to live and work throughout all the stages of life. Its boulevards, pathways, and green spaces will weave together highly crafted neighborhoods, a vibrant Town Center, and new employment districts. It then adds 21st century concepts in sustainability and environmental responsibility including allowing residents direct access to the open spaces that surround them.

The preceding statement is a by-product of months of extensive discussions with the City and property owners with deep interest in creating an exciting and forward-leaning vision for this important growth area of Fairfield. The vision incorporates the Guiding Principles described in Chapter 1.0 and as such represents a thoughtfully ambitious building project, unique within the recent history of the City and County. The table below demonstrates how key elements of the vision have been attained within the proposed plan.

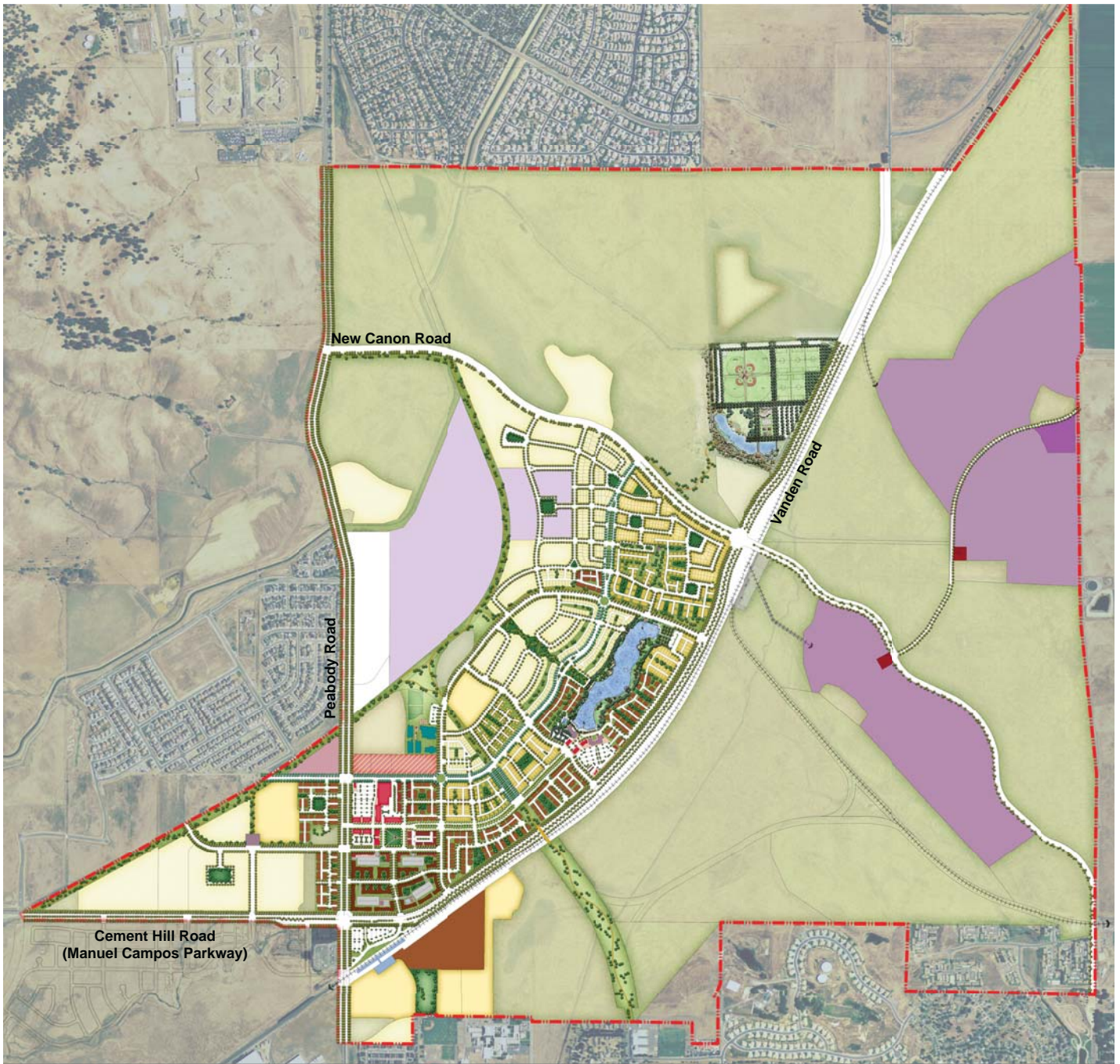
4.3 Urban Design Structure

Urban Design: The art of making places for people, including the connections between people and places, movement and urban form, the natural and built fabric, resulting in the building of successful settlements.

Although the arrangement of land use is important in delivering the Project vision, equally vital is the urban design structure of the community; that is, how the public spaces, thematic streets, views, footpaths, and greenways. It is this “public realm” that creates the primary experience of being in the community and determines whether perceptions and memories of it are positive and memorable or impersonal and easily forgotten.

The urban design structure also allows the residents to be less dependent on cars in meeting daily needs, because if a district is attractive and stimulating to walk through, the perception of distance is reduced and residents and workers are much more willing to walk to lunch, shops, work, and the train rather than drive short distances-trips that are fuel and emission intensive.

The urban design structure incorporated into the FTSSP is based on seven primary elements. If thoughtfully implemented, the result will be a visually rich, walkable, and uniquely organized living environment distinct from any other in the County. All development submittals or Master PUD Permits shall, in general, meet the intention of these urban design goals and shall fulfil the associated policies.



Disclaimer: This map is conceptual only and subject to substantial change without notice. No representations or warranties of any type are made by this plan.



Figure 4-1 Conceptual Illustrative Plan

4.3.1 Activity Cores

Activity cores of increased density and services allow more people to be close to the services and facilities they need on a daily basis. They also provide the opportunity for place-making elements such as urban parks, plazas and squares which frame and encourage community engagement. These cores will help create a background of human activity and energy within the community, especially in their key open spaces by intensifying land use with compact forms; providing convenient access and movement corridors; and surrounding public spaces with services, retail, and restaurants that people need and will visit frequently. The primary core and central gathering place for the community is the Town Center in PA2. This district has the quasi-urban density, mix of uses, and compactness appropriate for a transit-oriented living and working environment. The secondary community core is located at the south end of the lake in PA4 where a modest retail center, library plaza, and promenade create a service cluster for the central portion of the community. Activity core policies are addressed within the pertinent Planning Area policies. See Figure 4-2



4.3.2 The Density Transect

The graduation of density from higher in the Town Center to lowest along the community periphery is termed the “density transect.” Such a transect encourages taller buildings and more compact development forms closer to services and transit. Transit-oriented and reduced-carbon communities require such a transect to facilitate density and services from highest at the center to lowest at the edges. Therefore, the transect has become central feature for the FTSSP Land Use Plan. See Figure 4-3

Policy 4-1

Residential densities shall generally be of higher density closer to the core of PA2 and/or PA4.

Policy 4-2

Properties developed with more than one (1) residential density shall generally locate higher density units in closer proximity to the applicable PA core as determined by City through approval of an Master PUD Permit.

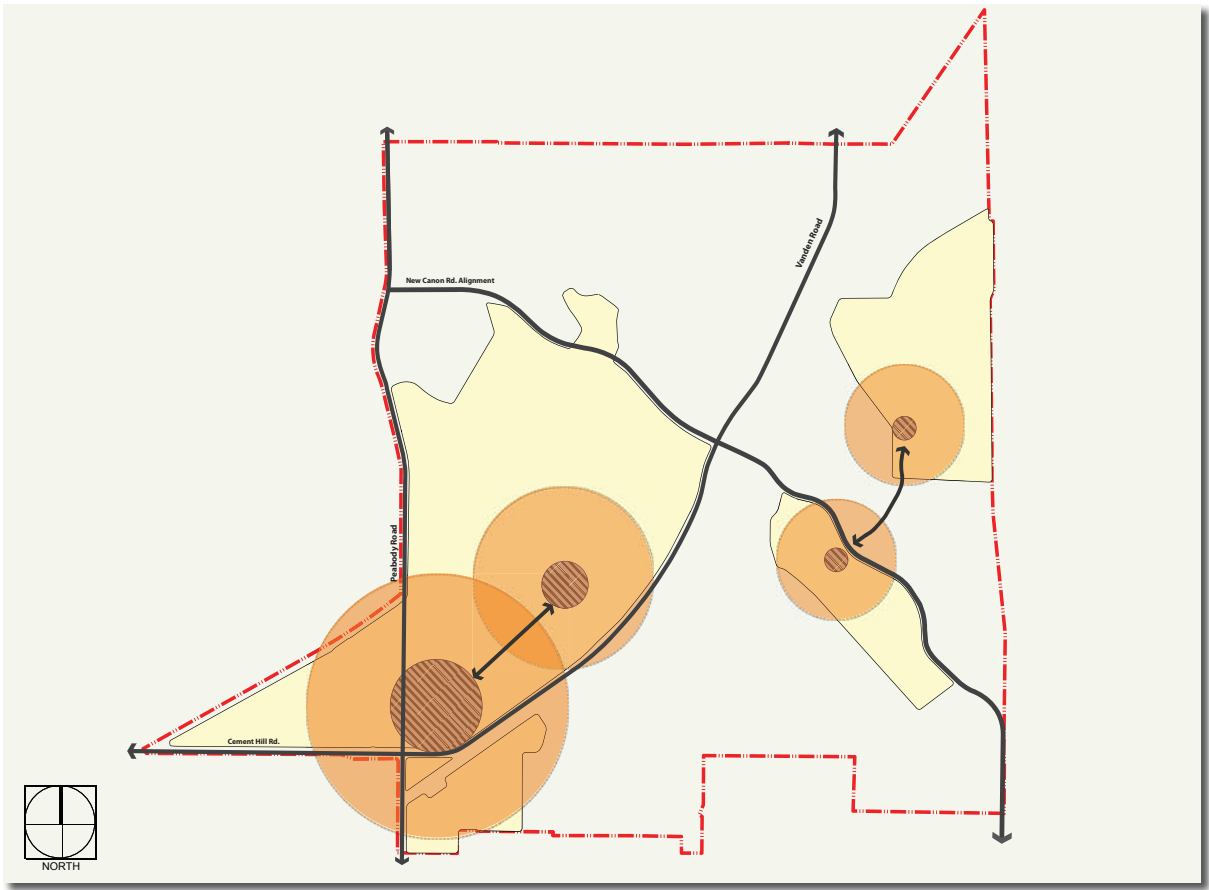


Figure 4-2 Activity cores anchor neighborhoods

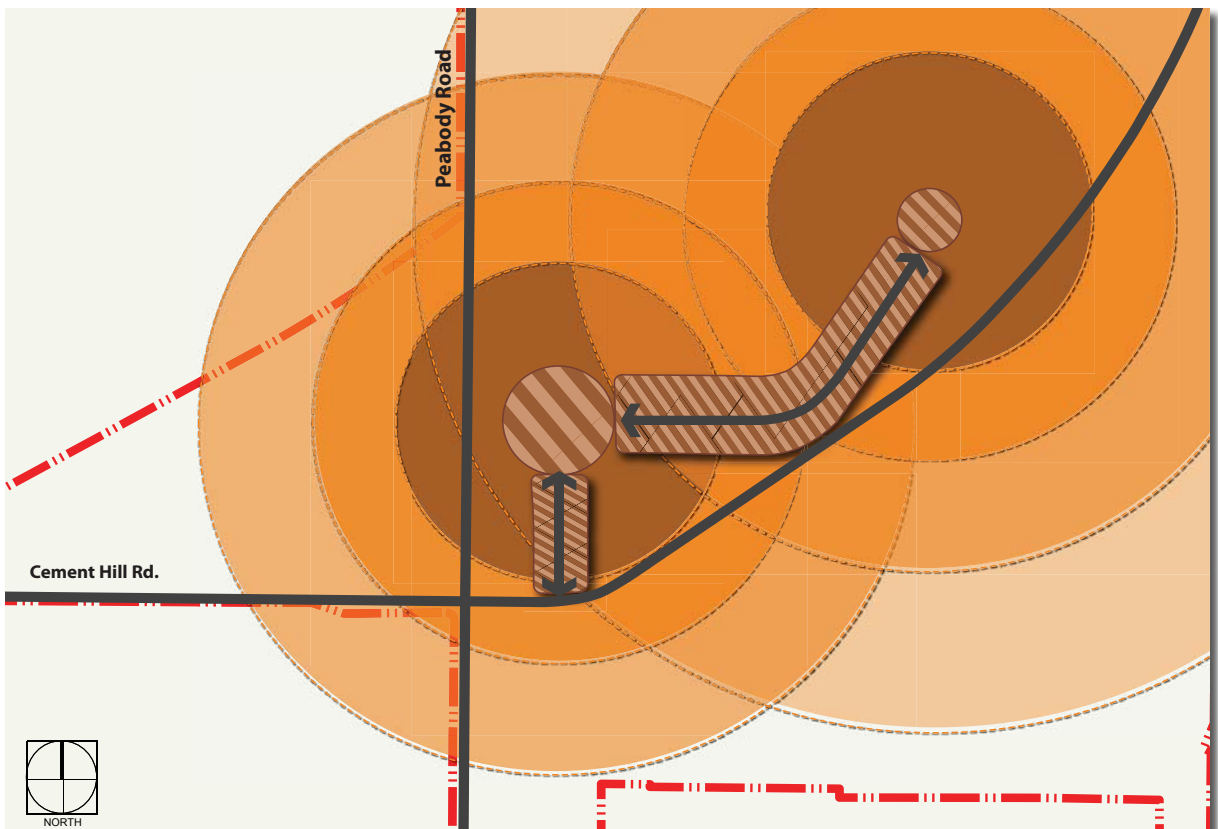


Figure 4-3 Civic Boulevard serving connects the cores of PA2 and PA4 to bridge the density transect

4.3.3 Open Space Network



A central component of the FTSSP urban design structure is a walkable open space network. The central spine of the open space network is the City's Linear Park Trail, a citywide pedestrian and bike facility. The open space network also traverses the project and connects all neighborhoods to key destinations, activities, and services. The network connects the Town Center Main Street and Town Square to the Lake Park via the Civic Boulevard, extending up to the Great Park and greater open space areas of the Greenbelt. This system provides an engaging sequence of experiences for the pedestrian or cyclist as they experience multiple spatial, landscape, land use, and recreational settings. The goal is to shift this corridor away from the conventional auto dominant environment to a clearly safe, attractive, and entertaining family-oriented open space system.

Open space linkages are proposed to provide walking and bicycle connections to the Linear Park Trail. In some cases they are thematic boulevards and in others auto-free greenways. Whatever the device, local streets and cul-de-sacs will link to the system with the intent that most homes in the community will not be more than a block or two from the open space network. See Chapter 9.0, Public Services, for additional information and standards.

Policy 4-3

Residential site plan design shall provide direct pedestrian connections to:

- The Linear Park Trail (Figure 9-2)
- This may include but is not limited to: open cul-de-sacs, pocket parks, and pedestrian pathways between homes
- Number and design of direct pedestrian connections shall be determined by City through approval of each Master PUD Permit.

Policy 4-4

The Linear Park Trail as defined by Figure 9-2 shall not be interrupted by streets, except those identified on Figure 10-1 and/or at the discretion of the Community Development Director.

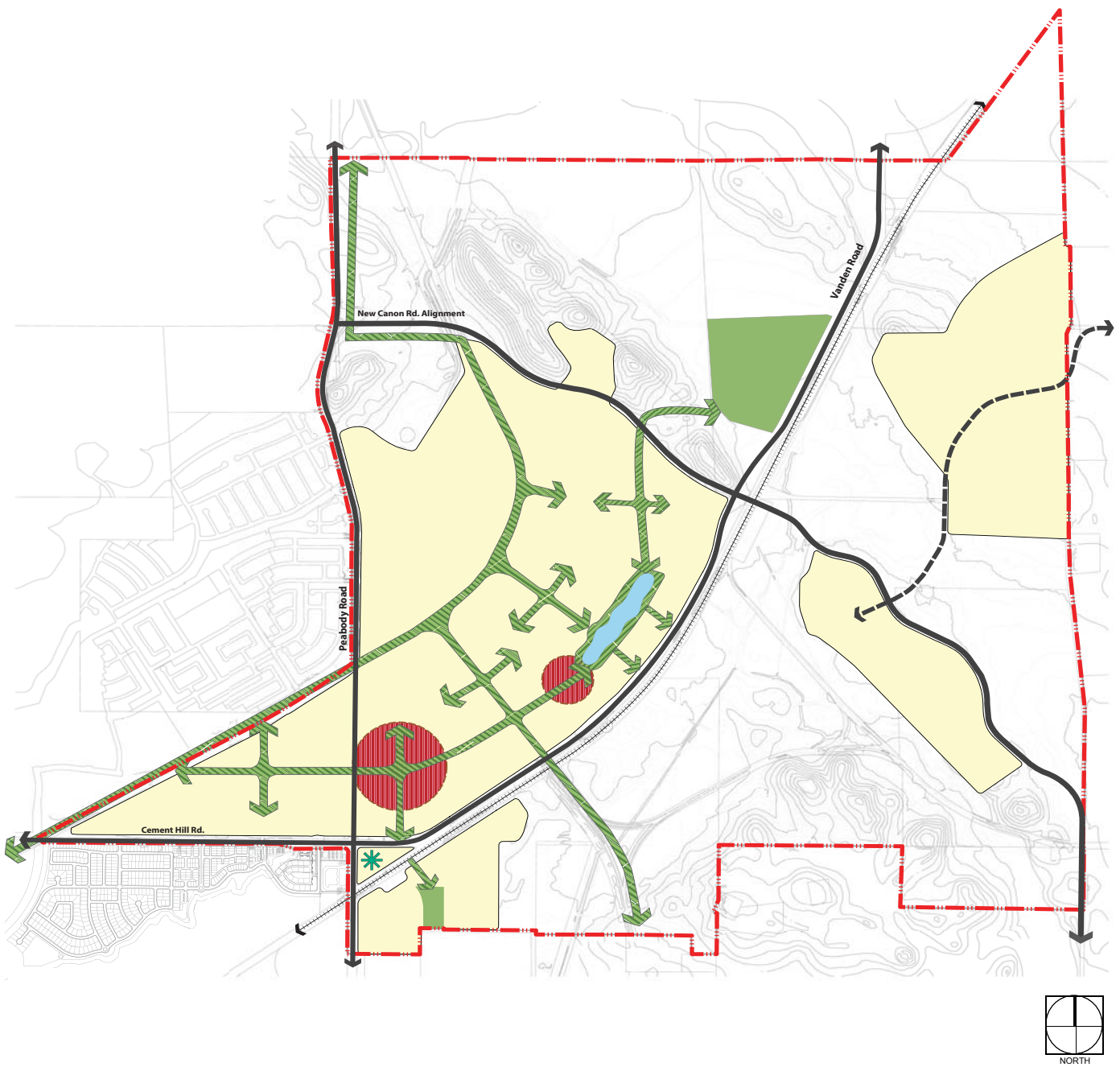


Figure 4-4 Open Space Goal Diagram



4.3.4 Thematic Streets

Specially designed Thematic Streets are proposed as key linkages between districts and destination. The Civic Boulevard links the Town Square to the Lake Park, both of these street section are considered Thematic Streets. Boulevards also link neighborhoods and provide place-making opportunities in the northern section of the plan. A Linear Park Trail street provides distinction in the old railroad right-of-way and a block of traditional Main Street helps make the Town Center a more walkable shopping and dining district. See Chapters 5.0 for Town Center Thematic Streets, Chapter 6.0 for Lake District Streets and Chapter 10.0, for more on other Thematic Streets.

Policy 4-5

FTSSP streets and Thematic Streets identified in Figure 10-1 shall be developed per the street section standards of Chapter 10.0, Circulation.

4.3.5 Intentional Place-Making

“Special places,” such as squares, plazas, promenades, and special parks, are proposed to provide pleasant and safe space to gather as a community or neighborhood. Most important locations are the Town Square and the South Lake Promenade, but a number of special small parks are also proposed to act as central gathering places for individual neighborhoods.



Figure 4-5: Thematic Streets include pedestrian amenities and enhanced landscaping features

To celebrate these special places and features, public spaces and community facilities will be sited in visually impactful locations; intentionally located for ease of identification and with the visibility they deserve as important components of civic life. Parks and open space policies applicable to Planning Areas and neighborhood planning are addressed in Chapter 9.0 Public Services. See section 9.6.4 Parks and Recreation Policies for further detail and specific policies.



Figure 4-6: Retail and mixed-use surround the Town Square as the primary gathering place of the Town Center

4.3.6 Lake Park

A “central park” helps establish a vibrant, interactive community by providing a place where residents comes together for major gatherings, concerts, festivals, or just for walking and people watching on a Sunday afternoon. For the FTSSP community, a central 22-acre Lake Park is located in PA4 and has an 11-acre lake as its core feature. Unlike most “residential lakes”, this has a park surrounding it and an uninterrupted mile-long walking trail. Articulated by seating areas, fishing outcroppings, trail heads, and landscape features, the park will serve as the community’s “outdoor living room.” Additional information, details and policies for the Lake Park are located in Chapter 6.0 of this Specific Plan. Development timing of the Lake Park is addressed in Chapter 9.0, Public Services.

4.3.7 Canon Station Relationship

A nonbinding Memorandum of Understanding (MOU) has been approved by the City with Canon Station LLC and Andrews Lando and Associates (Canon Station). Canon Station owns or has a controlling interest in approximately 1,570 acres within the Specific Plan. The MOU has been used to define the Project Description and will be used in the preparation of a draft Development Agreement that will be subsequently considered by the City Council.



4.4 Planning Areas



The regulating Land Use Plan is a by-product of an extensive collaborative effort between the City, the Canon Station LLC, and local property owners with an interest in the vision for the FTSSP Project. The Project is implemented through the adoption of the Land Use Plan, which is organized into 10 Planning Areas (Planning Area Map). Since flexibility is a key goal and being responsive to changes in market conditions is vital to the success of the Project, acreage adjustments among various Planning Areas are permitted. See Chapter 14.0, Implementation and Administration for density/unit transfer details. See Figure 4-7, Planning Area Map and Table 4-1, Land Use Summary.

4.4.1 Planning Area 1

Planning Area 1 (PA1) is a 144-acre triangular area located west of Peabody Road, northwest of the intersection of Peabody and Vanden Roads. PA1 is a district that transitions from higher density adjacent to the Town Center on the east to mid and lower density as distance from the Town Center increases. The immediate adjacency to the Town Center and Train Station provides this planning area with a high level of access to transit and community services.

Access to the Town Center will be by an enhanced walkway system and enlarged parkway along the north side of the east/west collector, which expands into a small park and wetland preservation area directly adjacent to Peabody Road, crosses at a signalized intersection and links directly to the Town Square in the core of the Town Center.

A segment of the citywide Linear Park Trail system runs the length of the north boundary allowing walking and bicycling access to the proposed elementary school and park in PA2. A centrally located north/south greenway trail links the center of the district to the Linear Park Trail.

Pocket parks are proposed within PA1 to provide residents convenient access to open space and informal recreation. It is the intent that these parks form the nucleus of individual neighborhoods creating a predictable, regular pattern of public amenities, within family-oriented and semi-urban neighborhoods. Additional open space is provided by a detention basin in the southeast portion of the planning area.

To provide the density of population to support the viability of the TOD, PA1 is planned to provide 1,050 dwelling units in a range of housing types, which may include, condominiums, townhomes, and small lot single-family homes.

PA1 Policies

The following policies shall apply to the development of PA1:

Policy 4-6

Existing mini-warehouse acreage within PA1 that is designated as “CC” by the Land Use Plan shall be considered a legal, conforming use for

Planning Areas

- PA1 - Residential
- PA2 - Town Center
- PA3 - Residential
- PA4 - Lake District
- PA5 - Residential
- PA6 - Industrial/Employment
- PA7 - Open Space / Recreation
- PA8 - Open Space / Conservation
- PA9 - Existing Residential
- PA10 - Not A Part

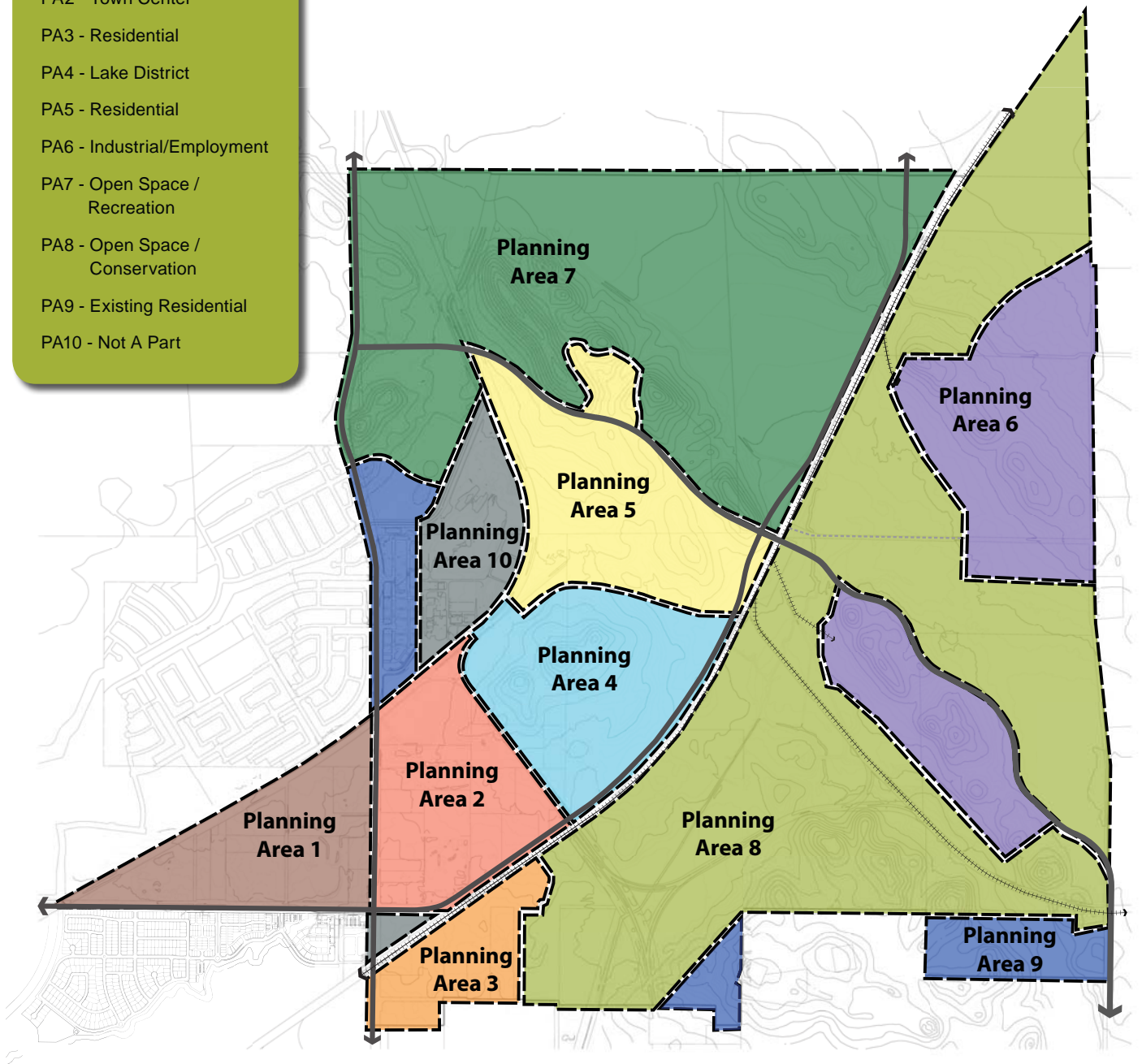


Figure 4-7: Planning Area Map

zoning purposes. Section 4.5.7 provides policies to govern the relocation of any existing, non-conforming uses within PA1 to PA6.

Policy 4-7

Through adoption of a Master PUD Permit, the City shall have the right to impose additional development standards outside the standards of this document on new residential development to ensure that it is compatible with existing, nonconforming uses. The scope of these standards will vary depending on the nature of the nonconforming use(s) and housing types proposed. The City may require the Master PUD Permit applicant(s) to submit supplemental studies regarding noise, air quality and hazardous materials for City to determine appropriate standards. The types of standards may include but are not limited to the following:

- i. Increased residential setbacks from nonconforming uses.
- ii. Construction of walls or fences along the property line shared by the existing nonconforming use and the proposed residential development.
- iii. Installation of landscape barrier (trees and shrubs) along the property line shared by the existing nonconforming use and the proposed residential development.



Figure 4-8: Planning Area 1 (PA1) Map

Land Use	PA1		PA2		PA3		PA4		PA5		PA6		PA7		PA8		PA9		PA10		Total	
	AC*	DU	AC*	DU	AC*	DU	AC*	DU	AC*	DU	AC*	DU	AC*	DU	AC*	DU	AC*	DU	AC*	DU	AC*	DU
Residential																						
High (Density 28.0)	14	392	39	1092	11	308	27	743	--	--	--	--	--	--	--	--	--	--	--	91	2535	
Medium (Density 14.0)	14	196	13	182	42	588	42	592	50	700										161	2258	
Low (Density 7.0)	66	462	8	56	4	28	47	329	91	637										231	1617	
Commercial Mixed Use Residential																						245
Residential on proposed Vacaville water treatment site									140												0	140
Residential on Burke property																					21	5
Subtotal	94	1050	60	1575	57	924	116	1664	141	1477	0	0	0	0	0	0	36	110		504	6800	
Existing/Proposed Residential outside Development Area																	83				83	-
Non-Residential	AC		AC		AC		AC		AC		AC		AC		AC		AC		AC		AC	
Community Commercial			17																			17
Mixed-Use Commercial			20																			20
Neighborhood Commercial							10															10
Limited Industrial/Employment											286											286
Existing Warehouse	6																					6
Subtotal	6	37	37				10				286											344
AC	AC		AC		AC		AC		AC		AC		AC		AC		AC		AC		AC	
Community Facilities (Note 1)/ Public Utilities/Institutional	1	11	11						21	5											84	122
Parks/ Recreation/ Passive Open Space/Conservation	24	30	30		8		44		12	15	15	646	908	2								1,690
Roads/Railroads	19	17	17		3		14		23	10	10	41	34	13							55	228
Train Station																					6	7
Planning Area Subtotal	144	155	155		69		184		196	316	316	687	942	134							145	
PROJECT TOTAL																						2,972
Note																						
* Acreage shown is Adjusted Gross Acreage (AGA) that includes the neighborhood parks and local street acreage rounded to the nearest whole number.																						
1 Includes Fire Training Center																						

Table 4-1: Land Use Summary



Policy 4-8

Specialized setbacks from existing and/or nonconforming uses shall be reviewed per the Nonconforming policies of this Chapter, Section 4.5.7.

Policy 4-9

Minimum setback from Cement Hill Road right-of-way for all buildings shall be 10 feet for High Density Residential areas and 20 feet for Low Density Residential areas. Multiple wall plane offsets, horizontal or vertical, shall be required. See Chapter 13.0, Design Guidelines for additional requirements. Parking shall not be permitted within this required setback.

Policy 4-10

Residential setbacks from powerlines shall respect the Residential Land Use policies of this document.

4.4.2 Planning Area 2

Planning Area 2 (PA2) is the Town Center, the heart of the community. With an area of 155 acres, it is envisioned to be the community's "downtown." Located adjacent to the Train Station, PA2 is planned as an active, walkable TOD with a mix of retail, commercial and residential uses, including up to 1,575 high, medium and low density dwelling units.

Community commercial and mixed-use commercial uses will frame a central Town Square designed and sized as a venue for concerts, festivals, fairs, and other community events. The two categories of commercial will provide many of the daily needs and services for the proposed project including groceries, food service, general services, office and financial services, entertainment and recreation, education and training, and medical.

A key component of the Town Center is a traditional Main Street that connects Peabody Road to the Town Square. The short two-story street segment, activated by small shops and restaurants with office uses above, will encourage strolling, sidewalk dining, and people watching. Also, as it is adjacent the Town Square, it will become a center of activity during evenings and on weekends. More detail on the urban design aspects of the Town Center can be found in Chapter 5.0, Town Center District.

The Town Center, PA2, includes education in the form of an elementary school, which has been located directly adjacent to a wetlands conservation area for potential use for interpretive purposes. Its location close to the City's Linear Park Trail system allows walking and bicycle access from other planning areas and the adjacent abandoned railroad right-of-way is planned for development as a community park allowing spillover for expanded recreation and sports activities. For further detail and policies for PA2 Town Center District, see Chapter 5.0, Town Center District

4.4.3 Planning Area 3

Planning Area 3 (PA3) is a 69-acre area located to the south of the railroad right-of-way, adjacent to PA8, and to the south of the Train Station. PA3 will include a total of 924 dwelling units of medium- and high-density residential consistent with a TOD concept. Adjacency to the Train Station and easy pedestrian access to the Town Center District via a railroad under-crossing strengthens the energetic TOD design of this area. A small 4-acre area is designated for low-density residential with a total of 28 dwelling units. A “Neighborhood Park” meeting the City requirements, of approximately 5-acre, will serve as a community hub between the three residential neighborhoods; see Chapter 9.0, Public Services for further detail. The identity of this area is further supported by its close proximity to the conservation/recreation in PA8 and Vanden High School to the south. A detention basin shall be an included feature of PA3.

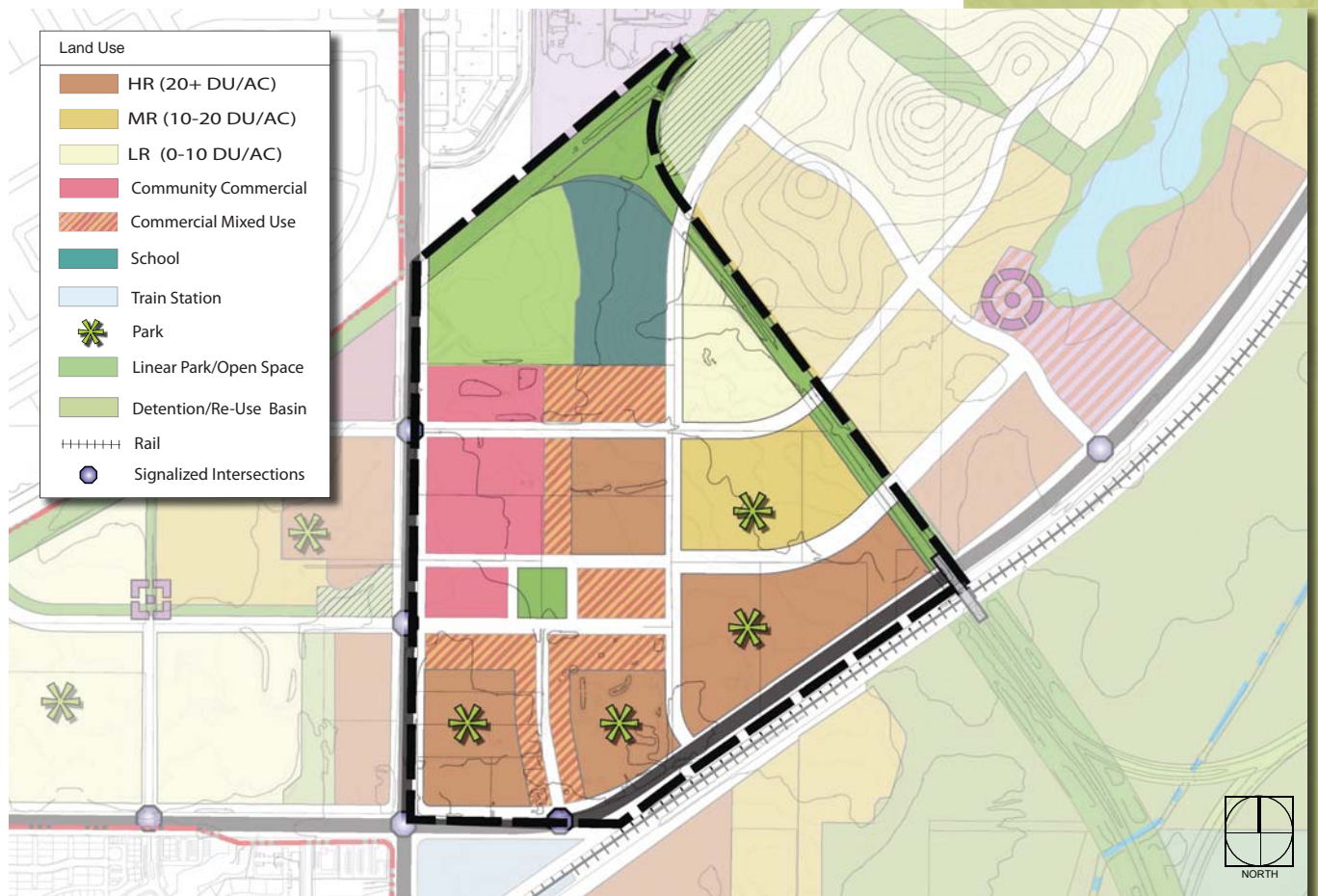


Figure 4-9: Planning Area 2 (PA2) Map



PA3 Policies

The following policies shall apply to the development of PA3:

Policy 4-11

The Master PUD Permit shall include a train station drop-off area. The dimensions, design, and specifications shall be included in the Master PUD Permit for PA3.

Policy 4-12

A pedestrian railroad under-crossing shall be provided for access between PA3 and the north side of Vanden Road. Placement and development timing shall be determined at time of Master PUD Permit submittal.

Policy 4-13

Pedestrian connections between residential neighborhoods and the neighborhood park shall be provided. Placement and development timing shall be determined at time of Master PUD Permit submittal. See Chapter 9.0, Public Services for further detail and policies.

4.4.4 Planning Area 4

Planning Area 4 (PA4) is the Lake District and includes a total area of 184 acres. The Lake District includes the three residential densities of low, medium, and high for a total of 1,664 dwelling units, an approximately 22-acre Lake Park, and neighborhood commercial, as well as Linear Park Trail and open space.



Figure 4-10: Planning Area 3 (PA3) Map

The Lake District is planned to be a major recreational amenity not only for the FTSSP but for the residents of the City at large. An approximately 22-acre Lake Park, the centerpiece of the district, will offer a setting for trails, picnicking, fishing, boating, limited shopping, and other day use recreational activities. A continuous greenway of open space will surround the lake allowing residents to walk entirely around its periphery on a trail punctuated by seating areas, small plazas, rock outcroppings, meadows, and tree bosques, spaced so that a special landscape or hardscape feature is encountered on average every 300 feet of walking distance.

The South Lake Activity Core is a multi-use complex of locally scaled neighborhood commercial, restaurant, private recreation center, and public library. This Activity Core abuts but is not part of the 22-acre Lake Park. The mix of uses ensures that the core area will be a center of local activity and a key people gathering place for the community. The location at the southern edge of the lake allows visitors to enjoy the views of the lake while sitting in an outdoor cafe, visiting local shops, or spending time outdoors at the recreation center. A lake front plaza, at the center of the core area, shall be sized for small events, concerts, or simple people watching while enjoying a coffee or sandwich. A formal pedestrian promenade will link the community recreation center, plaza, and retail uses to nearby residential neighborhoods. Residents in the district will have the ability to live within view of the lake and walk to shops, having the opportunity to lead a more active, pedestrian-oriented lifestyle. To the extent practical, offices and small businesses may be incorporated into the district to provide services to the residents; however, the final mix of tenants and facilities will be a function of market opportunity and timing as the community develops.

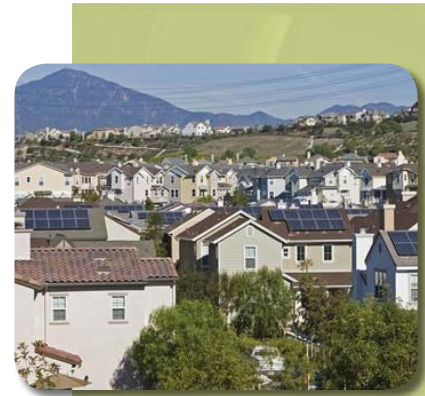


Figure 4-11: Planning Area 4 (PA4) Map



The urban design concept for the Lake District will incorporate a number of time-tested town building principles to ensure that human scale, connectivity, and quality place-making are achieved. Two thematic streets are envisioned to provide an enhanced walking environment and connectivity on a north/south axis. The first street is the Civic Boulevard linking the South Lake activity core to the Town Center and the second is the connector street running parallel and westerly of the lake.

Connections to the neighborhoods west of the Lake Park are provided by a Linear Park Trail and greenway that traverse up the hill and link to the citywide Linear Park Trail system. This open space element extends the amenity and premium value of the lake deeper into the community and allows a large number of residents to feel a part of the lake environment. For land use and development policies for PA4 Lake District, see Chapter 6.0 of this document.

4.4.5 Planning Area 5

Planning Area 5 (PA5) is a 196-acre primarily residential planning area that consists of low- and medium-density residential for a total of 1,477 dwelling units. PA5 is adjacent and to the north of the Lake District and in the southwest corner of New Canon Road Alignment and Vanden Road.

Due to proximity to the Lake District, PA5 will enjoy the amenities of the Lake in PA4. Higher density residential will be located closer to Vanden Road with single-family homes on larger lots located west of the major arterial that bisects PA5.

PA5 lies adjacent to the Vacaville-Fairfield-Solano Greenbelt that will separate development in this project with development in the City of Vacaville; thus the residential densities in this planning area are the lowest. This distribution of densities is consistent with the density transect described earlier which locates the highest densities around the Town Center with a gradual lowering of densities toward the project edges. PA5 is located adjacent to the Great Park-a 50-acre community park that provides a mix of active and passive recreational elements. A trail has been proposed to provide a walking and biking connection from residential neighborhoods within the planning area to the Great Park allowing a complete Town Center to Great Park walking system.

An internal North Greenway element follows the existing drainage corridor between neighborhoods on a north/south axis. The trail within this corridor will allow homes to have convenient walking and bicycle linkage to the Great Park to the north and the Lake Park to the south. A private recreation center is also suggested central to the eastern half of the planning area to provide swimming, special event, and play facilities for the adjacent mid-density neighborhoods.

PA5 Policies

The following policies shall apply to the development of PA5:

Policy 4-14

The following policies apply to residential development in the vicinity of and within the 20-acre potential water treatment plant site, owned by the City of Vacaville:

- i. Master PUD Permit shall include vehicular access to the potential water treatment plant site, owned by the City of Vacaville.
- ii. Through the approval of a Minor Specific Plan amendment filed by the City of Vacaville, the City of Fairfield may:
 - approve an alternate location for the water treatment plant site within the boundary of the Specific Plan, and
 - allow residential development of the existing Vacaville water treatment plant site in accordance with Residential Low LR regulations, with a maximum of 140 dwelling units.
- iii. If no Minor Specific Plan amendment is filed by the City of Vacaville prior to submittal of the Master PUD Permit application for PA5, the Master PUD Permit shall assume the Vacaville water treatment plant site will be developed as a water treatment plant. Through approval of Master PUD Permit, the City of Fairfield shall determine standards for adequate separation and buffering between the residential and water treatment plant site.

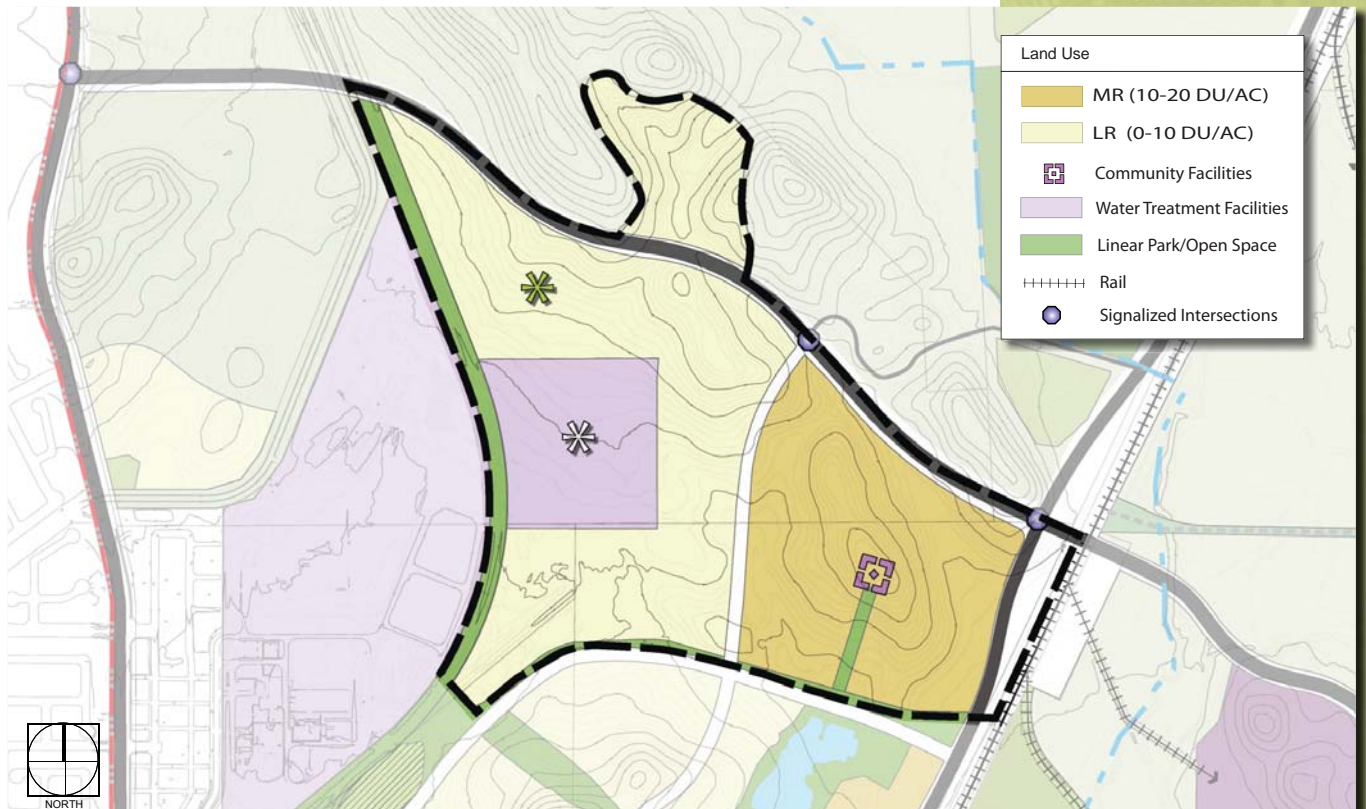


Figure 4-12: Planning Area 5 (PA5) Map



Policy 4-15

Development of the LR area north of New Canon Road shall:

- Not exceed a maximum of 4 dwelling units per acre
- Include measures to reduce the risk of wildlands fires, which may include fire breaks, noncombustible fencing and irrigated landscape perimeter
- Design and construct grading to visually screen this area from views from Peabody Road and Vanden Road
- Target Density remains at 7 dwelling units per acre. Property owner may transfer the density between the Target Density and actual density to another Planning Area, in accordance with the provisions of Chapter 14.0.

Policy 4-16

New Canon Road shall be constructed from Peabody Road to Vanden Road by landowner of PA5 prior to the issuance of any residential permits within PA5.

Policy 4-17

All improvements to New Canon Road, as described in Chapter 10.0, shall be constructed from Vanden Road to North Gate Road prior to issuance of a building permit for the 1,750th residential unit within PA4 and PA5 combined.

Policy 4-18

The landowners in PA 4 and PA5 shall be responsible for the construction of a minimum of 16 acres of the Great Park in PA 7. That park construction shall commence prior to the issuance of a building permit for the 1,750th residential unit within PA4 and PA5 combined and shall be completed within 24 months. The remainder of the Great Park shall be constructed by City or by developers if required by City through conditions of approval of subsequent development entitlements.

4.4.6 Planning Area 6

Planning Area 6 (PA6) is the employment hub for the FTSSP. Strategically located in the eastern portion of the site as a transitional land use between residential neighborhoods and the overflight zones from the air base, the approximate 300-acre PA6 is divided into two land areas which are separated by conservation wetland and the New Canon Road Alignment. The northern portion is located along Old North Gate Road Alignment and the southern portion is along the New Canon Road Alignment. Both parcels will be accessible and enjoy visibility from New Canon Road, which links Vanden Road to the north gate of Travis AFB. Both sections of the industrial park have the potential for access to the adjacent rail lines if rail spur(s) extensions are pursued.

PA6 will include the IL land use designation of the Train Station Specific Plan PD Overlay District. This designation allows light industrial, certain heavy industrial uses, business park, research and development (R&D), manufacturing, and service uses. It also allows the relocation of existing industrial and service uses from PA1 and PA2 in accordance with Policies 4-107 through 4-109. PA6 is envisioned to include a rich mix of employers, including incubator new business, multi-tenant commercial, light and heavy industrial, and service retail. With its high visibility from Vanden Road and location between Fairfield, Vacaville, and Travis AFB, it has the potential to attract high-end corporate headquarters and Class A office as well as industrial and manufacturing uses. Sites along the western edge of the planning area should be attractive for business park and campus-type uses that integrate office, research, and manufacturing into corporate headquarters-type complexes. Service uses such as banks and restaurants in PA2 will benefit from proximity to such employment uses.

Each industrial planning area will include a small local service area consisting of retail, professional office, food service, fitness and/or medical uses. These service cores allow employees to meet ancillary daily needs with driving inconvenient distances and also assist in reducing the carbon footprint of the district.

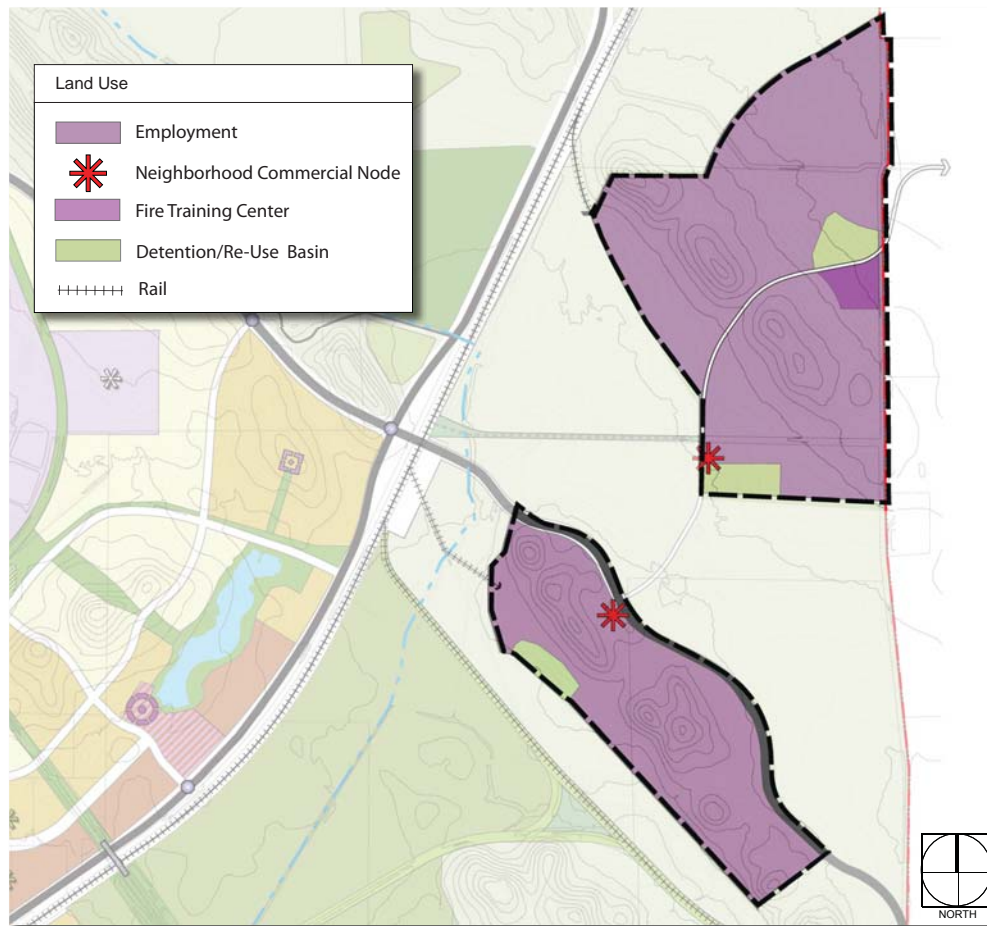


Figure 4-13: Planning Area 6 (PA6) Map



Architecture will be approached with special consideration for buildings and landscape visible from Vanden Road and New Canon Road. Additionally, adjacency to habitat conservation and mitigation bank areas of the site necessitates special treatment in site planning concerning edge conditions at sensitive habitat areas. To achieve this, the industrial edge landscape design shall:

- Integrate the character of conserved adjacent grasslands by using drought-tolerant tall grasses
- Incorporate agricultural patterns such as furrows, formal lines of trees, and a simple plant palette
- Promote tree planting to emulate orchards
- Protect surrounding land uses by storm water detention
- Screen unsightly areas through landscaping, berms and walls

See Figures 12-1 and 12-2 for a diagram of specific condition requirements.

PA6 Policies

The following policies shall apply to the development of PA6:

Policy 4-19

PA6 shall be developed per the IL regulations of the Train Station Specific Plan PD Overlay District.

Policy 4-20

At the discretion of the Community Development Director, up to 25 percent of PA6 (25 percent of each of the portions-northern and southern) shall be permitted to be developed per the IG zoning district of Tables 25-11 and 25-12 for the provision and development of heavy industrial park uses. These uses should be located away from Vanden Road visibility.

Policy 4-21

In conjunction with the construction of New Cannon Road through PA6, as described in Policy 4-17, a minimum of 50 acres of industrial land shall be developed in PA6. The term “developed” shall mean:

- A final map(s) has been prepared and approved for recording, which will create one or more industrial parcels;
- Street improvements have been constructed;
- Rough grading has been completed; and
- Sewer, water and drainage improvements have been constructed to serve the parcels created by the final map(s).

Policy 4-22

The Master PUD Permit for PA6 shall be submitted concurrently with the Master PUD Permit for PA5 in order to implement Policy 4-21.

Policy 4-23

One or more railroad spurs shall be constructed by the landowners to serve industrial development in PA6. The timing, location and extent of the railroad spur(s) shall be determined by City through approval of the Master PUD Permit for PA6.

Policy 4-24

Parcels within PA6 will be appropriate for relocation of existing industrial and commercial services uses currently located within PA1 and PA2, as allowed by the policies in Section 4.5.7.

Policy 4-25

Architectural, landscape and screening of edge conditions identified in Figure 12-1 in Chapter 12.0, Landscape, of this document shall comply with the standards of Figures 12-2 through 12-4 of this document.

Policy 4-26

The industrial properties which abut habitat conservation areas, as shown on Figure 12-1 shall:

- Maintain a minimum 20-foot setback to any building; streets, or landscape elements permitted within this setback.
- Locate loading, waste, heavy equipment and semi-parking away from these edges.
- Utilize a minimum of 1 edge condition consistent with the policies and standards of section 12.4 of this document; see Figures 12-2 through 12-4.
- Be graded to convey storm water drainage from industrial areas away from habitat areas.

Policy 4-27

The industrial properties with high visibility from Vanden Road, as shown in Figure 12-1, shall:

- Utilize a minimum of 2 edge conditions consistent with the policies and standards of section 12.4 of this document; see Figures 12-2 through 12-4.
- A maximum of 30 percent of the length of the edge shall utilize the Berm Industrial Edge Condition, Figure 12-3; the remainder of the length of the edge condition shall utilize at least 1 of the other edge conditions, found in Figures 12-2 or 12-4.
- Alternative edge conditions consistent with the intent of reducing the visual impact of industrial development as viewed from Vanden Road and Vacaville shall be permitted at the Community Development Director's discretion.





Policy 4-28

Exterior lighting in the industrial area shall be located and designed to minimize glare impacts on adjacent residential uses within the Specific Plan, conserved open space areas, Travis AFB, and the City of Vacaville.

Modified Land Use Regulations:

Policy 4-29

In addition to the land uses established by Table 25-11 IL zoning district of the Zoning Code, the following uses shall be permitted:

- Public/quasi-public buildings, facilities, and accessory; permitted
- On-site recreation facilities for on-site uses; permitted

Policy 4-30

All development within PA6 shall meet these minimum requirements:

- 30 feet minimum setback from New Canon Road
- 25 feet minimum setback from McCrory Road
- Loading area and rear elevations of buildings/developments shall not be oriented toward primary circulation corridors, New Canon Road or McCrory Road.

Policy 4-31

A sub core service area shall be incorporated into each of the two districts in the approximate location identified on the land use plan. These shall include pads or commercial space for retail, office, restaurant and/or other uses appropriate to meet the daily services needs of the working population. Approximately 12,000 square feet of gross floor area may be provided in each area.

4.4.7 Planning Area 7

Planning Area 7 (PA7) is a 687-acre open space buffer and recreation area located in the northern portion of the FTSSP, west of Vanden Road and east of Peabody Road. It includes the citywide Linear Park Trail, the proposed Great Park, and a potential water storage reuse basin. Union Creek traverses this planning area in a northwest-southeast direction.

This will serve as a major open space resource for Fairfield residents as well as residents of Vacaville and the remainder of the County. It will provide both passive and active recreation opportunities.

The 50-acre Great Park is an active recreation community park designed to serve the large-scale sports field needs of the FTSSP and the City of Fairfield. The Great Park will include sport fields, a skateboard node, sports courts, picnic areas, lake and other recreational activities.

A key goal is to increase the utilization of the Fairfield-Vacaville Greenbelt by adding a recreational dimension to its existing function as open space preservation and separator between the two cities. The reconfiguration of the Greenbelt in this area also allows its other portions east of Vanden Road to be more protective of natural habitat and biotic resources as well as to ultimately be enlarged.

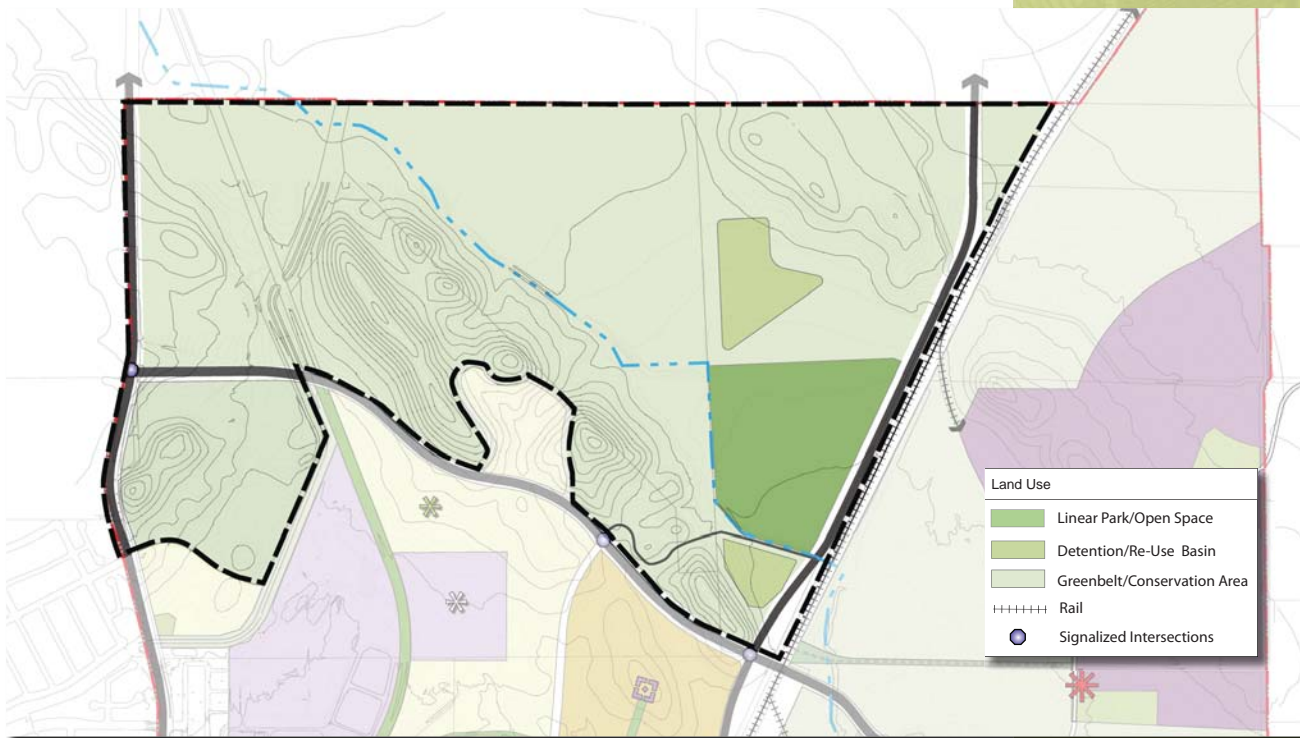


Figure 4-14: Planning Area 7 (PA7) Map



PA7 Policies

The following policies shall apply to the development of PA7:

Policy 4-32

The sports lighting in the Great Park shall be designed to minimize glare impacts on existing and future residents in Fairfield and Vacaville. Such measures shall include the following:

- Light fixtures shall be installed that have light sources aimed downward and shielded to prevent glare or reflection or any nuisance, inconvenience, and hazardous interference of any kind on adjoining streets or property.
- Exterior building materials on nonresidential structures shall be composed of a minimum 50% low-reflectance, non-polished finishes.
- Bare metallic surfaces (e.g., pipes, vents, light fixtures) shall be painted to minimize reflectance.

Policy 4-33

Land within PA7 shall not be subject to payment of taxes established by City through a Community Facilities District, assessment district, or similar financing district.

Policy 4-34

The Master PUD Permit for PA 7 shall be submitted by the landowners in PA 5, concurrently with the Master PUD Permit for PA 5. PA 7 Master PUD Permit shall include the Conceptual Plan for the Great Park and any trails or open space improvements in the remainder of PA 7.

Policy 4-35

City will provide a conservation easement or deed restrictions on land it owns or will own in PA 7. City will seek to acquire conservation easements on the remaining land within PA 7 designated for open space uses, as opportunities arise.



4.4.8 Planning Area 8

Located in the eastern half of the FTSSP site, Planning Area 8 (PA8) is 942 acres in size and is bordered by the railroad ROW on the northwest and North Gate Road on the east. This planning area includes habitat conservation and a mitigation bank and open space trails for long-term land conservation. The proposed modification of the boundaries of the existing Fairfield-Vacaville-Solano Greenbelt in this area allows a more precise determination of the boundaries and will provide permanent open space through the establishment of conservation easements.

PA8 Policies

The following policies shall apply to the development of PA8:

Policy 4-36

The southeastern portion of Linear Park Trail along the abandoned railroad right-of-way through PA8 shall be designed to minimize access into the abutting mitigation bank and habitat conservation area through fencing.

Policy 4-37

Land within PA8 shall not be subject to payment of taxes established by City through a Community Facilities District, assessment district, or similar financing district.



Figure 4-15: Planning Area 8 (PA8) Map

4.4.9 Planning Area 9

Planning Area 9 (PA9) denotes various, non-contiguous residential areas that include: (a) existing residential development, (b) residential development that received development entitlements from City prior to approval of Specific Plan, and (c) new residential development that would be allowed under the authority of the Specific Plan. There are approximately 500 residential units within the boundary of FTSSP that either exist or which have received development entitlements from the City.

By being incorporated into the Specific Plan, these existing or approved residential areas will enjoy access to a much enhanced park and services network as well as the Lake Park, Great Park, and activities and services of the Town Center. Approximately 110 new residential units are authorized by the Specific Plan in PA9, exclusive of those units which have already received development entitlements from the City.

PA9 is divided into 4 sub-areas, as described below.

PA9 Policies

The following policies shall apply to the development of PA9:

Policy 4-38

Except for PA9-D, new or existing residential units within PA9 shall not be eligible for density transfer via the “Dwelling Unit Allocation” of this document.

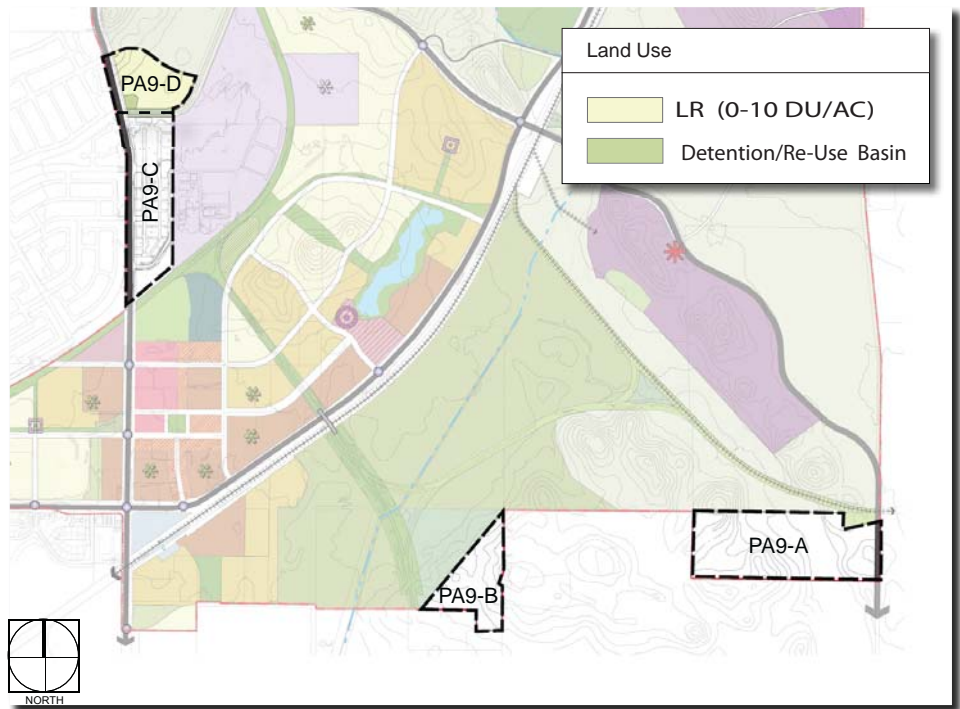


Figure 4-16: Planning Area 9 (PA9) Map

Planning Area 9-A: Patriot Village.

Policy 4-39

PA9-A was privately developed to provide approximately 300 dwelling units for active military personnel, under a long-term lease agreement with the U.S. Department of Defense. That lease will expire and this housing is vacant at the date of adoption of the Specific Plan. Development and uses within PA9-A are subject to the RLM zone district of the City's Zoning Code. Any redevelopment of the site or change in use from current military housing use shall require a Specific Plan amendment. Public water and sewer utilities are provided by Travis AFB at the date of adoption of the Specific Plan. If the property owner of PA9-A wishes to receive water and sewer services from the City of Fairfield, the property owner shall be responsible to:

- Construct water and sewer utilities to the site from the nearest point of connection to the west; location and design of such utilities shall be subject to City approval
- Annex to any Community Facilities District(s) formed by property owners within the Specific Plan area, as described in Chapter 14.0, Implementation and Administration, for both the financing of infrastructure and cost of municipal services
- Pay Northeast Fees that are in effect at the time of utility connection, regardless of whether any buildings permits are issued.
- Pay all connection fees

Planning Area 9-B: Burke Property.

Policy 4-40

PA9-B may be developed with a maximum of five single-family detached homes on minimum 1-acre lots.

The Burke property owner shall be responsible to:

- Construct water and sewer utilities to the site from the nearest point of connection to the west; location and design of such utilities shall be subject to City approval
- Annex to any Community Facilities District(s) formed by property owners within the Specific Plan area, as described in Chapter 14.0, Implementation and Administration, for both the financing of infrastructure and cost of municipal services
- Pay any applicable FTSSP impact fees that are in effect at the time of utility connection
- Pay all connection fees





Policy 4-41

PA9-B shall be subject to all policies, provisions and standards of the FTSSP.

Planning Area 9-C: Madison Square Development.

Policy 4-42

PA9-C has received all entitlements from the City for development of a 199-unit, small lot single-family detached project. Development and use of this subarea shall be in accordance with the RH and RM zone districts.

Planning Area 9-D: McAvenia Property.

Policy 4-43

The principal access to McAvenia Property shall be from Peabody Road and shall align with adjacent street intersection on the west side of Peabody Road.

Policy 4-44

No subdivision map approvals or other planning approvals shall be granted until the existing agricultural preserve contract has been cancelled or until it expires.

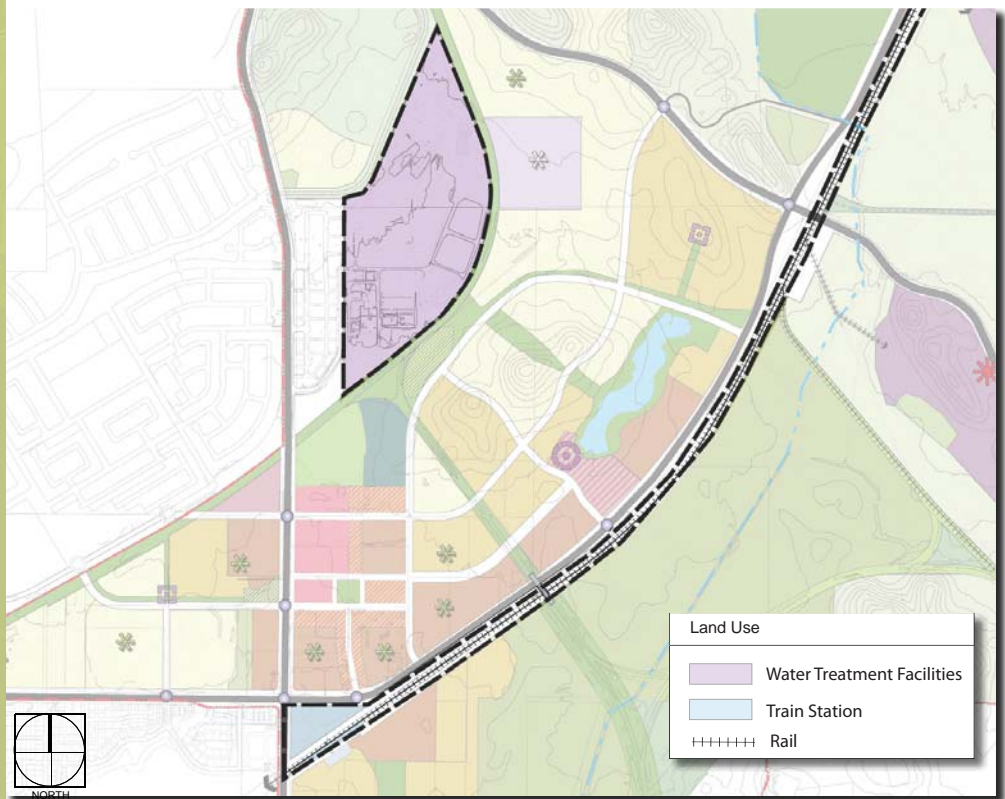


Figure 4-17: Planning Area 10 (PA10) Map

Policy 4-45

A maximum of 105 dwelling units shall be allowed within PA9-D, unless a density transfer is approved in accordance with Chapter 14.

Policy 4-46

PA 9-D shall be subject to all policies, provisions and standards of the FTSSP.

4.4.10 Planning Area 10

Planning Area 10 (PA10) includes two critical public facilities that serve the cities of Fairfield and Vacaville as well as the Specific Plan area: the Train Station at the southeast corner of Vanden Road and Peabody Road and the Northbay Regional Water Treatment Facility to the west of the Linear Park Trail.

PA10 Policies

The following policies shall apply to the development of PA10:

Policy 4-47

Development and use of the properties in PA10 shall be subject to the Public Facilities Zone District.

Policy 4-48

Land within PA10 shall not be subject to payment of taxes established by City through a Community Facilities District, assessment district, or similar financing district.

4.5 Permitted Uses and Development Standards

The concept for the FTSSP is implemented through the adoption of the Land Use Plan (Figure 1-1 in Chapter 1.0), which is organized into 10 Planning Areas. Each Planning Area reflects a set of character elements and focused land uses that best serve the needs of the Community, although a wide range of uses and activities may occur within each Planning Area. Additionally, since flexibility is a key goal and being responsive to changes in market conditions is vital to the success of the Project, density transfers among various Planning Areas are allowed as part of this Specific Plan. Please refer to Chapter 14.0, Implementation and Administration, for more detail.

All land use and development regulations for FTSSP development shall be regulated by the Train Station Specific Plan PD Overlay District. The standards in this Specific Plan supersede those of the Zoning Code, unless otherwise stated in this document. If the FTSSP is silent on an issue, existing Zoning Code regulations shall apply, as appropriate.



For regulatory purposes, as shown in the Land Use Plan (Figure 1-4) and discussed in this section, the Project is broken into 10 Planning Areas. The policies and development regulations in this section shall apply to each land use, or as a modified regulation particular to a Planning Area.

Refer to Table 4-1 in this Chapter for the distribution of the land uses within the Specific Plan area and Planning Areas.

4.5.1 General Land Uses

Permitted and regulated uses (by land use and Planning Area) shall be consistent with Figure 1-1 of this document and Section 25.10 of the Zoning Code except where specifically modified by this Specific Plan. Use modifications shall be limited to the correlating land uses established by the Land Use Plan, zoning standards of the Zoning Code, and the regulating modifications of this document.

Policy 4-49

Land uses shall be consistent with the Land Use Plan Figure 1-2.

Policy 4-50

All setbacks shall be measured from the Property Line (PL) unless otherwise noted within this document. Street PL shall be measured from the back of sidewalk or back of right-of-way (ROW) as established by the Street Sections in Chapter 10.0 of this document.

Policy 4-51

The following standards shall apply to all development:

- All buildings abutting Peabody Road shall be setback 10 feet from its right-of-way line.
- All buildings abutting Vanden Road shall be setback 15 feet from its right-of-way line.
- Multiple wall plane offsets, horizontal or vertical, shall be required. See Chapter 13.0 Design Guidelines for additional requirements.



Policy 4-52

Building facades within the MR, HR and shall comprise a minimum of 80 percent of the street frontage along the following sections to provide an architecturally dominant edge for the community:

- Thematic Streets
- Main Street
- Civic Boulevard
- Peabody Road
- Vanden Road (all roads designated on Figure 10-2)

Policy 4-53

Building facades of MR, HR and CM shall comprise 60 percent of the street frontage along:

- Connectors
- Connectors with Multi-Use Trail (all roads designated on Figure 10-1)

Policy 4-54

Surface and/or garage/carport parking shall not be permitted within the minimum building front setback area.

Policy 4-55

Signalized intersections on Peabody Road and Vanden Road shall be limited to those intersections shown on the Land Use Plan (Figure 1-2) of the Specific Plan. The number of non-signalized intersections with Peabody Road and Vanden Road shall be limited, to the extent feasible, and are subject to City approval through development entitlement process.

Policy 4-56

Primary circulation per Planning Area shall be primarily consistent with Figure 10-1, Circulation plan, in Chapter 10.0 of this document. Variations in alignment may be proposed by landowners and considered by City through approval of the Master PUD Permit.



Home-Based Business District

Home-Based Business Districts (HBBDs) refer to a permissible building type designed to suit the needs of entrepreneurial business owners that would operate out of their homes or in facilities on the same lot as their homes. Only uses that are complementary to, and can exist in harmony with, a residential neighborhood geared toward the special needs of home-based business owners are permitted.

An HBBD configuration should generally, in character and massing, look like the surrounding residential or commercial mixed-use development. This building/housing type allows a portion of the ground floor plan to be used for small, family-owned and operated businesses with no more than one non-family employee present at one time.

Policy 4-57

A HBBD residence or building shall provide:

- Floor area on the main floor for entrepreneurial home businesses
- Direct access to business space from the street or parking
- One additional parking space in addition to base zone parking requirements
- Appropriately scaled signage permitted on units along the street frontage for the promotion of business
- Permitting of business uses within the residential unit, with the inclusion of one non-family employee
- Circulation, access, and parking can be shared with, and be proximal to, residential uses.

Policy 4-58

An HBBD may be allowed subject to City's approval of a conditional use permit within the MR, HR, CM, and CN zones of the FTSSP. HBBD shall not be permitted within multi-family rental projects. Development standards of the applicable residential use shall apply.

Live/Work

Live/Work allows residential mixed-use within a commercial designation with residential and retail/service uses to occurring within the same structure. In such arrangements, residential uses may occur above or adjacent to retail/service uses located on the ground level of the same building. Circulation, access, and parking can be shared with and be proximal to residential uses. Direct internal access between the residential use and commercial is encouraged but not required.

Policy 4-59

Live/Work is permitted within MR, HR land uses. No separate standards are provided for Live/Work, as development standards of the applicable land use shall apply to this category.



4.5.2 General Residential Policies

The goal of residential development is to provide for a range of high-quality housing choices throughout the FTSSP community that support the TOD framework. A variety of housing types and configurations are proposed in a range of neighborhood settings that may include single-family detached, townhomes, condominiums, and apartments.

The plan dedicates 504 acres of land to residential uses for the development of up to 6,800 dwelling units; of these 140 units are potentially proposed on the proposed Vacaville Water Treatment site. Residential units are regulated by this Specific Plan through three residential land use categories:

- Low Density Residential (LR) 4-10 du/ac, Target Density 7 du/ac with a potential for 1,617 dwelling units
- Medium Density Residential (MR) 10-20 du/ac, Target Density 14 du/ac with a potential for 2,258 dwelling units
- High Density Residential (HR) 20-50 du/ac, Target Density of 28 du/ac with a potential for 2,535 dwelling units

The Land Use Plan provides for the development of 3,000 residential units within a half-mile radius of the Train Station.

In addition to the proposed residential development, there are approximately 500 residential units within the boundary of FTSSP that exist or have received development entitlements from City prior to FTSSP adoption. These residential units are anticipated to remain as existing, conforming uses.

The following policies apply to all properties developed as residential or including residential units within the property boundary, as established by this document:

Policy 4-60

Each property shall be assigned a “Dwelling Unit Allocation.” The Dwelling Unit Allocation is calculated by multiplying the Target Density by the amount of adjusted gross acres. The City shall determine the Dwelling Unit Allocation for each property through the approval of an Master PUD Permit.

Policy 4-61

Each density category has a “Target Density.” Each property shall have the right to develop at its Target Density.

Policy 4-62

To promote density and alternative transit use, development of the HR and MR parcels within a half-mile radius of the Train Station shall provide for a minimum of 3,000 dwelling units.





Policy 4-63

New residential structures shall not be constructed within 150 feet of the existing, overhead 230-kilovolt (kV) Transmission Line, located along Vanden Road. Portions of a residential development that may be located with the 150-foot setback requirement include:

- 1) Nonhabitable structures, such as garages or storage buildings,
- 2) Landscaped areas
- 3) Parking areas.

Compliance with this policy shall be determined by City through the development review process.

Policy 4-64

New residential structures shall not be constructed within 50 feet of an existing, overhead 60-kV or 21-kV Transmission Line. Portions of a residential development that may be located with the 50-foot setback requirement include: (1) nonhabitable structures, such as garages or storage buildings, landscaped areas and parking areas. Compliance with this policy shall be determined by City through the development review process.

Policy 4-65

To maintain the integrity of the pedestrian circulation in key areas of the community, property/parcels designated LR, MR or HR along streets identified in Figure 10-2, "Streets within FTSSP with No Driveways", shall:

- Be developed as non-front loaded units,
- Take vehicle access to garage from the alley, access drive, motorcourt or side/rear lot line(s)
- Have front and/or side facades face the street right-of-way and
- Not have curb cuts for driveways along these streets
- Not have continuous back yard or patio walls

Policy 4-66

Housing configurations, including small lot development, that include common open spaces such as courtyards or paseos shall have property lines recorded at the center of the common open space feature with recorded reciprocal use easements for common access.

Policy 4-67

Residential development, regardless of land use designation, shall be regulated by the applicable Zoning Code base zone by density/intensity except as modified by the Train Station Specific Plan PD Overlay District or policies of this Specific Plan.

Policy 4-68

Residential neighborhoods adjacent to common open space features shall provide pedestrian connectivity by paseo or other connection feature directly from the neighborhood to the common open space.

Density and Acreage Transfers

Transfer of residential units and/or acreage within the FTSSP may be permitted by City to meet the needs of a dynamic market. Refer to Chapter 14.0, Implementation and Administration of this document for further standards and regulations regarding density and unit transfer.

4.5.3 Low Density Residential (LR)

Low Residential (LR) addresses low-density detached residential development permitted to range in density from 4.0 du/ac to 10.0 du/ac with a Target Density of 7.0 du/ac. LR will generally be single-family detached homes with street front orientation where primary entries and walks face the streets. Private outdoor living space can occur in front, rear, and/or side yards. Residential parking spaces are provided in garages and guest parking spaces are provided in driveways and local streets.

This category also includes small-lot detached, single-family detached alley loaded lots, and limited small-lot attached configurations; this includes Zoning Code designations RL, RLM and Small-Lot Development. Various configurations are allowed including single-family detached alley loaded lots, duplex lots, motor court cluster, green court cluster and paseo cluster, single-family attached duplex, single-family attached green court cluster, single-family attached paseo cluster, manor homes, and row townhome units.

Residential development within the FTSSP LR land use shall be regulated per the applicable density regulations of the Zoning Code Table 25-3 and RLM regulations of Table 25-4 except where modified by the policies of this document. Refer to the Train Station Specific Plan PD Overlay District of the Zoning Code for allowed/regulated land uses within the LR district. Where the standards of this Specific Plan are silent, the regulations in Table 25-3 or 25-4 based on applicable density shall apply in their entirety. The following modifying policies shall apply to residential development within the LR zone:

Policy 4-69

Development within LR areas shall not exceed 1,617 dwelling units, unless City grants a density transfer.





Policy 4-70

All LR small-lot development shall meet the requirements of Zoning Code Section 25.20.3.2; property may be developed as Standard Detached or Small Lot unless otherwise determined by this Specific Plan or an Master PUD Permit.

Policy 4-71

Designated LR shall have the right to develop:

- at the “Target Density” of 7.0 dwelling units per adjusted gross acre,
- at no less than 4.0 dwelling units per adjusted gross acre.

Policy 4-72

Property/parcels designated LR (Low Density) may develop at a density above the Target Density provided:

- The density is not more than 10.0 dwelling units per adjusted gross acre; and
- One or more property owners in the Planning Area have transferred a portion of their “Dwelling Unit Allocation” equal to the number of dwelling units above the Dwelling Unit Allocation for the subject property.

Policy 4-73

LR development shall be subject to the policies of this section and LR specific policies by planning area. Refer to applicable planning area policies.

Policy 4-74

LR development of 7.0 du/ac or greater shall be developed per the LR policies of this document; where regulations of this document are silent see Table 25-4 or 25-5 of the Fairfield Zoning Ordinance per the discretion of the Community Development Director.

Policy 4-75

LR Small-lot development:

- Shall not take garage access directly from arterial streets or connector streets.
- Is permitted to take garage access from an alley, courtyard, or courtstreet.
- A maximum of 15 percent of the total small-lot development units are permitted to take garage access directly from the street where site conditions prevent all units from being accessed by an alley, courtyard, or courtstreet at the discretion of the Director.

Policy 4-76

Where the length of the driveway from a garage to a front property line is 18 feet or greater, parking in driveway shall count toward uncovered on-site parking requirements for the residential unit on that lot.

Policy 4-77

The following modified development standards shall be permitted for any LR development of 7.0 du/ac or greater:

- 10 feet minimum setback from any sidewalk to a porch or courtyard
- 5 feet minimum setback from any common open space to a porch or courtyard

Policy 4-78

Through the PUD Permit process, City may reduce the first- and second-story building-to-building separation across a paseo separation to 20 feet for a portion of the length of the paseo.

4.5.4 Medium Density Residential (MR)

Medium Density Residential (MR) addresses a range of attached housing types permitted to range in density from 10.0 du/ac to 20.0 du/ac, with a Target Density of 14.0 du/ac. The MR density may include town homes, flats, condominiums, and rental apartments. These may be clustered around auto courts, green courts, rear-loaded facing public streets, or front-loaded facing private lanes. Various configurations are allowed. Direct garage access shall not be taken from the public streets to maintain a pedestrian-friendly street character in the Town Center and Lake District planning areas.

Residential development within the FTSSP MR land use shall be regulated per applicable density by the RM and RH Zone districts of the Zoning Code Table 25-5 except where modified by the MR policies below. Refer to the Train Station Specific Plan PD Overlay District of the Zoning Code for allowed/regulated land uses within the MR district. MR land use areas within the FTSSP are subject to the regulating standards for the MR zone of the Zoning Code. Where the standards of this Specific Plan are silent, the regulations in Table 25-5 shall apply in their entirety.

The following modifying policies shall apply to residential development within the MR zone:

Policy 4-79

MR development shall not exceed 2,258 dwelling units within the FTSSP per the land use regulations of this document.





Policy 4-80

Each property designated MR shall have the right to develop:

- at the “Target Density” of 14.0 dwelling units per adjusted gross acre,
- at no less than 10.0 dwelling units per adjusted gross acre.

Policy 4-81

Each property designated MR may develop at a density above the Target Density provided:

- the density is not more than 20.0 dwelling units per adjusted gross acre; and
- one or more property owners in the Planning Area(s) have transferred a portion of their Dwelling Unit Allocation equal to the number of dwelling units above the Dwelling Unit Allocation for the subject property.

Policy 4-82

An HBBD shall be conditionally permitted within MR land uses. See Section 4.5.1 for HBBD details.

Policy 4-83

MR units shall not take garage access directly from a public street, except where site conditions prevent all units from being accessed by an alley, courtyard, or court-street. A maximum of 15 percent of the total units within each development may be permitted to take garage access directly from the street, as determined through the PUD permit process.

Policy 4-84

MR minimum private open space dimension:

- 10 feet minimum ground floor units,
- 6 feet minimum second floor or above units/stories;

Policy 4-85

MR private open space:

- may be provided on the first or second story.
- is not required to be roofed or enclosed but shall be defined by architectural features or walls
- shall be identified on architectural plans.
- may be provided in a single space or in two separate spaces, i.e., porch and interior courtyard
- porch/courtyard spaces within the front setback shall count toward private open space requirements.

Policy 4-86

MR Common open space:

- shall have a minimum dimension of 30 feet, except for spaces qualifying under Policy 4-88
- shall be usable and serve the community as an amenity
- shall comprise 20 percent of the net development site (total site area less public right-of-way)
- shall comprise 16 percent of net development site where common open space is aggregated into large parks, plazas, or amenities to serve a common neighborhood rather than an individual development/parcel
- aggregation of common open space resulting in the reduced standard shall be subject to approval of the Director.

Policy 4-87

Areas that meet the minimum building separation requirements shall count toward common open space requirements for the parcel if the area is:

- a minimum of 40 percent landscape/hardscape
- 50 percent usable

Policy 4-88

MR minimum building setbacks:

- 15 feet minimum clear setback from Vanden Road right-of-way
- 8 feet minimum setback from any sidewalk to a porch or courtyard
- 3 feet minimum setback from any common open space to a porch or courtyard
- 14 feet clear minimum paseo width to count toward common open space

Policy 4-89

Through the development review process, City may reduce the first- and second-story building-to-building separation across a paseo separation to 20 feet for a portion of the length of the paseo.

4.5.5 High Density Residential (HR)

High Residential (HR) addresses higher-intensity and mixed-use configuration housing types with a permitted density of 20.0 du/ac. to a maximum of 50.0 du/ac., with a Target Density of 28 du/ac. This HR land use may include townhomes, apartments, and condos typically located near neighborhoods serving commercial, the Lake Park, or transit development. Most buildings will be “rear loaded”; that is, garage access shall not be taken directly from public streets but from





internal lanes located in the rear of the building. This design promotes a sense of community by facing units onto streets and public places.

For a list of permitted, conditionally permitted, and prohibited uses, please refer to Section 25.20.0, Table 25-1 of the Zoning Code. HR land use areas within the FTSSP are subject to the regulating standards for the RVH zone of the Zoning Code. The following standards address exceptions to the RVH zone as applicable to FTSSP HR land uses and shall supersede applicable Zoning Code standards/requirements. Where the standards of this document are silent, the conditions in Table 25-6 shall apply in their entirety. The following modifying policies shall apply to HR residential development within the HR and CM zones:

Policy 4-90

A minimum of 50 percent of the dwelling units in the HR and CM zones shall be condominiums or townhouses, as defined in Section 25.50 of Zoning Code. Consistency with this policy shall be determined through approval of the Master PUD Permit and any subsequent planning approval.

Policy 4-91

New development of high-density residential units within the HR and CM zones shall not exceed 2,535 dwelling units, unless a density transfer is granted by City.

Policy 4-92

Each property designated HR, or HR development within the CM designation, shall have the right to develop:

- at the “Target Density” of 28.0 dwelling units per adjusted gross acre,
- at no less than 20.0 dwelling units per adjusted gross acre.

Policy 4-93

Each property designated HR (High Density) may develop at a density above the Target Density provided:

- the density is not more than 50.0 dwelling units per adjusted gross acre; and
- one or more property owners in the Planning Area have transferred a portion of their Dwelling Unit Allocation equal to the number of dwelling units above the Dwelling Unit Allocation for the subject property

Policy 4-94

Residential development in HR designated property/parcels from 20.0 to 24.0 du/ac shall be permitted to develop per the RH or RVH regulations in Table 25-5 of the Zoning Code.

Policy 4-95

HR residential development in CM designated property/parcels shall be develop per HR designation of Train Station Specific Plan PD Overlay District and policies of this document; where this document is silent refer to the Fairfield Municipal Code.

Policy 4-96

To allow for creative evolution of building, and best practice design solutions, all high-density attached housing configurations are allowed within FTSSP. Policies of this Specific Plan and the underlying Zoning Code district shall be upheld, however otherwise the design is unrestricted. Configurations of residential uses shall not be limited by the housing diagrams or images contained in the Zoning Code or the images of this document.

Policy 4-97

HR dwelling units located within a 10-minute walk of the Train Station and which have a density of 32.0 du/ac or greater are eligible for reduced parking standards, to be determined through the development review process, based on configuration, on-street parking, and amenities provided within the area.

Policy 4-98

HR units developed at 24.0 du/ac density or greater shall be permitted to reduce ground floor Private Open Space per Unit to a minimum of 60 square feet. Minimum dimension shall be 6 feet.

Policy 4-99

Surface parking for HR zones should generally be located behind or within building configurations to prevent visual impact of parking along public street frontage. Where surface parking is present along the frontage of a street identified in Figure 10-1, a minimum of 60 percent of the linear street frontage shall contain a building façade.

Policy 4-100

A variety of building types and styles of architecture shall be provided.

4.5.6 Nonresidential & Light Industrial/Employment

The plan designates 47 acres of land for commercial uses for up to 130,680 square feet of office, service commercial, and retail development. The goal of commercial land uses is to provide for services and employment opportunities within the community to increase the jobs/housing balance within the FTSSP and reduce the distance and frequency of vehicle trips between daily needs. Commercial land uses reflect three service categories consistent with Commercial Zones of the Zoning Code:





- Community Commercial (CC) with 17 acres of service intensive, larger-scale retail, and professional office uses within the Town Center District, PA2.
- Mixed-Use Commercial (CM) with 20 acres of mixed office, retail, and neighborhood services land within the Town Center District to allow opportunities for vertical mixed use around the Town Square and leading to the train station.
- Neighborhood Commercial (CN) with 10 acres of smaller-scale, neighborhood serving and boutique retail within the Lake District, PA4.

Per the standards of this document, commercial uses may be vertically or horizontally mixed with residential units of various configurations within these commercial land use designations.

Shared Parking

This Specific Plan encourages use of shared parking for commercial areas within PA2 and PA4. Development of a parking management plan is required to clearly define and achieve projected shared parking as part of the Master PUD Permit submittal. Shared parking studies and subsequent parking reductions shall conform to the Shared Parking policies of the CM zone adopted concurrently with this Specific Plan.

Policy 4-101

Shared parking is permitted and encouraged. Shared parking shall be approved subject to City review and approval of a shared parking study based on ULI accepted shared parking ratios, as may be amended.

Policy 4-102

Where adjacent nonresidential and/or mixed-use development is proposed, the baseline parking requirement shall be calculated as the sum of the requirements for each use per the applicable standards of Zoning Code Table 25-17 and/or applicable residential use parking requirements for the Municipal Code or this Specific Plan.

Community Commercial

Community Commercial (CC) within the Town Center is designed as a pedestrian-friendly, tight-knit mixed-use district. A Main Street configuration is required for a portion of the district with the remainder organized in more conventional parking, anchor store, and pads design. Setbacks along the Main Street and Town Square will be reduced with buildings and entries oriented toward the street and parking behind. On other building setbacks, larger-scale and lower Floor Area Ratio (FAR) will characterize this area. Architectural quality standards shall apply to create a shopping and services center consistent with the character of the FTSSP.

The CC policies of this document are addressed within the PA2 and shall be utilized as modified development policies for the development of the CC parcel and shall supersede applicable Zoning Code regulations. Where the standards of this document are silent, the land uses of Table 25-11 and regulations of Table 25-10 for CC shall apply in their entirety.

Mixed-Use Commercial

Mixed-Use Commercial (CM) is designed to create and support an actively integrated community by providing a development form with a compatible mixture of uses that allows for horizontal and vertical mixed-use configurations. Successful mixed-use provides links between uses to support a dynamic neighborhood that encourages people to walk to daily activities and socialize in an active outdoor setting.

Residential and nonresidential uses may be combined in this district to benefit from and support the use of rail transit and provide goods and services to transit users and residents for a full-service transit-oriented neighborhood. This may include live/work, residential over retail, office over retail, residential over office, and/or residential development with commercial/retail components in other compatible configurations.

Form and design within the CM zone are critical in creating a highly pedestrian-oriented setting consistent with the goals for the Town Center. A special focus will be put on the so-called “20/20 Zone,” that is, the first 20 feet horizontally and 20 feet vertically from the primary walkways. Ground floor elevation design, building entries, signage, materials, and, massing shall be executed in a manner that is a visually charming and human scale.

The CM land use of this Specific Plan shall be developed per the CM zone amendment adopted concurrently with this document. Additional CM policies for the FTSSP are addressed within the PA2 policies in Chapter 5.0 and shall be utilized as modified development policies in conjunction with the Train Station Zoning Amendment adopted with this document, for CM properties. Where the standards of this document are silent, the CM zone amendment and Zoning Code shall apply in their entirety.

Neighborhood Commercial

Neighborhood Commercial (CN) is designed for smaller scale retail including local serving shops, bank branch, private recreation or local library. CN development will generally be of a finer scale with greater attention to architectural detail. It will provide access to the Lake District Promenade and will include greater flexibility in setbacks and building form. Architectural quality standards shall apply to create a neighborhood retail center consistent with the character of the FTSSP.



The CN zone within PA4 shall include the development of a local branch library, consistent with the Solano County Library Facilities Master Plan.

The policies for CN in this Specific Plan shall be utilized in conjunction with the Train Station Specific Plan PD Overlay District as alternative development regulations for the development of the CN parcel and shall supersede applicable Zoning Code regulations.

For a list of permitted, conditionally permitted, and prohibited uses, refer to Section 25.22, Table 25-9 of the Zoning Code.

Limited Industrial/Employment

The plan dedicates 286 acres of land to Industrial uses (IL) within PA6 in for up to 4,600,000 square feet of potential development. The goal for the industrial land uses is to provide for an industrial and employment center with the potential to attract industrial, manufacturing, research and development (R&D), corporate headquarters, and administrative offices. This land use provision increases the jobs/housing balance within the FTSSP and reduces the distance and frequency of vehicle trips between daily needs. Portions of the industrial area have the potential to be rail-served by connecting to the Capital Corridor rail line. The industrial land use is consistent with the IL zoning category of the Zoning Code.

Each of the two portions of PA6 may include a modest service center for the provision of food service, business-oriented retail, health care, and/or other uses that allow employees to meet daily needs without driving or walking long distances.

The policies for IL/EMP in this Specific Plan shall be utilized in conjunction with the Train Station Specific Plan PD Overlay District as alternative development regulations for the industrial parcels and shall supersede applicable Zoning Code regulations.

For a list of permitted, conditionally permitted and prohibited uses, please refer to Section 25.22, Table 25-9 of the Zoning Code. Refer to the modified development regulations and policies of PA6 in this Chapter for IL/EMP development. Where the standards of this document are silent, the conditions in Table 25-12 for IL shall apply in their entirety.

4.5.7 Nonconforming Uses

There are certain existing uses that will become legal, non-conforming uses upon adoption of the Specific Plan and subsequent annexation to the city of Fairfield. These are primarily industrial and commercial service uses along Vanden, Peabody and Cement Hill Roads. They were approved by Solano County when the area was located in the unincorporated area and subject to the development regulations of the County.

Nonconforming Use Policies

The following policies shall apply to all applicable nonconforming uses within FTSSP:

Policy 4-103

Some of the existing uses in PA1 and PA2 are not allowed or conditionally allowed by the regulations of the Specific Plan. All such uses that are in existence at the date of annexation to the City and that had obtained all necessary permits and approvals by Solano County based on the Solano County regulations then in effect, shall be classified as legal, nonconforming uses and shall be regulated in accordance with Section 25.46 of the Zoning Code.

Policy 4-104

Through adoption of an Master PUD Permit, the City shall have the right impose development standards on new residential development to ensure it is compatible with existing, nonconforming uses. The scope of these standards will vary depending on the nature of the nonconforming use(s) and housing types proposed. The City may require the Master PUD Permit applicant(s) to submit supplemental studies regarding noise, air quality and hazardous materials for City to determine appropriate standards. The types of standards may include but are not limited to the following:

- i. Increased residential setbacks from nonconforming uses.
- ii. Construction of walls or fences along the property line shared by the existing nonconforming use and the proposed residential development.
- iii. Installation of landscape barrier (trees and shrubs) along the property line shared by the existing nonconforming use and the proposed residential development.

Policy 4-105

Existing structures within the Specific Plan area that do not conform to the standards and regulations of the Specific Plan shall be classified as nonconforming structures and shall be regulated in accordance with Section 25.46 of the Zoning Code.

Policy 4-106

To facilitate the orderly and timely transition from existing, nonconforming uses to uses that conform to the Specific Plan, the following policies are enacted:

- i. Existing, nonconforming industrial use shall be classified as a permitted uses in Planning Area 6, subject to the following provisions:





- Through development review process, City shall have the right to impose all necessary and appropriate conditions to ensure the compatibility of the use with adjacent land uses.
- Development review conditions may include but are not limited to building design, building height and location, hours of operation, lighting and noise standards, screening of outdoor storage, and operational aspects of the use.

Policy 4-107

The City may but is not obligated to, approve a relocation of an existing, nonconforming use to PA6, which would have a larger site area, larger building area, or greater intensity of industrial operations than exist at the time of annexation

Policy 4-108

The provisions of Policy 4-108 shall remain in effect until December 31, 2021. At that time, this policy shall become null and void without further action by the City.

Policy 4-109

The City shall allow some industrial land to develop in PA6 prior to the construction of the New Canon Road overcrossing. The amount of land which may be developed prior to the construction of the New Canon Road overcrossing shall be based on the traffic capacity of existing Canon Road and related traffic safety considerations. This amount shall be determined by City, based on traffic study submitted by landowners within PA6.

4.5.8 Community Land Uses

The plan dedicates more than half of plan area to community facility and public open space uses for the general well-being of the City and FTSSP community. The goal of community land uses is to provide for civic services such as schools, a library, community centers, recreation clubs, and fire safety facilities as well as parks and open space. Architectural treatment and design of all publicly-visible buildings associated with community land uses shall be consistent with the FTSSP character and design requirements.

For a list of permitted, conditionally permitted and prohibited uses, please refer to Section 25.26, Table 25-13 and Table 25-14 of the Zoning Code. For development requirements for all public, quasi-public, recreation and open space uses, please refer to Section 25.26, Table 25-14 of the Zoning Code.

Specific policies and requirements for community land uses are addressed within the applicable Planning Area policies and/or Chapter 9.0, Public Services.

Public / Quasi-Public / Institutional

Community Facilities/Public Utilities/Institutional acreage is provided in the following Planning Areas: PA1, PA2, PA3, PA4, PA6, and PA10. These facilities will include:

- Elementary school on approximately 10 acres within PA2
- Public buildings consisting of a library and “Village Club” community center within PA4
- Quasi-public facilities consisting of day care centers and private recreational clubs
- Fire training center on approximately 5 acres within PA6

Active Open Space / Parks / Recreation

The plan dedicates approximately 158 acres of land to Active Parks/Recreation uses which are public parks throughout FTSSP. Active Parks/Recreation acreage is provided in the following Planning Areas: PA1, PA2, PA3, PA4, PA5, and PA7 and PA8. Active Parks/Recreation uses may include:

- “Great Park” in PA7 on a minimum 50-acre site, with recreation activities; including sports fields, skate node, trails, picnic areas, and lake
- “Lake Park” in PA4 on an approximate minimum 22-acre site, with an 11-acre lake and approximately 11 acres of park land surrounding the water features
- Linear Park Trail connecting most of the FTSSP community on 54 acres, which includes a multi-use trail and landscaping
- Paseos and smaller public park areas within residential areas

Passive Open Space / Conservation

The plan dedicates 1,531 acres of land as Passive Open Space/Conservation. Open space uses include agriculture, habitat conservation, mitigation banks, detention basins, and public open space uses. Passive Open Space/Conservation acreage is provided in the following Planning Areas: PA1, PA2, PA3, PA4, PA6, and PA7 and PA8.



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5.0 TOWN CENTER DISTRICT PLAN



5.0 TOWN CENTER DISTRICT PLAN

July 26, 2011



5.1 Overview and Vision

The Town Center District, Planning Area 2, is the “Down Town” activity center of the Fairfield Train Station community. As a multi-use residential, retail and office district it represents the essence of emerging state and county smart-growth, low carbon and TOD policies. Linking to and building around the new Fairfield Train Station, PA2 has as its fundamental driver the provision of housing, jobs and shopping that are:

- 1) close to regional commuter rail access,
- 2) integrated into a walkable, mixed use district, and
- 3) supportive of a less auto-dominant life style by providing many services within walking distance.

The “big idea” is to provide a place within Fairfield where a single person, couple, family or empty-nester can have a starter home, apartment or retirement home with accessibility to transit, shopping, entertainment, civic life and high-quality public spaces while maintaining security, affordability and a level of community engagement rare in a suburban location.

In order to ensure the distinctive place-making nature and walkability of the Town Center District, specialized regulations have been established to guide development. This section of the Specific Plan shall serve as the regulating document for the Town Center District, PA2. It establishes an additional level of specific principles, form and land use regulations as well as design guidelines to ensure the built form of the Town Center District meets the goals of the FTSSP.

TOD Principles

1. Walkable design with pedestrian as the highest priority
2. Train station as prominent feature of Town Center
3. A regional node containing a mixture of uses in close proximity including office, residential, retail, and civic uses
4. High density, high-quality development within 10-minute walk circle surrounding Train Station
5. Designed to include the easy use of bicycles, scooters, and rollerblades as daily support transportation systems
6. Reduced and managed parking inside 10-minute walk circle around Town Center / Train Station



5.2 Town Center Planning Principles

The framework of the Town Center District is based on the planning principles described below. Increased density, quasi-urban street character, and a fine-grained interaction of land uses is required to achieve the City's policy intent for the district. Investment in quality public spaces, parks, squares and plazas will also increase visitation of retail, desirability of housing and economic value for land owners. Individual sites within PA2 shall meet the intent of the following principles:

5.2.1 Land Use

- Locate highest-density residential units along Vanden Road in close proximity to the Train Station.
- Centralize commercial uses within the Community Commercial parcel to balance community vehicular access with local pedestrian access.
- Integrate land uses to promote a live/work/play environment.
- Organize retail along a traditional Main Street with retail, service and restaurant uses.
- Encourage sidewalk eateries and decorative retail displays within and along pedestrian zone.
- Allow for a variety of housing types and residential configurations.
- Provide direct access to the street and public pathways for all residential units.

5.2.2 Urban Design

- Create character reminiscent of the best traditional downtowns of small town America.
- Implement a master plan that respects existing ownership patterns and maximizes efficiency of existing parcels.
- Design streets and pathways that create a walkable community; pedestrian are the highest priority.
- Direct pedestrian pathways to transit and commercial centers.
- Incorporate plazas, seating areas and community elements within each individual site design.
- Develop a traditional urban street grid pattern that follows existing land ownership patterns to the extent feasible.
- Utilize parking solutions that reduce the impact of off-street surface parking on the pedestrian experience.
- Increase building height and density close to retail and transit.
- Use lighting to invite activity along pedestrian corridors and places of interest.

5.2.3 Architecture

- Design buildings with a base, middle and cap.
- Articulate wall planes on buildings to create visual variety along the streetscape.
- Site buildings oriented to have a tight, strong relationship to the street and public spaces through placement, architecture and sensitive site design.
- Articulate the ground floor frontage adjacent to streets with glazing, entries and architectural detail to support a pedestrian friendly walking environment.



5.2.4 Landscape

- Provide street trees on all streets within the district.
- Provide a balance of natural landscape and built form even in urban environments.
- Provide biodiversity as a part of the landscape palette.
- Articulate plazas, courtyards and greens to maximize activity and use programming opportunities.
- Select tree species that maximize shade and reduce urban heat island effect.



Figure 5-1: Thematic Civic Boulevard provides pedestrian-scaled retail connection to Train Station

Key “CC” Design Elements:

1. Strong visibility and access from Peabody Road
2. Ample parking in front of major anchors
3. Provision of free-standing retail pads as needed
4. Development of “hybrid” configuration melding conventional strip-type center with main street-type center
5. Direct vehicular and pedestrian linkage between strip portion and Main Streets
6. Ample pedestrian space in front of anchor and related shops
7. Articulated architectural style compatible with other commercial buildings in district
8. Ample landscape space in front to stores and tree planting within parking lot.

5.3 Town Center Land Use

To ensure that the Town Center District achieves its policy and design goals, particular attention is required in its design and execution. Design guidance shall come from two separate sources:

- 1) The City of Fairfield Development Regulations (separate from this document), and
- 2) The design policies of this chapter and in Chapter 13.0, Design Guidelines, of this document.

Land uses within Town Center District are identified on Figure 5-2, and are summarized on Table 5-1; they shall be consistent with the application of Zoning Code except where modified by this Specific Plan. Where the FTSSP is silent on a standard, in this section or the Specific Plan, the Zoning Code shall apply.

Although long-term build-out of the Town Center district requires these structured regulations for continuity, it also necessitates flexibility to respond to the evolving design, building and economic demands. A reasonable amount of flexibility is embedded in the guidelines to allow design creativity and land use, density and market response.

5.3.1 Community Retail Village

The weekly and daily shopping needs of the Specific Plan area will be served by a community shopping village consisting of a neighborhood grocery store acting as anchor, retail pads and smaller line shops. The grocery store should be located to enjoy full visibility and parking access from Peabody Road. A portion of the smaller tenants are planned to be directly adjacent to the south creating a walking link to the Main Street component

Commercial mixed-use land uses (CM) are located around the Town Square, flanking Main Street and providing a commercial/residential corridor through the Town Center District. Approximately 20 acres of CM have been designated by the FTSSP Land Use Plan to enable sufficient commercial, office and live/work development. Horizontal and vertical CM configurations are permitted, limited only by the development standards and requirements of this document and the applicable Zoning Code. CM parcels are encouraged to include a range of uses and configurations that may include retail, neighborhood services, small offices, live/work units and residential above commercial buildings. CM development shall be regulated by the CM zone amendment adopted concurrently with this Specific Plan.

Community Commercial (CC) uses are the higher-intensity, larger-footprint commercial uses required within a community. This is intended for grocery, neighborhood services and larger-scale commercial. Approximately 17 acres of CC have been designated by the FTSSP Land Use Plan (Figure 1-1) within the Town Center District to provide a central community location serving all of FTSSP.

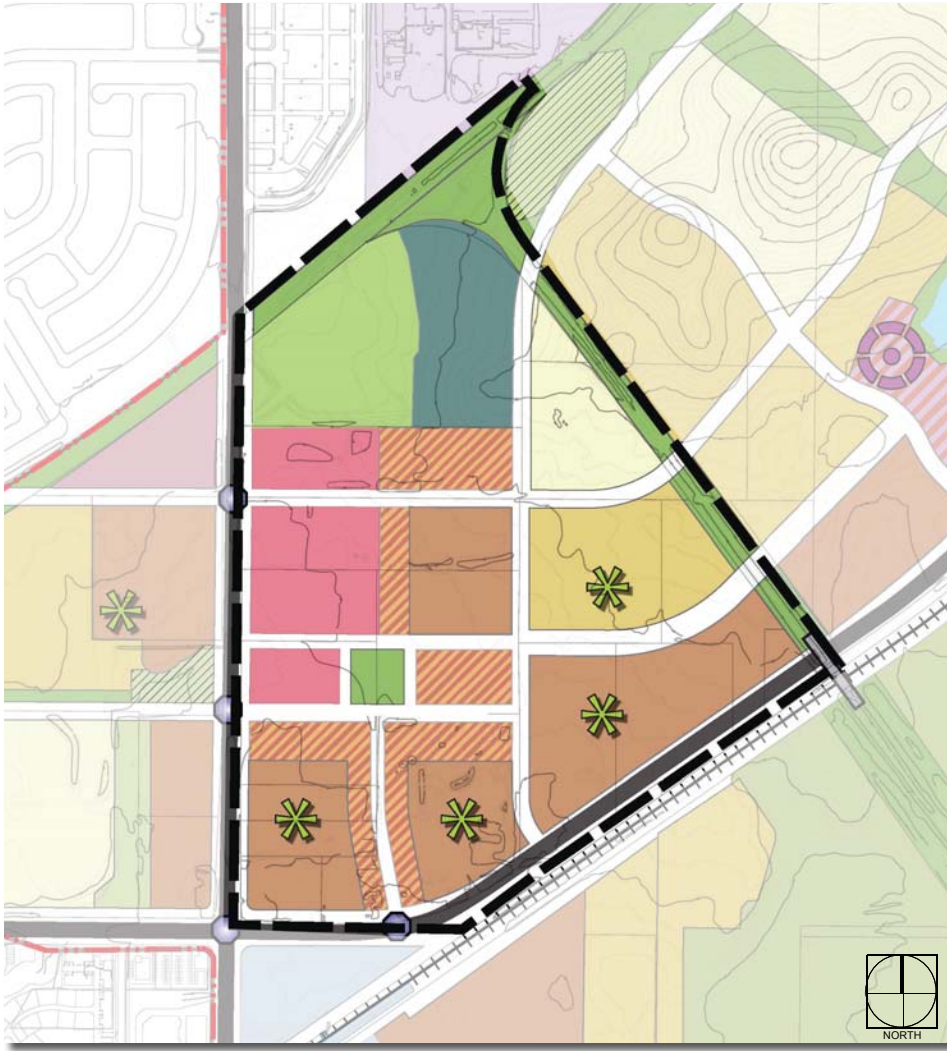


Figure 5-2: Town Center District, PA2

Land Use	Acreage	Dwelling Units
Residential		
Low Density Residential	8	56
Medium Density Residential	13	182
High Density Residential	39	1,092*
Mixed Use Residential		245
Non Residential		
Community Commercial	17	
Community Mixed Use	20	
School (K-6)	11	
Linear Park Trail	13	
Detention Basin	12	
Town Square	2	
Open space within Development	3	
Arterial Roads	17	
PA2 Total	155	1,575

Table 5-1: Town Center Land Use Summary

*HR units may be within HR or CM land uses; total unit number shall not be exceed

Key Mixed-Use Design Elements:

1. Minimum building setbacks along local streets to allow optimal plotting efficiency and a comfortable street walking environment.
2. Integration of ground floor commercial around Town Square
3. Integration of live/work and home based business along key walking streets
4. No exposure of parking lots or garages along key walking streets
5. Along residential edges of local streets, an appropriate landscape, courtyard and/or stoop treatment
6. Articulation of massing and rooflines on long buildings

5.3.2 Mixed-Use & Residential

Mixed-Use and high-density residential complete the land use program for the district. As noted above, the goal is to provide housing for as many residents as possible within walking distance of the community commercial center, the Town Square and the Train Station. In order to ensure compliance with this goal minimum densities have been set for parcels within the Town Center consistent with the low end of the land use density range for each parcel as shown on the land use plan.

To encourage a variety of building forms, economic development and flexibility for parcel owners, a wide variety of uses are allowed within the mixed use zone including commercial, live/work and residential. The city's land use regulations have been modified for these uses to allow a higher level of efficiency and optimal utilization of the land parcels.

Thematic Town Center streets (identified in Figure 10-1) are envisioned to be mixed use in character. Buildings facing or interacting with these streets are encourage to incorporate mixed use development, however only buildings directly facing the Town Square shall be required to include both residential and non-residential uses within a single building. Where portions of these street are developed as residential only, special care must also be given to the manner in which the buildings address the street at the ground plane. Raised stoops, small patios, landscape courtyards, porches and other devices must be used to create a friendly and inviting street character.

Should mixed use be pursued on portions of these streets not directly adjacent to the Town Square, several incentives may be provided to by the City to facilitate a successful economic result. City may consider:

1. Reduced processing and permitting time
2. Increased height and density
3. Reduced parking requirements
4. Reduced building setbacks

5.3.3 Train Station Connection

The Train Station anchors the south end of E Street and is one of the catalysts for activity within the Town Center District. Located at the intersection of Peabody and Vanden Road, it will serve the Capitol Corridor system and is scheduled for operation in 2014. The land use program for the Town Center is driven in part by the policy position that the area around the station be of a density of development appropriate to feed the station and provide an opportunity for residents and workers to be within walking distance of regional transit. On this basis, the Land Use Plan provides for a minimum of 3,000 dwelling units located within a one-half mile radius of the Station. Key design requirements of a successful Train Station relationship to the Town Center:

- Direct, safe and convenient walking access to the core of the Town Center
- Concentration of housing and jobs within a half mile distance
- Station operational prior to home sales within the half mile distance, especially within a quarter mile distance.
- Provision of support services such as food service and local retail close to the station



Figure 5-3: Conceptual perspective looking down street toward Train Station

5.3.4 Land Use Policies

In addition to the requirement to achieve land use consistency with the land use plan, the following policies constitute minimum requirements related to land use and density within the Town Center District.

Policy 5-1

Land use shall be consistent with the Land Use Plan Figure 1-1. A wide range of uses is allowed in the Town Center to allow flexibility and creativity on the part of each parcel owner. Variations may be proposed by landowners and considered by City through approval of the Master PUD Permit.

Policy 5-2

City shall encourage the development of residential parcels at or above “Target Density” for HR and CM areas within one-half mile radius of the Train Station. City shall have the right to not approve a density transfer which would result in development below the target density within the one-half mile radius.

Policy 5-3

PA2 may include a 2,500 square foot minimum “Neighborhood Center” adjacent to the elementary school site. If constructed, this “Neighborhood Center” would be owned and operated by the City of Fairfield and be used for after school programs and other recreational activities.

Policy 5-4

Development of the “CC” parcel shall include:

- A grocery market with a minimum floor area of 20,000 square feet within the “CC” parcel directly west of the Town Square
- An open space/plaza feature with a minimum area of 4,500 square feet

Policy 5-5

Development of the “Town Square” shall be:

- 1.75 net acres minimum
- Developed privately
- Public accessible
- Maintained by the HOA / Property Owner Association

Policy 5-6

CM development adjacent to the Town Square shall be restricted to non-residential commercial or community service uses on the ground floor. See Figure 5-3 for specific location, denoted in red dots.

Policy 5-7

CM development adjacent to streets B, D, E or the Civic Boulevard, as shown on Figure 5-8:



- Shall be permitted up to four (4) stories or 50 feet in building height where non-residential uses are designed on the ground floor
- Non-residential commercial uses are encouraged on the ground floor; their entries should face the street
- Shall be permitted to count street parking spaces directly in front of their parcel toward non-residential parking requirements
- Shall be permitted a ten (10) percent reduction in parking requirements

Policy 5-8

HR (High Density) residential development shall be permitted in the CM district as a single-use individual project or horizontal/mixed-use project.

Policy 5-9

Residential development in the HR land use:

- From 20.0 du/ac. to 24.0 du/ac. shall develop per the RH or RVH regulations in Table 25-6 of the Fairfield Zoning Ordinance.
- From 24.0 to 50.0 du/ac. shall develop per the HR standards adopted by the Train Station Specific Plan PD Overlay District.

Policy 5-10

Within PA2 all residential units in the CM land use, regardless of density, shall be developed per:

- The HR zone established by Train Station Specific Plan PD Overlay District adopted concurrently with this document
- At a “Target Density” of 10.0 du/ac.
- The policies and guidelines of this Specific Plan and Chapter 13.0
- Within the CM, HR developed as part of vertical mixed-use
- Shall be subject to “Dwelling Unit Allocation”. The City shall determine the Dwelling Unit Allocation for each property through the approval of the Master PUD Permit.

Policy 5-11

Within PA2 the following modified building setback development regulations shall apply to CC, CM and HR development along Main Street, D and E streets and the Civic Boulevard, as shown on Figure 5-8:

- Minimum 8 foot setback from PL to living space
- Reduced setback of 6 feet from PL to living space permitted for up to 25 percent of living space frontage
- Minimum 2-foot setback from PL to porch



Policy 5-12

Within the CM land use there shall be no restriction on combining compatible land use categories (residential with nonresidential) within the same building except as limited by the land use regulations of Table 25-9, modified permitted land use regulations of this Specific Plan, and any imposed by the State Building Code or other federal, state, or local regulations.

Policy 5-13

Within individual residential parcels, open space nodes shall be provided to meet the requirements of this document and the recreational open space needs of the residents. Refer to Section 9.0, Public Services of this document for specific guidelines, requirements and standards.

5.4 Urban Design Structure

The Town Center District urban design framework consists of a series of special thematic streets converging on the “Town Square” which is meant to be the “outdoor living room” for the district and overall community. This framework, and achieving a high level of open space, building and landscape crafting within it, is essential to achieve optimal market and economic performance for the planning area. See Figure 5-4 for more details on the urban design structure.



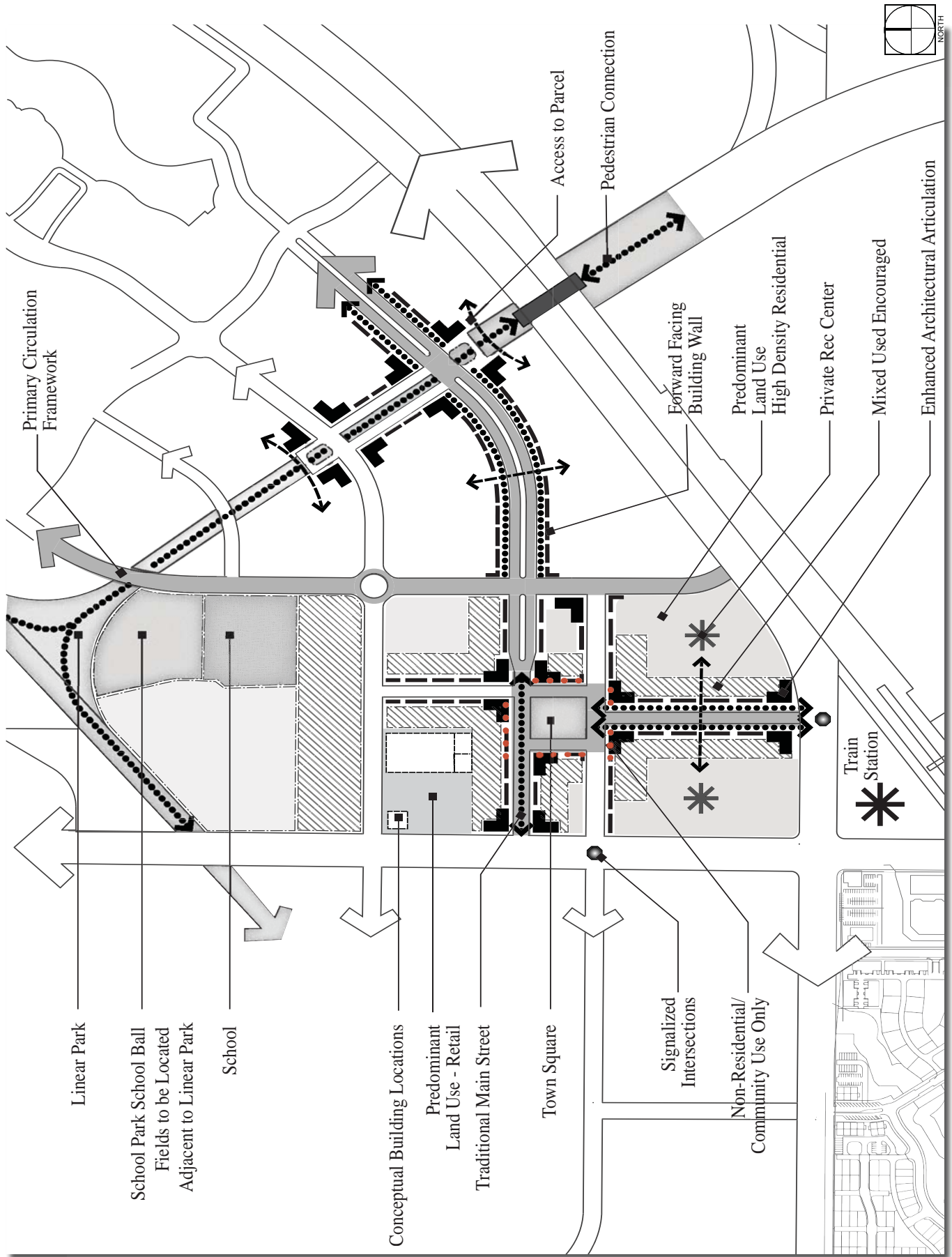


Figure 5-4 Conceptual Urban Design Structure

Key Main Street Design Elements:

1. Walking environments scaled for pedestrian use and lined with retail tenants.
2. Broad sidewalks shaded by tree planters
3. Buildings adjacent to walkways incorporating shop windows, street cafes, boutique signage and enhanced architectural detail
4. Shop entries oriented toward Main Street only
5. Pass-through arcades to allow access from parking
6. Parking on the Main street either parallel or angle
7. Maximum 2 travel lanes on the street except at major intersections
8. Minimum two story massing
9. Orientation at a right angle to a major arterial to allow retail visibility

5.4.1 Main Street

The Main Street is envisioned as the quintessential small-town shopping street with some parking located directly in front of the shops but most behind the stores. Shoppers will access the storefronts by walking through pedestrian arcades between buildings to the store entries which will be exclusively oriented toward the Main Street.

Although the location of parking behind the stores may seem initially less convenient than that a typical “strip center”, it has been demonstrated in many centers across the country that this arrangement increases pedestrian traffic in front of stores as well as creates a more inviting total shopping experience. The Main Street area, as well as the area around the Town Square, will be perceived as a “District” rather than a line of single stores. The district concept will encourage visitors to extend the duration of their stay because more shops, restaurants and services will be within easy and enjoyable walking distance once the car is parked and the visitor is within the district's pedestrian zone.

The Main Street is oriented at a right angle to Peabody Road so that views from the high trafficked arterial down the street to tenant frontage and signage is maximized. This intersection is a right-in, right-out only which allows good access from the city at large while limits the amount of traffic allowing the street to stay essentially two lanes in width.

It is important that the buildings along the Main Street be at least two story in height to create the appropriate sense of scale and spatial containment. This suggests that building area above the first story be professional offices, community services or residential units. See section 5.6.1 and Figures 5-8 and 5-9 for further detail.



Figure 5-5: Main Street provides comfortable pedestrian setting with strong building relation to the street, street trees and angled parking.

Key Town Square Design Elements:

1. Large usable area of at least 1.75 net acres
2. Surrounding uses to be commercial in nature on the ground floor
3. Buildings held tight against street setbacks to maintain comfortable walking environment
4. Ground floors across from Square to have primary tenant entries, glazing, blade signage and enhanced architectural detail
5. Buildings to be minimum two stories directly surrounding Square
6. Walkways and sidewalks to be tree lined for maximum shade
7. On-street parking around Square, either parallel or angle
8. Site and landscape plan for Square to allow a variety of programmable events and activities



5.4.2 Town Square

The Main Street leads to the central open space of the district, the Town Square. A formal civic space, the Town Square is envisioned as a venue for concerts, holiday events, fairs, festivals and simple strolling or afternoon picnics. The Town Square is linked directly to the Train Station allowing commuters to access the heart of the Town Center and associated retail, dining and other services. As in other traditional town squares, it is surrounded by a mix of commercial spaces, home-based businesses, live work opportunities, high-density residences and community facilities. This should ensure the space is activated and animated throughout the day and into the evening.

Similar to the Main Street the building massing around the Square must be at least two stories and preferable three to four. This is due to the size of the Square which is dictated partially by minimum offsets between surrounding intersections.

Also similar to the Main Street, ground floor uses must be commercial or community services in nature. Due to the distance from major arterial traffic and lack of visibility, the square is not envisioned as a primary retail destination but more for office and other services, however retail and restaurants are strongly encouraged. On the east and south sides of the Square, the primary land use should be high density residential with the ground floor of buildings facing the Square containing commercial or community uses. Buildings facing the north and west side also require ground floor commercial with the upper floor(s) being either office or residential.



Figure 5-6: Conceptual Town Center



Figure 5-7: Mixed-Use Buildings Line the Town Square

5.5 Architectural Character and Design Criteria



The Town Center will be a dynamic, mixed-use core including uses such as commercial, office and residential. As the major character icon and gathering place of the Fairfield Train Station Community, the Town Center buildings and streetscene require the highest level of consideration and detail in the non-residential design.

The Town Center will include streetscenes that resembles an idealized image of traditional main street blocks. Historically; the organic development of main street blocks involved the addition or replacement of buildings over an extended period by individual variety and spontaneity fostering pedestrian and business vitality.

Building design within the Town Center should be authentic interpretations of various traditional main street design vocabularies along with more contemporary interpretations. Ideally, an aesthetic combination of these elements will be included to reinforce the perception of a long-established core built over time.

Due to the wide variety of land uses and building types represented within the Town Center; the general guidelines of this section and the specific guidelines below will apply to all buildings within the Town Center, regardless of use, to foster a coherent and dynamic setting. Guidelines include:

- A visually stimulating design vocabulary patterned after small-town American main streets should be incorporated
- To the extent feasible; buildings should generally appear as a collection of individual small buildings, rather than a single uninterrupted large building, and building forms should appear to have been designed on a tenant-by-tenant basis, with varied facade treatments, and developed over time
- Multiple building heights and parapet/fascia treatments are encouraged
- Ground floors shall have storefront designs with large windows and articulated entries
- Elevations with tenant/visitor entries should incorporate glass
- Pedestrian-scale windows, features and massing shall be incorporated
- Building/tenant entries shall be articulated
- Horizontal definition between uses, generally between the first- and second-story, is encouraged
- Larger single-tenant buildings shall use massing offsets and other architectural elements to create visual interest in the building and reduce its overall mass
- Recesses and/or projections are encouraged to articulate the facade with light and shadow variation



- Balconies, pronounced window trim or recessed windows and awnings are encouraged on upper stories
- Design should be compatible to create an understandable and intuitive development
- Material and/or color variation is encouraged
- Multiple building heights and parapet/fascia treatments are encouraged
- Parapets, when used, should be contiguous and wrap building sides to reduce the “false front” appearance
- Chain businesses are encouraged to incorporate their logo/marketing image with the architectural vocabulary of the building/development parcel rather than the standardized logo/marketing building of the individual chain store

The following policies and criteria constitute minimum requirements related to architectural character and design within the Town Center District. Additional design guidelines and policies are located in Chapter 13.0, Design Guidelines, of this document.

Policy 5-14

Development abutting the Town Square shall be designed with:

- Nonresidential uses on the ground floor
- Front building facade oriented toward the Town Square

Policy 5-15

Parcels facing Main Street, D Street and the Civic Boulevard, as shown on Figure 5-8, shall maintain a minimum of 80 percent of the linear street frontage with buildings at the minimum setback or within 5 feet thereof to provide an architecturally dominant streetscene for the Town Center.

Policy 5-16

HR and CM parcels facing A, B, and E Street, as shown on Figure 5-8, shall maintain a minimum of 60 percent of the linear street frontage with buildings at the minimum setback or within 5 feet thereof.

Policy 5-17

Parcel frontages along “Thematic Streets” within PA2 (Main Street, D and E streets and Civic Boulevard), as shown on Figure 5-8, shall provide special architectural, massing and detail enhancement within the so called “20/20 Zone”, that is, the first 20 feet horizontally and 20 feet vertically from the street adjacent walkway.

- Surface parking or garages shall not directly front the street
- Front elevations for buildings that contain shops, offices and/or community facilities shall provide at least 50 percent surface area as glazing



- Accent paving (colored concrete, unit pavers, tile insets) shall be used on a minimum of 10 percent of sidewalk surface.

Policy 5-18

Building elevation and architectural style of HR and CM shall change at least once every 200 linear feet along a block face. Styles may be within the same genre.

Policy 5-19

Tenants, shops, offices and/or other uses with nonresidential uses on the ground floor shall have at least one (1) main public entry that faces the primary frontage street or public space adjacent to the building. For corner lots with more than one street frontage, the public entry may be oriented toward the corner. Non-service only entries into the rear of shops along Main Street are prohibited.

Policy 5-20

Within the length of a block, there shall be at least one (1) location where a one (1) story variation in building height occurs.

Policy 5-21

Single-tenant retail anchor buildings larger than 15,000 square feet shall:

- Use massing offsets on the horizontal plane at least every 60 feet to create visual interest in the building and reduce its overall mass
- Incorporate towers, overhangs, variable roof forms and/or other devices to create variation in the vertical profile and elevation

Policy 5-22

Recesses and/or projections shall be incorporated in any elevation visible from a public street to articulate the facade with light and shadow variation

Policy 5-23

Balconies, pronounced window trim or recessed windows and awnings are encouraged on upper stories

Policy 5-24

Within the length of a single block, the appearance of multiple buildings shall be achieved with the use of variations in style, height, massing, material and/or color. Parapets, when used, should be contiguous and wrap all building sides to avoid a “false front” appearance

Policy 5-25

Chain businesses are encouraged to incorporate their logo/marketing image with the architectural vocabulary of the overall development parcel rather than the standardized building design of the individual chain store.

5.6 Town Center Circulation

Transportation is a key driver for the Town Center District given its location between two major arterials and the incorporation of a regional commuter rail station. A goal of the Town Center District is to create a viable TOD with multiple transit options through the provision of residential and commercial density within a close proximity to the Train Station. The circulation system is planned as a balance between vehicular efficiency and pedestrian and bicycle friendly design with multi-modal pathways and a high level of connectivity between residential neighborhoods, commercial nodes and the Train Station. In addition the street pattern emulates the historic and efficient urban grid of traditional downtowns providing multiple routing and development efficiency due to the rectilinear block form.



Figure 5-8: PA2 Town Center Circulation

The circulation plan, Figure 5-8 provides multiple connections to Peabody and Vanden Roads to accommodate the volume of traffic entering the district. The number of full turn intersections and signals are limited due to the efficiency requirements of these major arterials. Lesser right-in, right-out intersections also provide flexibility in routing and bypass at peak periods.

Legend

- On-Street Bike Path
- Off-Street Bike Path
- Class 1 Bike Trail
- - - Multi-Use Trail

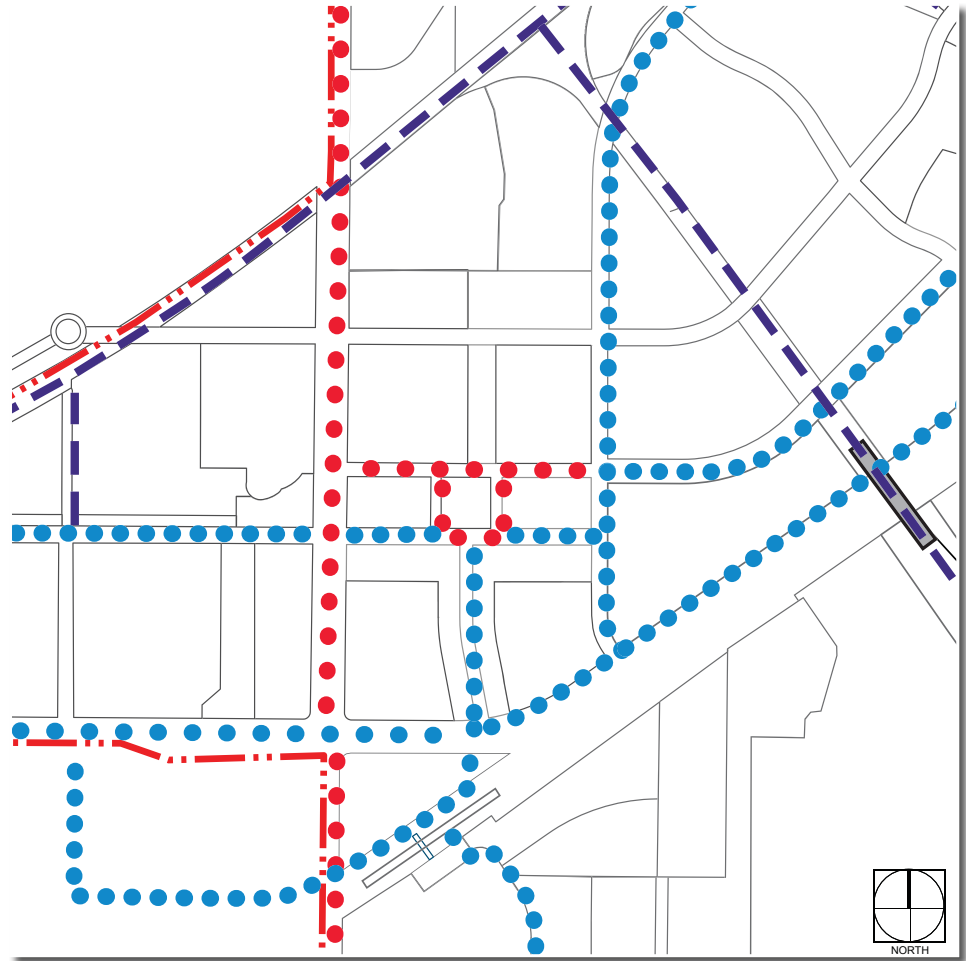


Figure 5-9: PA2 Town Center Bicycle and Pedestrian Plan

The circulation diagram for the Town Center District, Figure 5-9 illustrates circulation and access planned for the Town Center District as well as the primary entrances into the District and connections to the Boulevard and other parts of the plan. Figure 5-8 shows bicycle and pedestrian circulation through PA2 and connecting to the Civic Boulevard and Lake District of PA4.

Approximately 17 acres of land have been designated for streets and circulation within the Town Center District by the FTSSP Land Use Plan. Street sections within the Districts shall be in conformance with the Street Sections in Chapter 10.0 of this document. Landscape, walls and fences, street lights and hardscaping in the Town Center District shall be consistent with the community character and the standards and landscape design policies of Chapter 12.0, Landscape, of this document.

5.6.1 Thematic Streets

As an urban neighborhood, the Town Center has a specific design setting that is primarily based on street design and associated architectural framing. Several thematic streets have been incorporated into the urban design framework to create a more diverse, interesting and memorable urban neighborhood. The Main Street has already been discussed but several other streets are planned to provide a unique identity that will help ‘brand’ the surrounding neighborhoods. Figure 5-10 delineates Thematic Street locations within PA2. See section 13.4.6 for more details regarding urban neighborhoods. See Chapter 10.0 for larger sections and further detail.

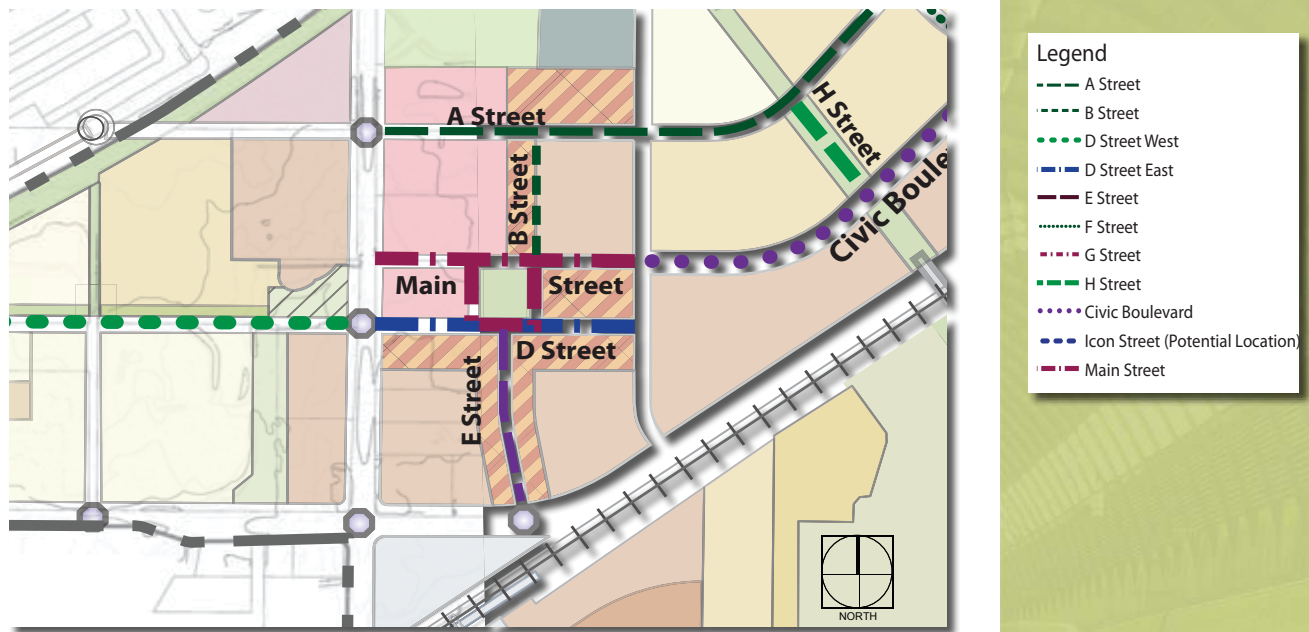


Figure 5-10: Thematic Streets Diagram

Main Street

The cross sections of the Main Street possibilities are illustrated below to demonstrate the intimate, pedestrian-scaled and retail dominant character of this central Thematic Street. See Figures 10-16 and 10-17.

Civic Boulevard

The Civic Boulevard is a landmark Thematic Street, connecting the Town Square and the Lake Park by providing a central walking promenade between two major activity centers. Framed by mid- and high-density residential, the character will be highly formal with tight setbacks, stoops, porches and small urban courtyards flanking tree lined walkways. The key feature is an extra wide landscape median in the center of the boulevard reminiscent of the grand boulevards found in turn of the century cities throughout California. The cross section illustrates the broad median for pedestrian and bicycle use, tight setbacks and architecturally dominant streetscape of this key Thematic Street. See Figure 10-15.



D Street

Thematic Street D transitions from Main Street to the Civic Boulevard, and Thematic Street E serve as the primary link between the Train Station and the core of PA2, the Town Square. These quasi-urban Thematic Streets that requires special treatment to foster the active urban setting envisioned. With tight setbacks, two- to four-story building heights and mixed use lining the ground floor the street epitomizes TOD design character. Tree grates, street furniture, accent paving and an emphasis on high-character architectural design are meant to create a pleasant and engaging backbone walking link through the district. See Figures 10-21 and 10-21.

E Street

The north/south pedestrian axis from the Train Station up to the Town Square through PA2 is established by Thematic Street E Street. This connection provides the opportunity for ground floor commercial uses, mixed-use, live/work and/or high density residential in close proximity to transit and the activities of the Town Center. Special design emphasis will be put on the paving, landscape and architectural character directly adjacent to the street pedestrian zone to set a pedestrian environment that supports the transit-oriented theme of this section of the Town Center. See Figure 10-23.

B Street

The north/south pedestrian axis through PA2 is continued by Thematic Street B Street. This connection also provides the opportunity for ground floor commercial uses, live/work and/or Home Based Business. It is transitional between the community retail center and residential neighborhoods to the east and has potential to be of a mixed use character. Special design emphasis will be put on the paving, landscape and architectural character directly adjacent to the street pedestrian zone. See Figure 10-20.

5.6.2 Circulation and Parking Criteria

The following policies and criteria constitute minimum requirements related to circulation and parking within the Town Center District. Additional design guidelines for each land use category can be found in Chapter 13.0, Design Guidelines, of this document

Policy 5-26

HR residential development within 600 feet of the Train Station parcel boundary shall be permitted to reduce parking standard requirements by 10 percent.

Policy 5-27

To meet the community needs for circulation, transportation and transit, PA2 streets:

- Shall generally be designed on a grid pattern
- Should have intersections occurring at regular intervals
- Should attempt to achieve block sizes no larger than 300 x 400 feet in size
- Shall limit curb cuts to reduce points of conflict between pedestrian/ bicycle and vehicles
- Should be designed with expanded sidewalks (see Street Cross Sections in Chapter 10.0 of this document).
- Shall include off-street bike-paths within the District

Policy 5-28

The development of individual parcels also has a strong impact on the circulation and transit access within the District. To promote internal multi-modal connectivity, access to transit and a walkable neighborhood, the design of individual parcels:

- Shall include through streets (in addition to Figure 10-1 streets) in a grid pattern to achieve desired block size as necessary
- Shall provide racks for bicycles
- Shall provide sidewalks on at least one side of all internal private drives, except alleys.

Policy 5-29

Parking areas shall generally be located to the rear of the building and shall not allowed in front of the building between the street and building face except for on-street parking. Parking locations and configurations designated on Figure 5-4 and parking in relation to the grocery store shall be exempt from this policy.

Policy 5-30

Buildings that do not directly front a street directly shall have at least one primary entrance that adjoins a pedestrian walk. The primary entrance should be connected to the street by a walkway that is clearly defined and separated from parking areas.

Policy 5-31

Surface parking lots with more than 150 spaces shall provide a pedestrian walkway, separate from vehicular access or parking, which connects the building's principle entrance with adjacent public street.



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6.0 LAKE DISTRICT PLAN



6.0 LAKE DISTRICT PLAN

July 26, 2011



6.1 Overview and Vision

The Lake District, Planning Area 4 (PA4), will be the centerpiece of the community, creating a vibrant, mixed-use district organized around an approximate 22-acre Lake Park. The Lake District will have a mix of high, medium and low density residential integrated with community-focused services and amenities tied together by the lake and its surrounding trails and open space. The creation of a strong public realm, provision of local services and a higher density of residential surrounding these elements to enhance pedestrian accessibility forms the basis of the urban design structure of the Lake District.

The Lake Park will be a unique amenity within Fairfield, offering a setting for trails, picnicking, day use areas, and small craft, non-motorized boating. The park area around the lake shall be designed to have activity nodes every 300 feet to help create an engaging public realm. The residential uses around the lake park shall be oriented towards the lake with the residential pads to the northwest of the lake terraced to take advantage of view premiums. The Lake Park will be part of an overall pedestrian system that connects the train station and the Town Center to neighborhoods in the northern portion of the community.

To ensure the distinctive place-making nature of the Lake District, specialized regulations have been established to guide development. This section of the Specific Plan shall serve as the regulating document for the Lake District (PA4). It establishes an additional level of specific principles, form, and land use regulations as well as design guidelines to ensure the open space and built form of the District meets the goals of this the FTSSP.

Figure 6-1 shows the Lake Park Illustrative, and Figure 6-2 shows the Lake District Concept Perspective.

6.2 Lake District Planning Principles

The framework of the Lake District is based on the planning principles described below. The dedication of approximately 22 acres to the City to create a public Lake Park will not only add greatly to the value of the residential uses in the District but also elevate the quality of life of residents by providing recreational amenities and access to local services. The Master PUD Permit for, and any individual Site Plans within, PA4 shall promote development that meets the intent of these principles:

6.2.1 Land Use

- Aggregate the local serving retail uses and community amenities, including private recreational facilities, to create the critical mass needed to develop a village core at the south end of the Lake
- Provide a variety of housing densities around the Lake Park
- Allow for a variety of housing types and residential configurations
- Orient the visibility from residential units located around the Lake Park towards the lake to enhance the quality of the public realm
- Locate retail uses that need high visibility along Vanden Road
- Organize the retail uses and the library around the Village Square
- Encourage paseo connections from interior neighborhoods to the Lake Park to enhance connectivity



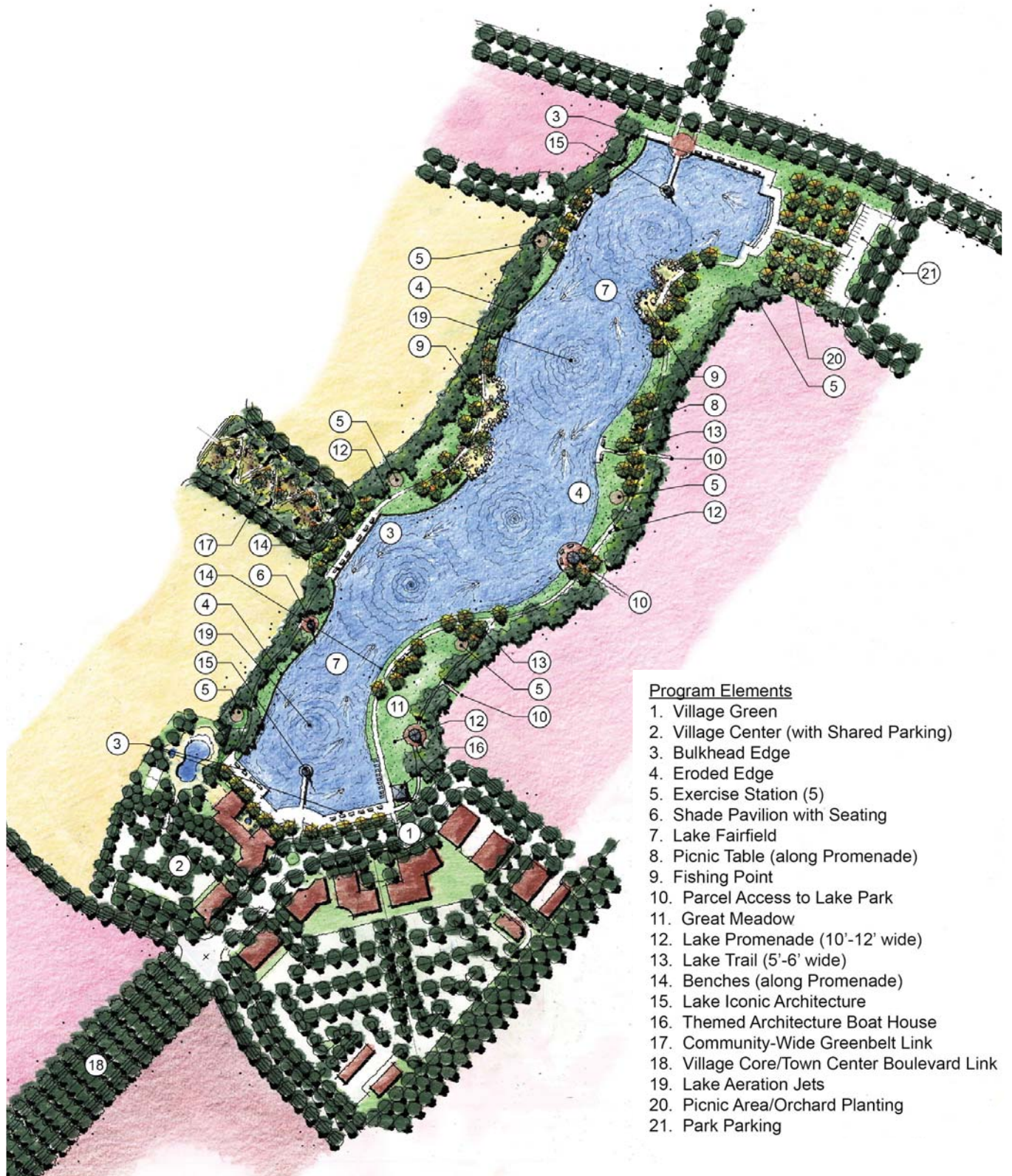


Figure 6-1: Lake Park Illustrative



6.2.2 Urban Design

- Locate Village Core at the terminus of the Civic Boulevard at the south end of the lake to strengthen the relationship between the Town Center and Lake District
- Use the Lake Park as the link for pedestrian connectivity between the Town Center District and neighborhoods to the north
- Use the Lake as a visual terminus for the streets and paseos, where possible
- Locate the high density residential uses along Vanden Road
- Terrace residential pads northwest of the Lake Park to respect the existing grade of the site and to take advantage of views to the Lake
- Vary building heights, setback and massing along the Lake Park to create architectural interest in the building wall
- Design streets, pathways and paseos that create a walkable community; pedestrians are the highest priority

6.2.3 Architecture

- Enhance architectural articulation of buildings at key intersections and around the Village Square
- Visually integrate buildings through the use of coherent architectural themes, elements, and complementary materials
- Articulate architectural facades for buildings facing community paseos
- Wrap the architectural treatment around street corners to help enhance streetscape
- Where possible utilize garages or building walls as sound barriers from traffic noise on Vanden Road
- Articulate wall planes on buildings to create visual variety along the streetscape
- Allow flexibility in the design and layout of residential buildings

6.2.4 Landscape

- Incorporate pathways, plazas, seating areas, and community elements to help activate the Lake Park
- Create amenity nodes at regular intervals along the Lake Promenade
- Use the lake in a limited basis for storm water management, consistent with its principle role as a recreational and visual amenity
- Provide street trees on all streets within the district
- Provide bio-diversity as a part of the landscape palette
- Select tree species that maximize shade and reduce urban heat island effect



Figure 6-2: Lake Park District Concept Perspective



6.3 Lake District Land Use

To ensure that the Lake District achieves its policy and design goals, particular attention is required in its design and execution. Design guidance shall come from two separate sources: 1) the City of Fairfield Development Regulations (separate from this document), and 2) the Design Criteria (listed below and in Chapter 13.0 of this document).

Land uses within the Lake District shall be consistent with the application of the Zoning Code except where modified by this Specific Plan. Where the FTSSP is silent on a standard, in this section or the Specific Plan, the Zoning Code shall apply in its entirety.

Lake District land uses are identified on Figure 6-3, and are summarized on Table 6-1. Although long-term build-out of the Lake District requires these structured regulations for continuity, it also necessitates flexibility to respond to the evolving design, building and economic demands. A reasonable amount of flexibility is embedded in the guidelines to allow design creativity and land use, density and market response. Modifications have been made to the City of Fairfield zoning application, as administered by this document, to balance long-term continuity with a dynamic market.



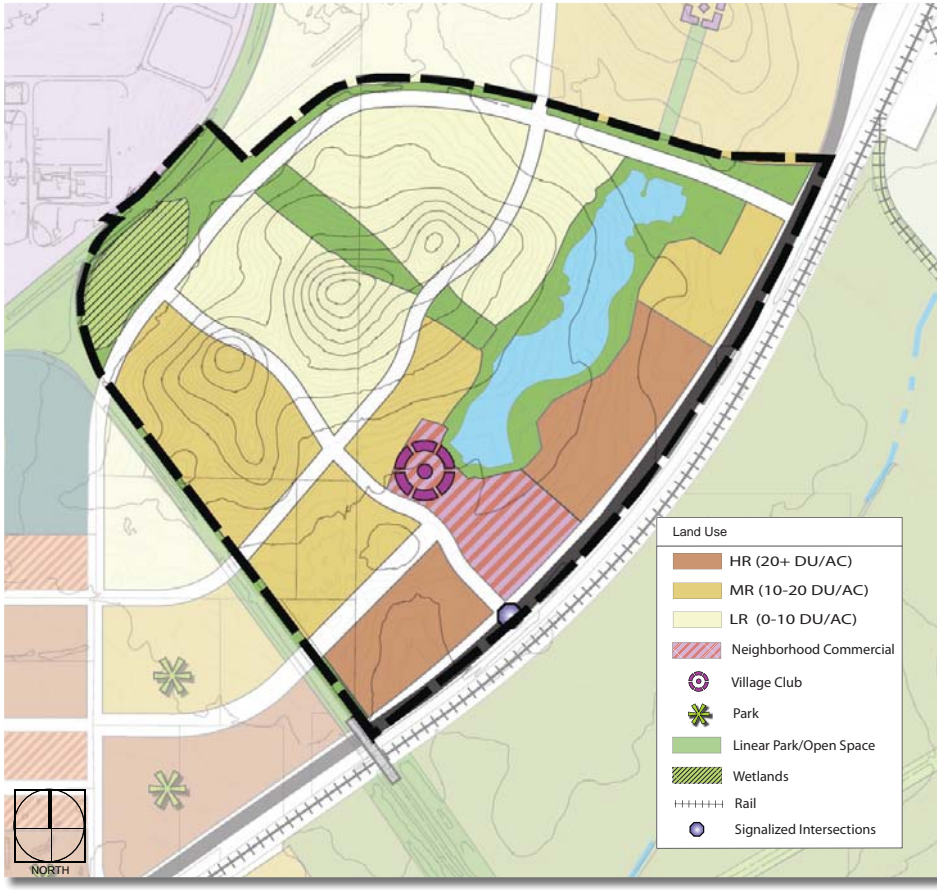


Figure 6-3: Lake District, PA4

Land Use	Acreage	Dwelling Units
Residential		
LR - Low Density Residential	47	329
MR - Medium Density Residential	42	592
HR - High Density Residential	27	743
Non Residential		
CN - Neighborhood Commercial	10	
Lake Park	20	
Linear Park Trail	2	
Open Space within Development	17	
Wetlands	5	
Arterial Roads	14	
PA2 Total	184	1,664

Table 6-1: Lake District Land Use Summary

6.3.1 The Village Core

Located at the south end of the lake, the Village Core will serve as the anchor for the Lake Park, as well as serve as the terminus to the Boulevard linking the Lake District to the Town Center. As a complement to the retail in the Town Center the Village Core provides a modest addition of local serving retail. The retail uses may include cafes, a fast food restaurant, a gas station and other local services. The village club, the library and the retail uses organized around a village square provide the critical mass needed to create a viable activity destination (Figure 6-4, Village Core Perspective).

The Village Core has been designated a Neighborhood Commercial land use (CN), that allows for a variety of uses including retail, neighborhood services, and small offices. Approximately 10 acres of CN have been designated by the FTSSP Land Use Plan. The development within the CN shall be limited only by the development standards and requirements of this document and the applicable Zoning Code. Key design policies for Village Core in the Lake District include:



Figure 6-4: View looking across the lake toward the Village Core; Civic Boulevard leads into the Village Core

Policy 6-1

Retail uses and the library shall be designed around the Village Square to help create a sense of place.

Policy 6-2

A retail pad shall be provided near the signalized intersection with Vanden Road, to promote strong visibility and vehicular access.

Policy 6-3

Parking for the retail uses and the community facilities, shall be located behind the buildings (not adjacent to the Lake frontage and promenade) to maximize building orientation of the Village Square. Primary entries to businesses and other uses shall relate to the Village Square and/or promenade.

Policy 6-4

Direct pedestrian linkages shall be provided:

- Through the Village Core connecting the west and east sides of the Lake Park; such pedestrian connection shall have a minimum width of 50 feet and shall abut the Lake.
- Through the Village Core connecting with the Civic Boulevard; the Village Core landowner shall dedicate to City a public access easement for these linkages at the time of the subdivision map creating the Village Core parcel(s).

Policy 6-5

A traffic circle or other design element shall be provided on axis with the entry street into the Village Core to serve as visual terminus.

Policy 6-6

Enhanced architectural articulation shall be provided around the Village Square and at the terminus of the Boulevard.

Policy 6-7

Non-residential buildings in the Village Core shall be designed with equivalent architectural detail on all sides.



6.3.2 Residential Uses

The residential uses within the Lake District should be designed to provide strong, pedestrian connections to the Lake Park and the Village Core. A wide variety of housing types and densities will surround the Lake Park catering to a broad demographic spectrum with diverse needs and income levels (Figure 6-5, Residential Perspective). Following the density transect concept the highest density of residential has been located adjacent to Vanden Road and in close proximity with the Village Core with density gradually dropping further away from the core.

Low Density Residential (LR)

Located in the northwest quadrant lower density neighborhoods will be terraced down the hillside to capture Lake views. A community paseo will connect these residential neighborhoods to the Lake Park. To provide a variety of housing types adjacent to the lake a parcel of LR has been proposed on the northern edge of the Lake. The low density residential (LR) zoning category will mostly consist of single-family detached and small lot detached homes. The neighborhoods will be based on traditional neighborhood crafting elements with curvilinear

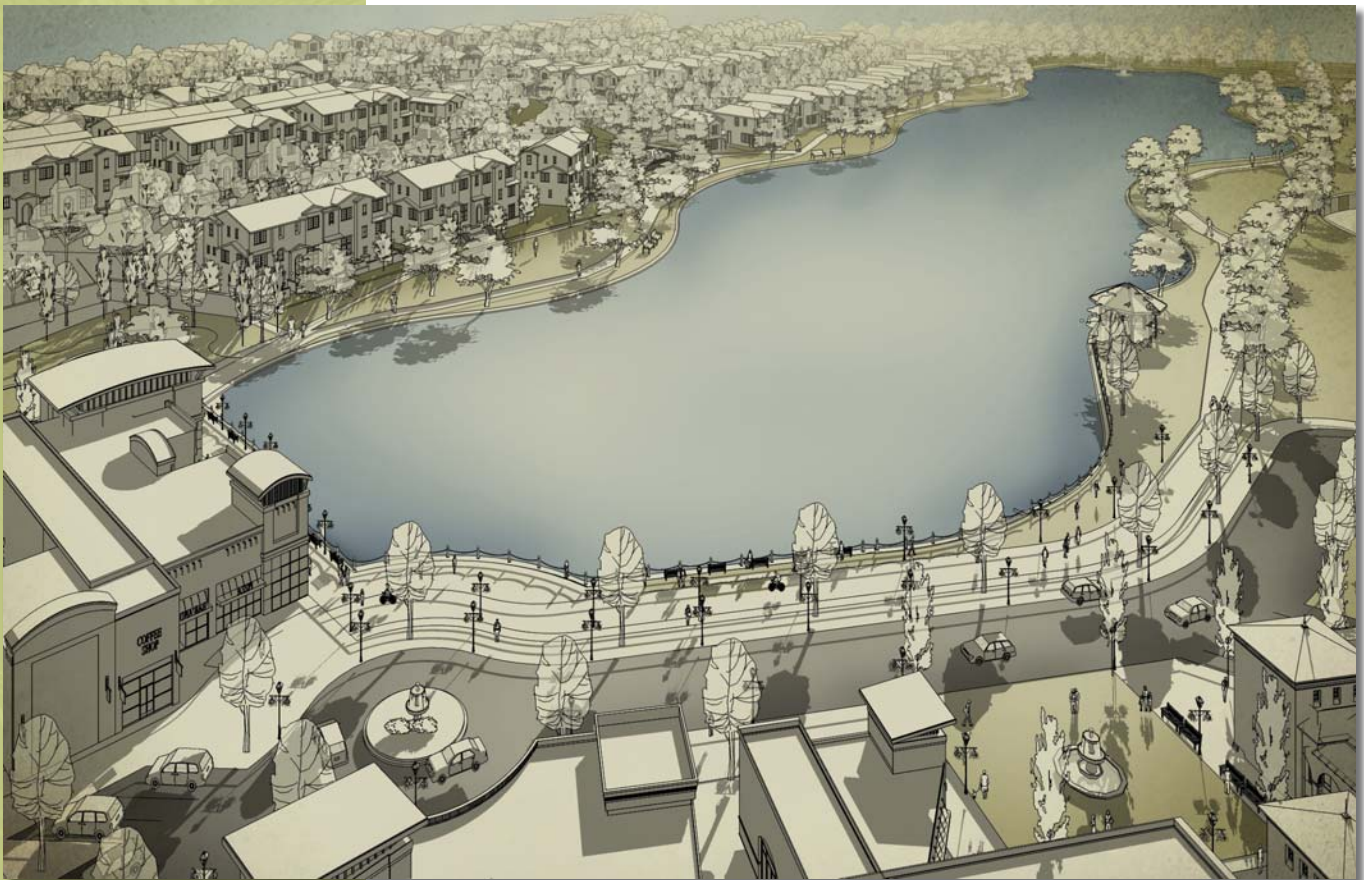


Figure 6-5: Lake Park surrounded by Village Core and residential on the far side

streets and view corridors. Architecture, massing, and streetscape design should revive the classic architectural styles of the past with a focus on quality materials and elevations. Approximately 47 acres of low-density residential (LR) neighborhoods has been proposed within the Lake District.

Medium Density Residential (MR)

Medium-density residential (MR) will be located in the southwest quadrant of the Lake District. This location puts this mid density zoning category close to the Village Core as well as adjacent to the Town Center District allowing increased accessibility to urban services. Architectural character and massing should complement the iconic architecture of the Village Core to create a thematic activity node. Approximately 42 acres of medium density residential (MR) neighborhoods have been proposed by the FTSSP Land Use Plan within the Lake District.

Policy 6-8

For MR and HR parcels abutting the Lake Park, a portion of their required, private open space shall be dedicated to City as public park such that there is a minimum width of 50 feet of park land around the lake. The total area of the Lake Park, with the addition of this private open space shall be 22 acres.

High Density Residential (HR)

Approximately 27 acres of high-density residential (HR) neighborhoods have been proposed by the FTSSP Land Use Plan within the Lake District. These neighborhoods will be located near the Village Core to maximize proximity to local services. HR has also been proposed along Vanden Road as the 3-4 story buildings associated with this density may help in shielding the rest of the community from the traffic noise from the Vanden Road. Higher density homes may include townhomes, different configurations of green-court and auto-court homes and apartments. General HR policies are addressed in section 4.5.4, High Density Residential (HR); in addition to those policies the following are HR policies applicable within PA2.

Key design policies for residential uses in the Lake District:

Policy 6-9

Roof forms and building massing shall be varied along the Lake Park frontage residential units to avoid the development of a flat, nondescript building wall. See Chapter 13.0 for further details.

Policy 6-10

Paseos and green courts in neighborhoods adjacent to the Lake Park shall connect to the Lake Park to provide pedestrian connectivity from interior units.





6.3.3 Land Use Policies

Policy 6-11

LR and MR development northwest of the Lake shall be designed as terraced neighborhoods to maximize views to the Lake. See Chapter 13.0, Design Guidelines for additional terracing/tiered neighborhood requirements.

Policy 6-12

A conceptual grading plan for PA4 shall be submitted by landowner concurrently with PA4 Master PUD Permit. The grading plan shall be designed to accomplish the design objectives of this chapter.

Policy 6-13

Noise attenuation from the railroad and Vanden Road shall be provided, consistent with the adopted policies of City's General Plan.

Policy 6-14

The PA4 Master PUD Permit shall include a conceptual land plan for a "CN" neighborhood commercial area of approximately ten (10) acres. The CN parcel shall include:

- A minimum 3.5 acre site for the Village Club, a privately owned and accessed recreational facility
- A minimum 1.5-acre site for a public library building consistent with the Solano County Library Facilities Master Plan
- Approximately 30,000 square feet of potential local serving commercial, which may include a café or restaurant and a service station
- The local serving commercial and library shall be organized around a Village Square

Village Club Program

3.5 acres net

15,000 sf clubhouse

- Fitness center
- Meeting room
- Kitchen
- Business center
- Game Room
- Entertainment room

Great Lawn

Lap pool

Spa

Kids pool

Totlot

2 Tennis Courts

Parking



Policy 6-15

The ‘Village Club’ designated on Figure 6-1 shall be owned and operated by the Homeowners Association for PA4 and PA5. The type and size of the Village Club facilities shall be determined by City through approval of the PA4 Master PUD Permit. The features of the Village Club facility shall be comparable to the example shown below.

Policy 6-16

Inclusion of a gas station within PA4 shall require a buffer of 300 feet or more between a large gasoline station and CN or residential buildings. See Project EIR for additional mitigation measure standards required for large gas station facilities.

Policy 6-17

The Lake shall be surrounded in its entirety by a pedestrian multi-use “Promenade”. The following standards shall apply:

- 200 foot minimum dimension of the Lake at any given point
- “Promenade” park element shall vary in width with a minimum width of 50 feet
- “Promenade” shall include appropriate signage and lighting.

The intention of this requirement is to provide a true active pedestrian space that is fully usable for the entire length. Refer to Figure 6-1 for the Lake Park Illustrative.

Policy 6-18

The community paseo to the north of the Lake shall connect directly to the Lake Park Promenade providing continuous pedestrian linkages from terraced neighborhoods to the Lake and Village Core.

Policy 6-19

Amenities shall be provided every 200 to 300 feet of the “Promenade” to create active engagement along the pathway. Amenities may include but are not limited to:

- Seating
- Exercise equipment/informational boards
- Dock/pier
- Fishing point
- Plaza
- Gazebo
- Play equipment
- Special landscape feature
- Other amenities administratively approved by the Community Development Director



6.4 Urban Design Structure

The Lake District derives its design inspiration from grand, hill-nestled villages where all streets lead to the lake. This center of the village is where festivals occur, farmers markets offer seasonal delights along neighborhood retail sidewalks, and the center of the village is just down the short stroll from your home. This nostalgic idea has been wrapped into the Lake District, serving as a village core for the FTSSP. The urban design framework for this 184-acre Lake District creates a series of neighborhoods of varied densities around the Lake Park, providing maximum accessibility to the highest number of residents.

6.4.1 Lake Park

At the heart of the Lake District is the Lake Park (see Figure 6-6). Approximately 22 acres in size, the Lake Park contains an 11 acre lake and roughly 11 acres of park space. The strong linear design of the Lake maximizes lake frontage. The Village Core that anchors the Lake Park at the south end is the destination for the trail that wraps around the lake. The trail around the lake will be well used as it follows the desire lines for pedestrian movement within the project - the trail connects the northern end of the project to the Village Core and the Boulevard extending into the Town Center. The Lake is primarily designed to be a visual and recreational amenity, but will have a limited role in functioning as a detention basin for peak storm flows. The Lake will have about 1-foot of freeboard for storm water detention.

The park element that surrounds the entire Lake will widen in places to form passive gathering places and pinching down in others for a stronger relationship between the Lake and adjacent neighborhoods. The lake edge is designed to take on a different character in different sections of the Lake Park. A hard edge is proposed near the Village Core with plazas, seating areas, and bollards giving the area an urban feel, while areas to the north have a softer, more organic edge that is more compatible with the residential neighborhoods. Key policies for the Lake Park include:

Policy 6-20

A minimum of 4 foot drop in elevation from the edge of the residential around the Lake Park and the surface of the water shall be observed to maintain privacy and Lake view for the surrounding units.

Policy 6-21

Parking lots shall be shielded from view from the Lake Park by buildings or other landscape treatments.

Policy 6-22

Parking shall be provided at the both the north and south end of Lake Park to encourage the use of the park by residents from different parts of the City of Fairfield. The parking at the south end can be shared with other uses within the Village Core.



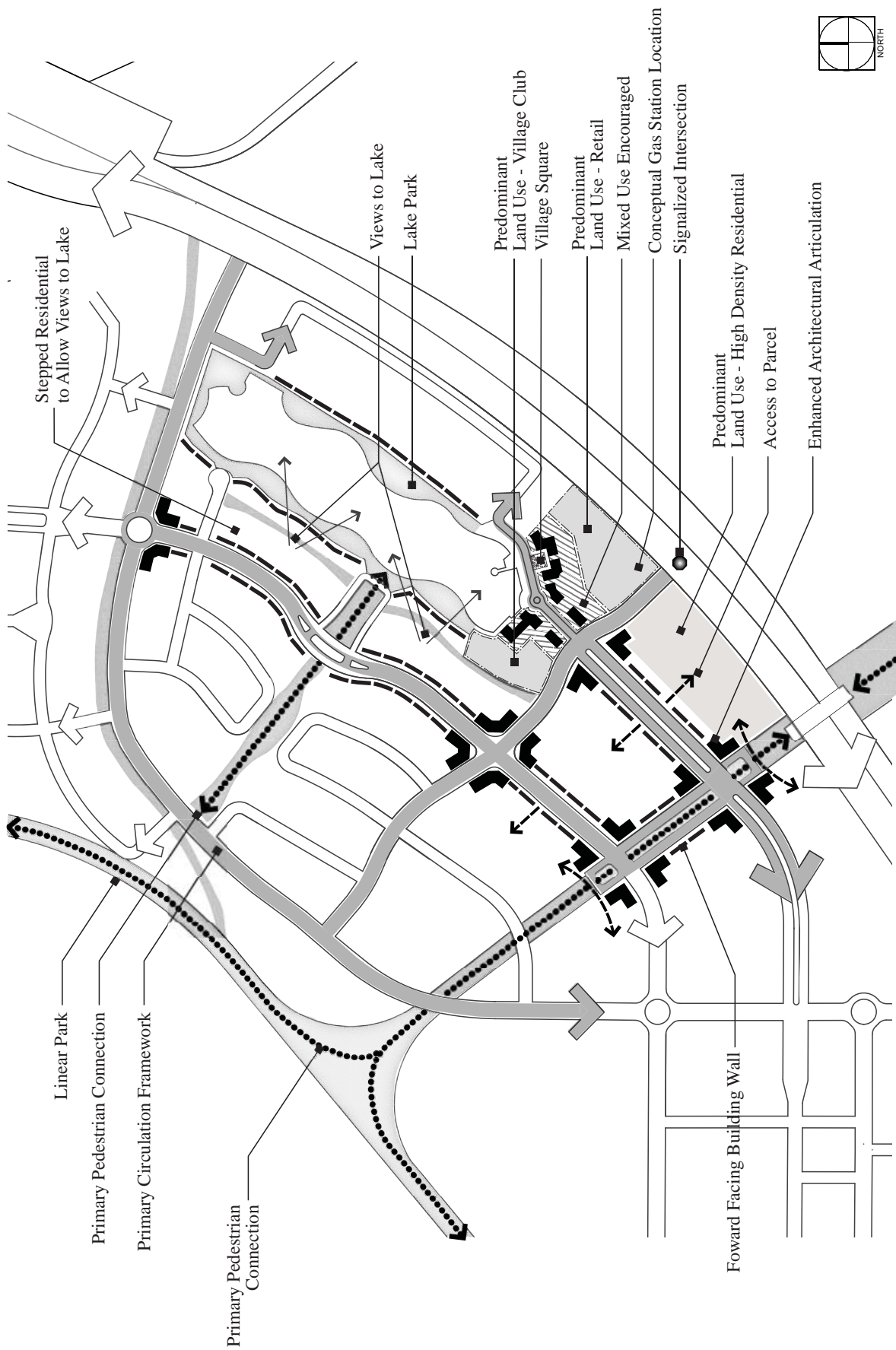


Figure 6-6: Lake Park District Urban Design Diagram



Policy 6-23

A transitional design zone shall be provided between the park and the adjacent residential uses. This transitional zone shall:

- Be a minimum of 15 feet in width
- Be contained wholly within the builder parcels
- Result in a minimum 4-foot vertical distance between an abutting residential pad and the park land elevation with the residential pad being higher
- Be landscaped with solid groundcover and shrub species in a manner restricting public access onto builder parcels
- Privacy, patio and/or boundary walls along the transitional zone shall not exceed 3 feet in height

Policy 6-24

The Lake Park shall be designed and constructed substantially in accord with the concept plan shown in Figure 6-1. The approximate acreage for the Lake Park, including water feature and surrounding park land shall be 22 acres.

Policy 6-25

A Lake Park Master Plan shall be prepared by landowner and submitted concurrently with the Master PUD Permit application. The Lake Park Master Plan shall include (a) landscape plan, (b) trail plan, (c) types and locations of any recreation facilities and icon architecture, (d) description of planned recreational facilities, (e) lighting plan, and (f) lake management plan, which will address operation of lake for storm water detention, recreation and water quality.

Policy 6-26

The lake and related drainage improvements shall be created with the initial grading in the planning area. The elevation of each abutting residential lot shall be a minimum of 4 feet above the elevation of the Lake.

Policy 6-27

The phasing of the landscaping, iconic architecture, amphitheatre, loop trail and similar features shall be determined by City through approval of the Master PUD Permit for PA4.

6.4.2 Village Square

The local serving retail and the library will be organized around the Village Square. The idea is to create a formal civic space that can be used for people gathering and picnicking. There will be no buildings placed along the edge of the Village Square that faces the Lake to maintain a strong visual link between the square and the Lake. The design of the Village Core shall ensure that the Village Square is shielded from the prevailing wind through the use buildings or other

landscape elements. Similar to the Town Square, the Village Square should have an enclosed feel created by the building mass around the square, the buildings located around the Village Square should generally be 2 stories in height, but one story elements may also be permitted as long as the height of the building is a minimum of 15 feet. There is no street proposed between the square and the buildings around it, this is done to help maintain the intimate scale of the Village Square. A lake front pedestrian promenade shall be created along the lake edge across from the Village Square (Figure 6-7, Village Core Design Diagram).



Figure 6-7: Village Core Design Diagram

6.5 Architectural Character and Design Criteria

Daily needs of Fairfield Train Station Community residents will be met by businesses and services in a coherent and well-designed neighborhood commercial development. These commercial spaces will be of a pedestrian-scale and may include such diverse businesses as florists, personal services, gas stations, food services, laundry services, independent businesses or offices, and financial branches. Accessibility, pedestrian scale and site/building design are of great importance to the success and suitability of the neighborhood commercial developments.

Guidelines include:

- To the extent feasible, buildings should generally appear as a collection of smaller individual buildings rather than a single uninterrupted large building including:



- Building forms should appear to be on a tenant-by-tenant basis, having either varied facade treatments or facades with unified design, materials and color palettes with varied massing
- Varied massing with unified design, materials and color palettes should be used
- Design should be compatible to create an understandable and intuitive development
- Building/tenant entries shall be articulated
- Elevations with tenant/visitor entries should have glazing
- Pedestrian-scale windows, architecture features and massing should be incorporated
- Wall plane offsets are encouraged
- Material and/or color variation is encouraged which may include:
 - Multiple building heights and parapet/fascia treatments
 - Parapets, when used, should be contiguous and wrap building sides to reduce the “false front” appearance
 - Projections or architectural elements to create sheltered pedestrian areas are encouraged
 - Faux glazing on upper stories may be incorporated to create the impression of active elements

6.5.1 CN Architectural Policies

The following policies and criteria constitute minimum requirements related to architectural character and design within the Lake District. Additional design guidelines for each land use category can be found in Chapter 13.0 of this document.

Policy 6-28

Neighborhoods developed adjacent to the Lake Park are encouraged to design site plans to orient homes to front onto the public open space whenever feasible/applicable to the housing type.

Policy 6-29

Edge/view lots within terraced neighborhoods shall feature view fencing to increase the visual impact of the terraced hillside.

Policy 6-30

Along Street G, the “Main Street” within the Lake District, additional guidelines include:

- Buildings shall be oriented at or near the back of the sidewalk
- Buildings should form a continuous edge that gives definition and scale to the street and should be treated architecturally to accentuate

building entries, pedestrian connections, courtyard/plaza spaces or outdoor seating/eating areas

- Buildings shall be oriented to frame the corner of an adjacent street intersection
- Off-street parking areas should be framed and enclosed

Policy 6-31

Major building and tenant entries should be located to front onto the main pedestrian street frontage. Larger retail stores or commercial tenants should be allowed to have a secondary entrance(s) from an off-street parking lot,

Policy 6-32

MR and HR residential uses adjacent to the CN land use shall be architecturally designed to complement the site plan and architecture of the Village Core.

Policy 6-33

A specific architectural style is not designated for the commercial portion of the Lake District, however a single or mix of styles is encouraged based on the historic architecture of the region. The overall architectural intent is of a human scale, walkable village similar to that found in small northern California towns during the first half of the century.

Policy 6-34

Buildings should appear as a collection of smaller individual buildings, or village, rather than several large buildings

Policy 6-35

Building elevations shall appear to be on a tenant-by-tenant basis, having varied facade or massing treatments

Policy 6-36

Pedestrian-scale windows, architecture features, signage, detail and massing should be of a scale and complexity depicting a village character

Policy 6-37

Multiple building heights should be achieved with at least one building in a cluster being two story Parapets, when used, should be contiguous and wrap building sides to avoid a “false front” appearance

Policy 6-38

Projections, awnings, overhangs and/or arcades should be provided on at least 65% of retail store fronts to provide shelter from inclement weather Faux glazing on rear elevations and upper stories shall be incorporated to create the impression of active elements on otherwise blank walls in excess of 60 feet in length.





6.5.2 CN Development Standard Policies

The following policies shall apply to the CN development within PA4:

Policy 6-39

Within the CN designation, the following modified development standards shall apply:

- Non-residential FAR of 0.7 shall be permitted
- Building height shall not exceed four (4) stories to a maximum of 50 feet.
- Iconic architectural features shall be permitted to a maximum height of 65 feet.
- 0 foot minimum setback from front facade to street PL
- 15 foot minimum setback from side facade to street PL
- 10 foot minimum to interior PL, except where adjacent to the same land use then 0 feet minimum shall be permitted
- 15 foot minimum building separation between non-residential buildings
- 25 foot minimum building separation is encouraged when separation will be utilized as a pedestrian connection.
- Buildings located at street intersections shall adhere to minimum street PL setbacks for at least 60 percent of the building facade minimum lot size and dimension regulations may be waived at the administrative discretion of the Director for any nonresidential development, including property that is residential/commercial mixed-use within the Specific Plan. This waiver shall not require a CUP.
- Minimum 6 foot landscape area shall be provided around any/all buildings, except where sidewalk or pedestrian path is provided, then landscaping shall not be required.

Policy 6-40

The following development standards shall apply to the “Village Square” :

- Net usable area shall be between 0.75 – 1.0 acre.
- Shall be privately developed
- May be a public park or private recreation area owned by property owner association
- Shall be HOA maintained regardless of public/private access
- Shall be shielded from the prevailing wind through the use buildings or other landscape elements.
- Should be surrounded by commercial uses and/or community facilities

- Surrounding buildings shall at least be two (2) stories OR one (1) story with a minimum 15 foot building height
- Generally building setbacks shall be paved to allow for easy access and pedestrian circulation.
- Buildings sharing a PL with the Village Square shall be developed with non-residential uses on the ground floor.
- Non-residential ground floors facing or directly across from Village Square shall have primary tenant entries, glazing, and enhanced architectural detail.

Policy 6-41

Walkways and sidewalks shall be tree lined to provide shade per the standards of Chapter 12.0, Landscape.

Policy 6-42

Master PUD Permit, site plans and landscape plan for the Village Square shall provide for a variety of programmable events and activities subject to approval by the Director.

Policy 6-43

The quality of the pedestrian environment should be activated by form and landscape including:

- Architecturally vibrant storefronts with bay windows, awnings and arcades
- Benches and planter walls for seating opportunities
- Fountains, murals, public art as focal objects
- Accent or festive lighting to enhance nighttime ambience

Policy 6-44

Building setback requirements from interior property line(s) abutting residential zone or use (of Zoning Code Table 25-10 Section 25.22.3) shall apply per land use except where residential uses are incorporated into horizontal mixed-use development.

Policy 6-45

Buildings with nonresidential uses on the ground floor shall have at least one (1) main public entry that faces the primary street or public space adjacent to the building. For corner lots with more than one street frontage, the public entry may be oriented toward the corner.

Policy 6-46

Buildings that do not directly front a street shall have at least one (1) primary entrance that adjoins a pedestrian walk. The primary entrance should be connected to the street by a walkway that is clearly defined and separated from parking areas.



Policy 6-47

CN buildings shall be designed for double-sided exposure, presenting equivalent architectural details on both the front and side elevations. Buildings should front onto the central activity area, the “Promenade” or other pedestrian-oriented space. Rear façades shall have architectural consideration to provide a quality elevation facing visible within the Specific Plan.

Policy 6-48

A three (3) foot high landscape hedge or decorative wall shall be used to screen surface parking from view of from all adjacent streets.

Policy 6-49

Assigned parking for MR and HR uses may be permitted within the CN surface parking:

- Where a shared parking study has been prepared for the applicable uses
- By administrative approval of the Community Development Director
- Where residential parking spaces are clearly marked as reserved

Policy 6-50

Standards established for outdoor seating by Section 25.32.9.6.D.3 shall apply to the CN land uses within the Specific Plan. Standards and permits shall be the same except as modified by policies of this Specific Plan.

6.5.3 Open Space and Parks Criteria

Policies for the design and execution of the Lake Park and other parks within PA4 are addressed in Chapter 9.0, Public Services. In addition to the requirement to achieve land use consistency with the land use plan and the policies of Chapter 9.0, the following policies constitute minimum requirements related to open space and parks within the Lake District. Additional design guidelines for each land use category can be found in Section 13.0 of this document

Policy 6-51

Community paseos shall connect surrounding neighborhoods to the Lake Park increasing access to open space recreation areas. Two pocket parks are programmed for the Lake District to provide passive flexible play areas. See Figure 9-2 for Community Paseo locations and Figure 9-3 for Pocket Park distribution.

Policy 6-52

Within residential neighborhoods, open space nodes shall be provided to meet the requirements of this document and the recreational open space needs of the residents. Refer to Chapter 9.0 Public Services and Chapter 12.0 Landscape sections of this document for specific guidelines, requirements and standards.

6.6 Lake District Circulation

The underlying philosophy behind the design of the street framework in the Lake District is to create a connected grid of streets that allows for dispersion of traffic with the aim to reduce the number of lanes for streets within the plan and enhance the pedestrian friendly nature of the plan. The circulation plan provides two points of entry to the District from Vanden Road, with the only signalized intersection being the street that comes in at the south end of the Lake. The circulation diagram for the Lake District, Figure 6-8 below, illustrates the multiple levels of access planned for the Lake District as well as the primary entrances into the District and connections to the Boulevard and other parts of the plan. Figure 6-9 shows the bicycle and pedestrian system proposed for PA4.

Street sections within the Districts shall be in conformance with the applicable street sections of Chapter 10.0, Transportation, as identified by Figure 10-1. Landscape, walls and fences, street lights and hardscaping in the Lake District shall be consistent with the community character and the standards and design guidelines of Chapter 13.0 of FTSSP.

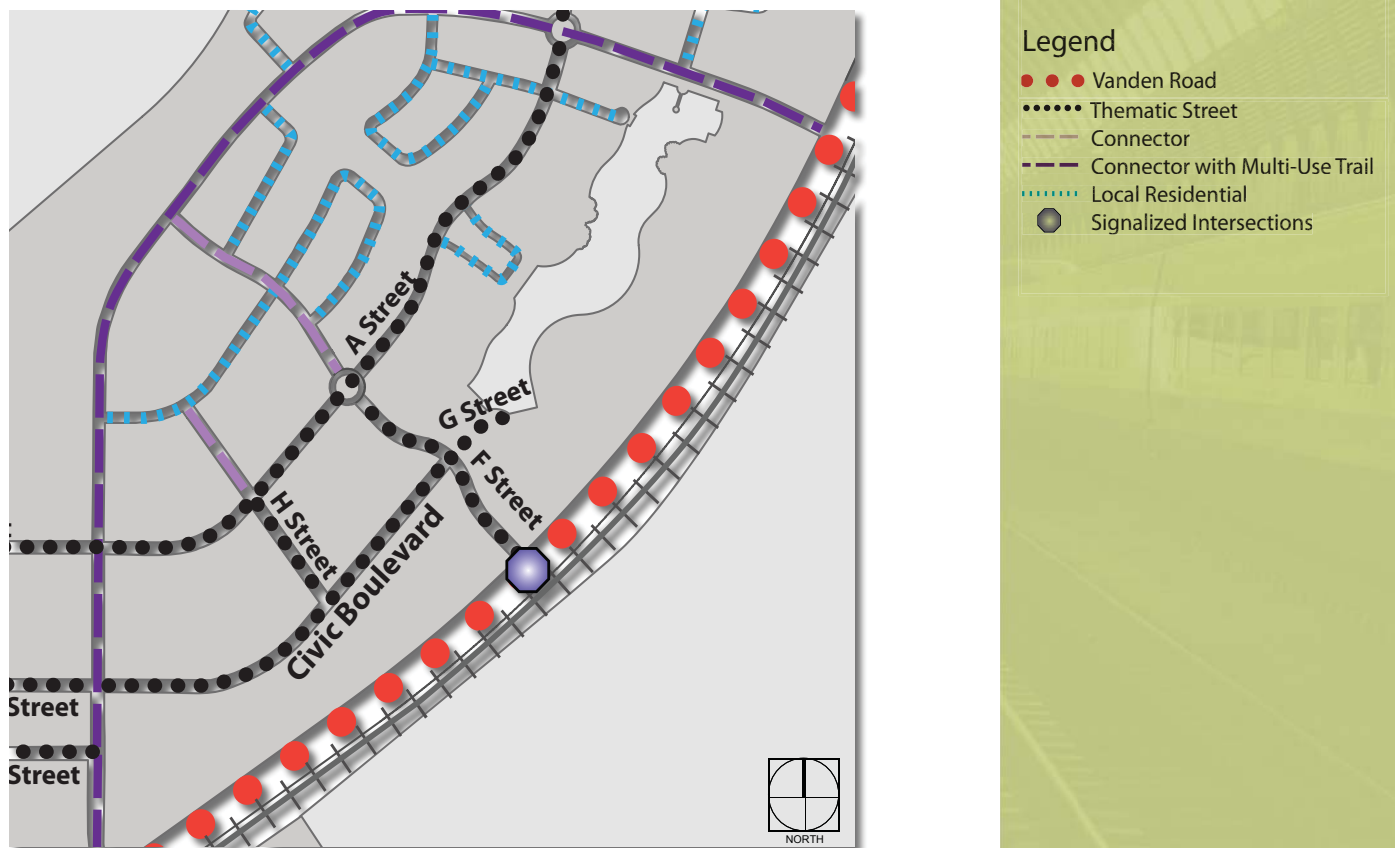


Figure 6-8: Lake District Circulation

Legend

- On-Street Bike Path
- Off-Street Bike Path
- Class 1 Bike Trail
- - - Multi-Use Trail

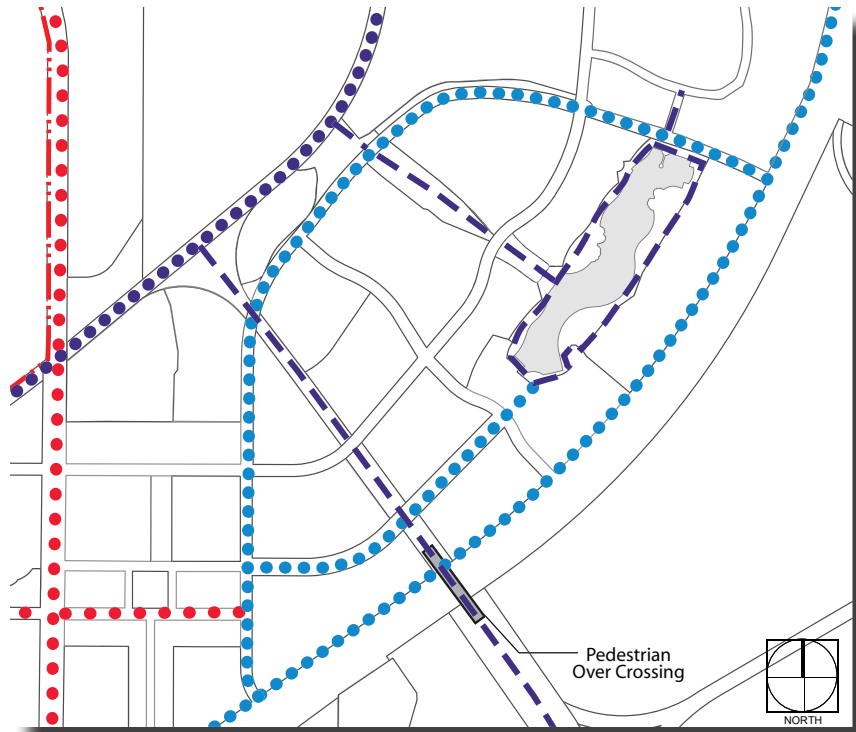


Figure 6-9: Lake District Bicycle and Pedestrian Plan

6.6.1 Thematic Streets

Design around the central feature of PA4, the Lake, is imperative to maintain the focus of the district and maximize access into the Lake Park. For this reason streets within the lake neighborhoods and park neighborhoods need to be carefully considered to create thoughtful neighborhood design and heightened access (Figure 6-10, Thematic Streets Diagram). To support these goals, several thematic streets have been incorporated into the urban design framework to create a more diverse, interesting and memorable district. Streets are planned to provide a unique identity that will help 'brand' their surrounding neighborhoods. See Section 13.4.6 for more details regarding lake and park neighborhoods.

Civic Boulevard

The Civic Boulevard, detailed in Chapter 5.0, Town Center, originates at the Town Square and continues through the Lake District to terminate at the Village Core. This Thematic Street provides a central walking promenade between two major activity centers. See section 5.6.1 in Chapter 5.0 for further discussion and the roadway section. See Figure 10-13.

A Street

Another key Thematic Street within the Lake District is A Street, envisioned as a romantic boulevard that sits at the base of the hill overlooking the Lake Park. The street is the primary vehicular link

between the Town Center and the neighborhoods to the north. There will be a mix of MR and LR units along the street, but all units facing this street shall be alley loaded. The proposed geometry of the street shall respond to existing site topography as well as key design features such as the community paseo that connects the Lake Park to this street. Front doors articulated by stoops and porches, on-street parking and a double row of trees provide this street a special pedestrian character. Traffic calming elements, such as roundabouts, should be incorporated in the overall design to ensure walkability. See Figures 10-16 and 10-17.

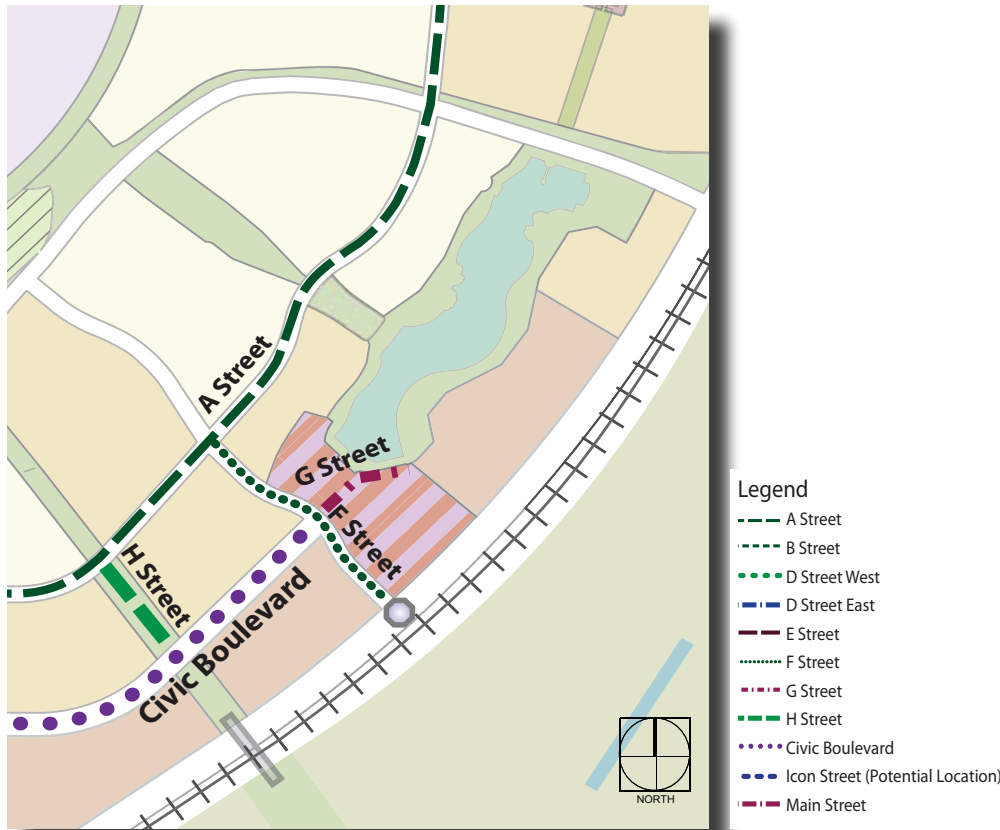


Figure 6-10: Thematic Streets Diagram

G Street

This street will mimic the Main Street within PA2, bringing angled parking, pedestrian scaled environments and urban setbacks to the retail/commercial setting of the Village Core. Depending on the location of the street section, the Village Square may inhabit one side of the street section while the Lake Park inhabits the other side. Angled parking will provide access and pedestrian friendly streets to the area. See Figure 10-23.



6.6.2 Circulation and Parking Policies

The following policies and criteria constitute minimum requirements related to circulation and parking within the Lake District. Additional design guidelines for each land use category can be found in Section 13.0 of this document.

Policy 6-53

Public roadways, additional to those shown in Figure 10-1, may be required to achieve the preferred block sizes ranging from 300-500 feet in length. Such roads should be through street-to-street, if practical, to enhance connectivity.

Policy 6-54

Residential access streets intersecting Vanden Road shall be limited to right-in, right out movements and subject to review during the Master PUD Permit approval process. Signalized intersections on Vanden Road shall be limited to the intersection shown on the Land Use Plan of this Specific Plan.

Policy 6-55

Parking shall not be permitted between buildings/development and the Lake Park.



7.0 RESOURCE MANAGEMENT



7.0 RESOURCE MANAGEMENT

July 26, 2011



7.1 Existing Conditions

The FTSSP area is located at the base of the Inner North Coast Range foothills, just east of the current City limits of the City of Fairfield in Solano County. The topography within the Specific Plan area is flat in some areas and gently rolling in other areas. Slopes up to 30 percent are present in some areas. Elevations range from approximately 60 feet to 180 feet above mean sea level (MSL).

Much of the site was historically used for cattle ranching and large portions of the Specific Plan area still are used for grazing. Much of the area has been extensively levelled for agricultural use, which has resulted in substantial disturbance and alteration of natural habitats on the site, including historic vernal pools and vernal pool grassland. The natural mound and swale microtopography that existed historically has been virtually eliminated from the Specific Plan area, and the vegetation has been largely converted from vernal pool endemic plant communities to communities dominated by nonnative annual grasses, such as Italian ryegrass and Mediterranean barley. In most cases, the surface topography has been altered sufficiently to result in major hydrological changes, such as shortened hydroperiod. However, seasonal wetland depressions remain and biological resources typically associated with vernal pool grasslands are still present in portions of the Specific Plan area, primarily east of Vanden Road and south of Canon Road.

The majority of the Specific Plan area is within the Union Creek watershed. However, some of the west-central portions of the site are part of the McCoy Creek and McCoy Basin watershed. The southeastern portion of Noonan Ranch drains to the Denverton Creek watershed and the northeastern portion of Noonan Ranch is part of the Barker Slough watershed.



For more detailed information on the biological communities associated with the Specific Plan area, please see the Biological Report (Appendix to the Fairfield Train Station Specific Plan EIR document).

7.1.1 Plant Communities

The predominant plant community in the Specific Plan area is annual grassland, but wetland plant communities are present within vernal pools and swales, drainage channels, and other depressions on-site. Vegetation within the vernal pools in the Specific Plan area is characterized by hydrophytic plants and typically dominated by common spikerush (*Eleocharis macrostachya*) and smooth goldfields (*Lasthenia glaberrima*). On-site ponds and some of the drainage channels support freshwater marsh dominated by cattails (*Typha* sp.). Occasional willow trees or shrubs (*Salix* sp.) are encountered in these areas, as well. Trees exist along fencerows, roadsides, and home sites, as well as some other scattered locations. Trees in the Specific Plan area include oak trees, blue gum (*Eucalyptus globulus*), fruit trees in remnant orchards, and willow and cottonwood trees. Figure 7-1 shows the plant communities and other habitats within the Specific Plan area.

Sensitive Plant Resources

A total of 21 special-status plant species have been documented to potentially occur in the Specific Plan area. Alkali milk-vetch, heartscale, brittlescale, San Joaquin spearscale, Contra Costa goldfields, papoose tarplant, Carquinez goldenbush, and Baker's navarretia are considered to have high potential for occurrence because suitable habitat conditions are present and there are known occurrences in the immediate Specific Plan area vicinity.

7.1.2 Wildlife Communities

The Specific Plan area supports an abundant and diverse fauna. The large and contiguous open spaces on-site are important to native wildlife species associated with grassland and seasonal wetland habitats. The Specific Plan area provides habitat for raptors that prefer large tracks of open grassland for foraging. The combination of open grassland with available seasonal and perennial water sources is attractive to many of the common wildlife species found in Solano County, as well as a few special-status wildlife species (discussed below).

Numerous common wildlife species were observed within the annual grassland habitat on-site. Songbird species observed during these surveys include western kingbird (*Tyrannus verticalis*), western meadowlark (*Sturnella neglecta*), cliff and barn swallows (*Hirundo pyrrhonata* and *H. rustica*), horned lark (*Eremophila alpestris*), Brewer's blackbird (*Euphagus cyanocephalus*) and northern mockingbird (*Mimus polyglottos*). Birds of prey species found in the site's grasslands include red-tailed hawk (*Buteo jamaicensis*), Swainson's hawk (*Buteo*



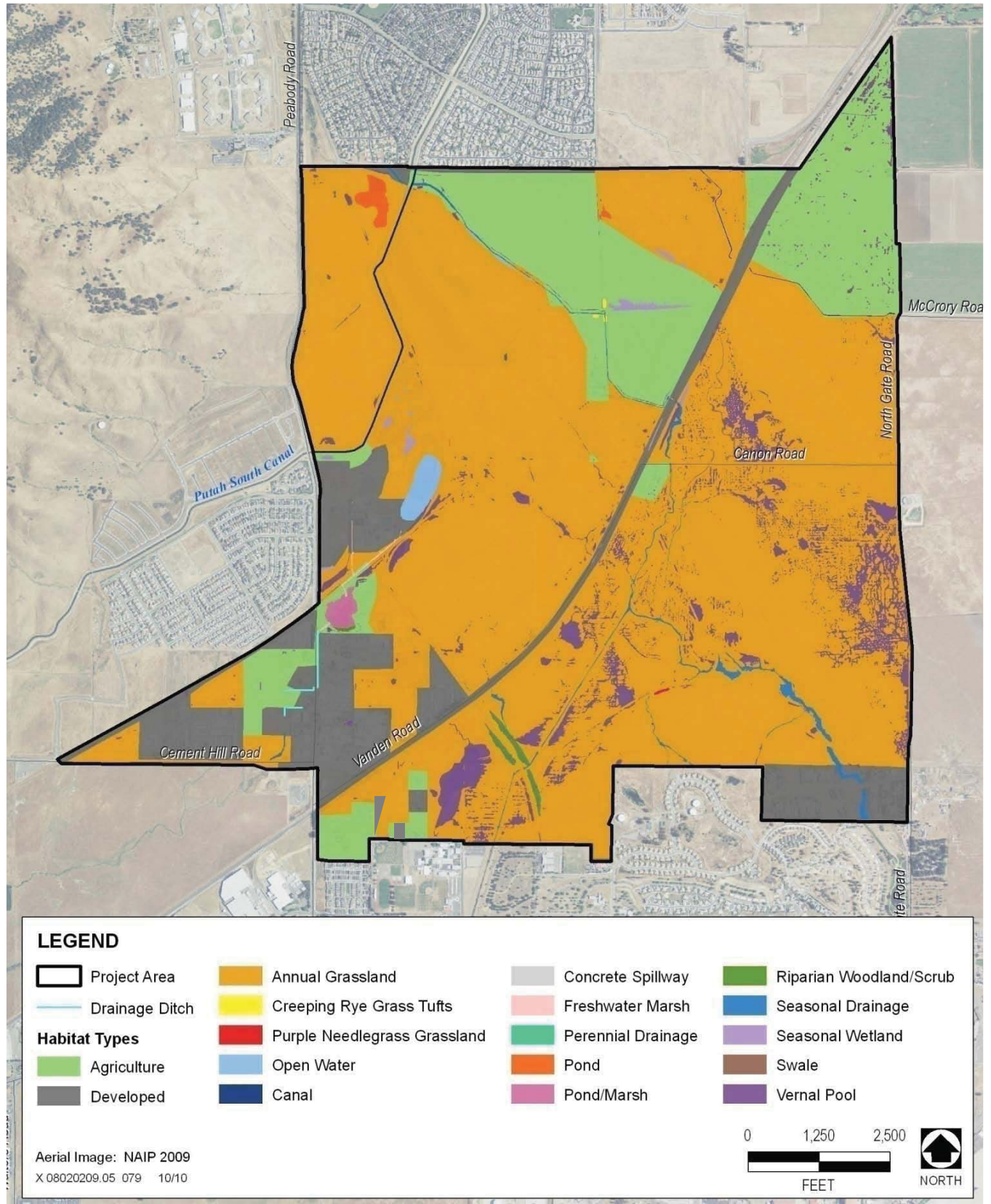


Figure 7-1: Habitat

What is a Habitat Conservation Plan?

A Habitat Conservation Plan (HCP) is a conservation program that protects, restores and enhances habitat areas; monitors and reports on covered species populations; and avoids and minimizes impacts on species and their habitats. A HCP can also provide new commitments of land protection, personnel and resources dedicated to habitat conservation.

swainsoni), American kestrel (*Falco sparverius*), golden eagle (*Aquila chrysaetos*), great horned owl (*Bubo virginianus*), burrowing owl (*Athene cunicularia*), and turkey vulture (*Cathartes aura*). Common reptile species associated with the on-site grassland habitat include gopher snake (*Pituophis catenifer*), racer (*Coluber constrictor*), and western fence lizard (*Sceloporus occidentalis*). Common mammal species include deer mouse (*Peromyscus maniculatus*), California vole (*Microtus californicus*), raccoon (*Procyon lotor*), striped skunk (*Mephitis mephitis*), and coyote (*Canis latrans*). California ground squirrels (*Spermophilus beecheyi*) were observed at the edges of the Specific Plan area in the existing and old raised railroad rights-of-way.

In addition to the grassland species, several wildlife species were observed in association with aquatic habitats of Union Creek and the irrigation ditches, ponds, and vernal pools in the Specific Plan area. These species include red-winged blackbird (*Agelaius phoeniceus*), mallard (*Anas platyrhynchos*), cinnamon teal (*Anas cyanoptera*), belted kingfisher (*Ceryle alcyon*), American bullfrog (*Rana catesbeiana*), mosquito fish (*Gambusia sp.*), western pond turtle (*Actinemys marmorata*), and larvae of Pacific chorus frog (*Pseudacris sierra*).

Sensitive Wildlife Resources

Sensitive natural communities present in the Specific Plan area include vernal pools and swales, seasonal wetlands, freshwater marsh, purple needlegrass grassland, and creeping grass tufts.

Special-status wildlife species have been observed in the Specific Plan area. According to database searches and field surveys, the presence of vernal pool fairy shrimp, California tiger salamander, western pond turtle, burrowing owl, Swainson's hawk, and loggerhead shrike have been documented in the Specific Plan area. Figure 7-2 shows critical habitat within the Specific Plan area.

7.2 Vacaville-Fairfield-Solano Greenbelt

In 1994, an agreement was approved through adoption of a joint resolution of the City Councils of Fairfield and Vacaville and the Solano County Board of Supervisors regarding the establishment of a greenbelt between the cities of Fairfield and Vacaville. That agreement formed the Vacaville-Fairfield-Solano Greenbelt Authority.

The boundary of the proposed Greenbelt is generally the unincorporated area between the Cities of Fairfield and Vacaville. A very small portion of the Greenbelt has been annexed to either Fairfield (northerly portion of the Northbay Water Treatment Plant site) or Vacaville (portion of the hill between Lagoon Valley and Paradise Valley, adjacent to I-80). Its western boundary is I-80 and the eastern edge is defined by Lewis Road. The Greenbelt contains approximately 4,400 acres.

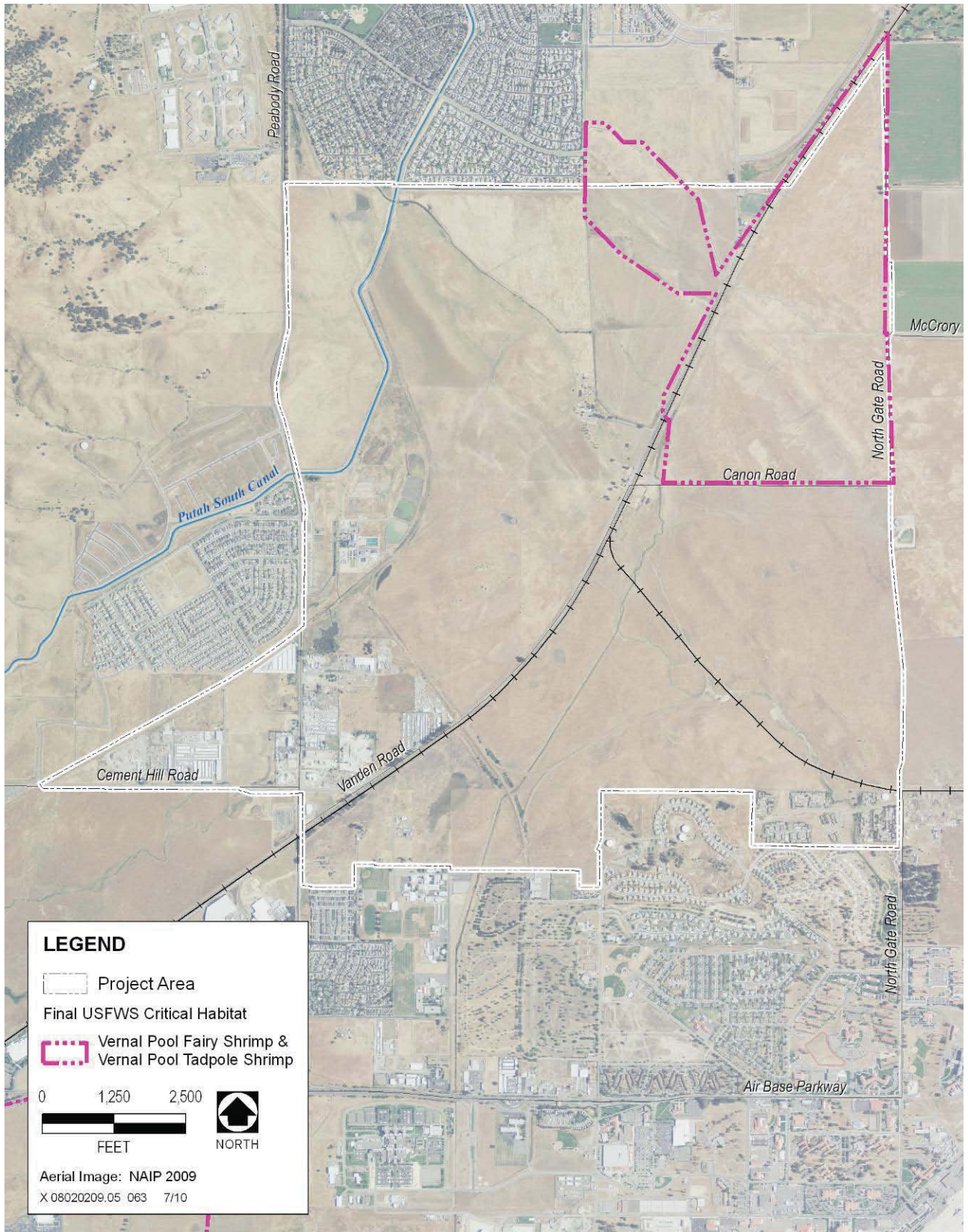


Figure 7-2: Critical Habitat



The purpose of the Greenbelt is to (1) provide for preservation and conservation of viable agricultural and open space land; (2) provide a permanent separation between urban areas of Fairfield and Vacaville; (3) provide in the area between the northern portion of Fairfield and the southern portion of Vacaville, land uses that are compatible with open space values and that achieve a balance between providing development opportunities and preserving high-quality open space; (4) promote the protection and viability of Travis AFB through the Greenbelt planning process; (5) provide trail linkage of open space areas of the Greenbelt to other open space areas; and (6) provide for the urban limit line buffers.

The Greenbelt Authority has not actively pursued implementation of the Greenbelt since its establishment in 1994. The only formal implementation action taken by the Vacaville-Fairfield-Solano Greenbelt Authority was approval of a Greenbelt Zoning Ordinance in October of 1995. That approval was later rescinded by the Greenbelt Authority in March of 1996. The Greenbelt Authority has not taken action to develop or implement Greenbelt plans, which were intended to contain the boundaries of the Greenbelt and Urban Limit Line Buffers, land use policies and standards, determination of appropriate Greenbelt uses, and mechanisms for acquisition and maintenance of Greenbelt properties.

The Greenbelt Authority met on December 14, 2009, and August 9, 2010 to conduct preliminary discussions regarding a possible modification to the Greenbelt boundary, consistent with the proposed Specific Plan. Approval of any Greenbelt boundary modifications would be considered by the Greenbelt Authority following certification of the Environmental Impact Report for the Specific Plan. If the Greenbelt boundary were to be modified to be consistent with the Specific Plan, there would be an approximate net increase of about 400 acres of Greenbelt. Approximately 148 acres would be removed from the Greenbelt east of Vanden Road and 115 acres of land would be removed from the Greenbelt west of Vanden Road. Within the Vanden Road corridor, about 663 acres would be added to the Greenbelt. In addition, 1,647 acres would be added to the Greenbelt along the area identified as the “urban buffer” in the vicinity of the Travis AFB runway.

7.3 Solano Multispecies Habitat Conservation Plan (SMHCP)

The Specific Plan area is located within the proposed Solano Multispecies Habitat (SMHCP) area. The SMHCP is currently in draft form and is not an approved Habitat Conservation Plan (HCP). The purpose of the proposed SMHCP is to promote the conservation of biological diversity and the preservation of endangered species and their habitats consistent with the recognition of private property rights; provide for a healthy economic environment for the citizens,

agriculture, and industries; and allow for the ongoing maintenance and operation of public and private facilities in Solano County.

The draft SMHCP establishes a framework for complying with state and federal endangered species regulations while accommodating future urban growth; development of infrastructure; and ongoing operations and maintenance activities associated with flood control, irrigation facilities, and other public infrastructure undertaken by or under the permitting authority/control of the HCP participants within Solano County over the next 30 years.

The overarching conservation goal of the SMHCP is to preserve contiguous functional landscapes that encompass the full suite of ecological diversity, maintain connectivity among natural communities, and functionally buffer natural communities from direct and indirect impacts from anthropogenic pressures. The Conservation Strategy, if adopted, would provide a comprehensive program for avoiding and minimizing impacts; implementing specific conservation measures designed to preserve, restore, and manage habitats for 37 covered species; and providing long-term monitoring and adaptive management to maximize conservation values on established reserves over time. Implementing the current draft Conservation Strategy, for all County-wide areas covered by the draft SMHCP would result in the establishment of an estimated 25,000 to 30,000 acres of reserves, preserves, open space lands, and other cooperative habitat restoration sites.

7.4 Conservation and Open Space

The Specific Plan includes establishment of approximately 1,532 acres of open space that would preserve most of the highest quality habitats in the Specific Plan area, including areas identified in the draft SMHCP as medium- and high-value conservation areas. The Noonan Ranch Conservation Bank has already been established on 203 acres in the southern portion of the Specific Plan area, east of Vanden Road, in a SMHCP high-value conservation area. The Noonan Ranch Conservation Bank preserves 3.96 acres of seasonal drainage, 0.15 acre of freshwater marsh, and 23.85 acres of vernal pools in a grassland matrix and supports vernal pool fairy shrimp, California tiger salamander, and a large population (between 12.4 and 17.2 million plants estimated in 2009) of Contra Costa goldfields. Another 450 acres of high-value conservation area would be designated adjacent to the established Noonan Ranch Conservation Bank as part of the Specific Plan area. These open space conservation areas preserve the majority of the vernal pools and other seasonal wetlands in the Specific Plan area, including the large playatype vernal pools and wetlands known to be occupied by Contra Costa goldfields.



7.5 Resource Management Policies

Policy 7-1

All improvements and proposed construction activities within the Resource Management area, including clearing, grubbing, or excavation, shall obtain the appropriate permits and pay the appropriate fees.

Policy 7-2

Before the approval of grading and improvement plans and before any groundbreaking activity associated with each distinct project, the project applicant(s) of all projects shall secure the necessary permits (i.e., Section 404) and implement all permit conditions to ensure no net loss of functions and values of wetlands, Other Waters of the United States, and Waters of the State.

Policy 7-3

Before approval of any grading or improvement plans, before any ground-disturbing activities, project applicant(s) of all project phases shall secure take authorization for Federally Listed Vernal Pool Invertebrates and implement all permit conditions.

Policy 7-4

Before approval of any grading or improvement plans, before any ground-disturbing activities, project applicant(s) of all project phases shall preserve and restore wetland and adjacent upland habitat consistent with the SMHCP Conservation Strategy and any other adopted HCP.

Policy 7-5

Before approval of any grading or improvement plans, before any ground-disturbing activities, project applicant(s) of all project phases shall secure and implement Section 1602 standards/conditions for the mapping of riparian habitat and other sensitive natural community.

Policy 7-6

Before approval of any grading or improvement plans, before any ground-disturbing activities, project applicant(s) of all project phases shall map noxious weed infestations and avoid these areas to the extent feasible.

Policy 7-7

The Project shall avoid and minimize impacts to Tricolored Blackbird and Loggerhead Shrikes colonies by taking precautions to avoid colony disturbance during nesting season.



Policy 7-8

The Project shall conduct special status plant surveys, avoid loss of special-status plants and loss and degradation of special-status plant habitat and sensitive natural communities.

Policy 7-9

The Project shall not conflict with local policies or ordinances protecting biological resources, such as the City of Fairfield General Plan and City ordinances.

Policy 7-10

As a goal of the Specific Plan, conservation easements shall be placed on all land designated as Open Space Conservation or Open Space Recreation. In some cases, the placement of easements shall be done in conjunction with development entitlements and, in other cases, the City or a nonprofit organization may acquire such easements.

Policy 7-11

All such conservation easements shall be consistent with the following:

- Be granted to City or to a nonprofit organization, such as a land trust that is formed for the purposes of preserving land as open space, agriculture, public park, or wildlife habitat
- Be conveyed to City or nonprofit at no cost and shall be granted in perpetuity
- Be conveyed with an endowment to easement holder that is sufficient to maintain property for its intended purposes in perpetuity
- Prohibit subdivision and limit development of property; specific terms of easement may vary by property depending on the intended use as open space, agricultural, public park, or wildlife habitat purposes
- Be reviewed and approved by City, with respect to the easement form and terms and the associated endowment prior to its conveyance
- Allow construction of public improvements and facilities as needed to support development within the portions of Specific Plan designated for urban development

Policy 7-12

The City shall establish an Open Space CFD in lieu of CFD 2004 on residential land within the Specific Plan area. The tax revenue from this Open Space CFD shall be used to support the maintenance and acquisition of land within the Greenbelt, including: (1) maintain the Great Park, (2) maintain open space land, and (3) acquire open space land.





City shall also establish a Non-residential Public Safety and Open Space CFD on industrial land within the Specific Plan area. The tax revenue may be used to maintain habitat conservation land within PA8.

Policy 7-13

Canon Station shall encumber all its land designated as “Open Space Conservation” in PA8, including habitat conservation, mitigation bank, and vernal pool conservation area, with a conservation easement(s), as shown in Figure 1-1, Land Use Plan. The conservation easement(s) shall be dedicated or an irrevocable offer of dedication will be granted by Canon Station concurrently with initial Parcel Map or Final Map within PA4 or PA5.

Policy 7-14

Canon Station shall encumber all its land designated as “Open Space Recreation” in PA7, including Greenbelt recreation/ open space and the Great Park, with a conservation easement(s), as shown in Figure 1-1, Land Use Plan. The conservation easement(s) shall be dedicated or an irrevocable offer of dedication will be granted by Canon Station concurrently with initial Parcel Map or Final Map within PA4 or PA5.



8.0 SUSTAINABILITY & CLIMATE CHANGE PLAN



8.0 SUSTAINABILITY & CLIMATE CHANGE PLAN

July 26, 2011



8.1 Overview

Interest in sustainable development has been growing throughout the world. This concept integrates economic development, community livability and ecology. Promoting development without compromising the ability of future generations to meet their needs has become more goal for many segments of society. Many public and private sector entities have re-evaluated their plans to address long-term sustainability.

The threats from climate change and its severe consequences have never been so evident. The Intergovernmental Panel on Climate Change (IPCC), an international group of scientists and representatives of 113 governments in a statement released on February 2, 2007, concludes “The widespread warming of the atmosphere and ocean, together with ice-mass loss, support the conclusion that it is extremely unlikely that global climate change of the past 50 years can be explained without external forcing, and very likely that it is not due to known natural causes alone.”

California is leading the way with legislation in response to climate change. In the fall of 2006, Governor Schwarzenegger signed AB32, the global warming bill, into law. AB32 requires achievement by 2020 of a statewide greenhouse gas emissions limit equivalent to 1990 emissions, and the adoption of rules and regulations to achieve the maximum technologically feasible and cost-effective greenhouse gas emissions reductions. Subsequent state legislation and executive orders, such as SB375, SB1493, and SB1078, reinforced California’s determination and commitment to sustainability.

Along with other California cities, the City of Fairfield has been demonstrating its leadership and commitment to sustainability in many ways, from the embracing green building feature in City Hall design to the recent City of Fairfield Sustainability Initiative 2009. Fairfield also has a strong planning and regulatory foundation for a sustainable development program.

What is a Sustainable Community?

FTSSP interprets “sustainable community” to mean a community that is designed as a “whole” to foster health and vitality by connecting residents and visitors to the land and providing an enjoyable place to live, work, and play. A sustainable community accomplishes these goals without wasting natural resources or compromising the ability of future generations to meet their needs and enjoy an equal or higher quality of life.

This trend underscores the significance of FTSSP’s commitment to sustainability by incorporating a thoughtful sustainable development program. The FTSSP is planned with the future in mind, embracing sustainable development while minimizing impacts on climate change. The sustainability program acknowledges costs and focuses on optimizing the “triple bottom-line” of economics, environment, and society. The FTSSP’s sustainability program recommends measures that result in environmental, societal and economic benefits.

8.2 Vision and Guiding Principles

The overall sustainability vision for the FTSSP development seeks:

To create a community that balances human and natural resources while optimizing long term ecological, social and economic health.

FTSSP includes a comprehensive sustainability program in support of the proposed development, shaped by the following guiding principles:

- Building upon transit oriented development with proper density and mix of uses
- Promoting multi-modal transit connectivity
- The preservation of key habitat areas and creation of a robust open space system
- Optimizing value of the triple bottom-line to enhance long-term ecological, social and economic health
- Utilizing high performance design technologies to achieve cost effective energy use, water use, and clean air

8.3 Sustainable Low Carbon Community

Implementation of the FTSSP would generate CO₂ emissions from new residential, commercial, recreational, and public facility land uses. Greenhouse gas emissions from the Project would specifically arise from sources such as motor vehicles, natural gas consumption, solid waste handling/treatment, and electricity uses. The Bay Area Air Quality Management District (BAAQMD) has established the Thresholds of Significance for operational-related GHG emissions:

For land use development projects, the threshold is compliance with a qualified GHG Reduction Strategy; or annual emissions less than 1,100 metric tons per year (MT/yr) of CO₂e; or 4.6 MT CO₂e/SP/yr (residents + employees). Land use development projects include residential, commercial, industrial, and public land uses and facilities.

This emission target and level of sustainability is achievable through the following measures/strategies, most of which have been incorporated in the preferred alternative or are required by newly published state/federal mandates. These measures address various sustainability and emission factors, including building energy, water, transportation, and public realm energy.

8.3.1 Energy

Since building energy constitutes nearly 40 percent of the national carbon footprint, strategies on curbing building energy use is an integral part of the solution. This approach is a low-cost, high-return investment strategy to reduce energy consumption and associated emissions. In response to AB32, the 2008 Title 24 was adopted to provide California with an adequate, reasonably-priced, and environmentally-sound building energy use profile. With the approval of the most stringent, eco-friendly state-wide building code in the United States, the new building code standards “CALGreen” took effect in January 2011. CALGreen lays out specific constraints for newly constructed buildings and raises the bar of building energy performance for all future development.

The City will adopt the mandatory provisions of CALGreen in the local building code, therefore, requiring future FTSSP buildings comply with CALGreen standards. It is anticipated that new structures built in compliance with the green building code will achieve at least a 15 percent reduction in energy usage when compared to the State’s existing energy efficiency building code standards.

Public realm energy is commonly known as the energy used in public space between private buildings, including all streets, alleys, sidewalks, and parks. Public realm energy conservation measures deal mainly with lighting measures for public realm. Although it only represents a relatively small percentage of the total Project carbon footprint, it is one of the most cost-effective ways to reduce energy use and energy cost.

8.3.2 Water

In California, water related energy use includes conveyance, storage, treatment, distribution, wastewater collection, treatment, and discharge, which are components of the water use cycle. Water related energy use and emissions are termed “water embedded energy” since the energy used in moving or treating water is considered to be “embedded” in the value of the water. Even though water related emissions represent a relatively small percentage compared to building energy and transportation emissions, they contribute to the overall objective of reducing the overall water usage of the Project.

The 2010 CALGreen standard calls for the reduction of indoor potable water by at least 20 percent from current code standards through the use of low and ultra low flow fixtures. It also requires buildings to have more efficient controllers for exterior irrigation system.





8.3.3 Transportation

Transportation accounts for nearly a third of our nation's contribution to GHG emissions and has been rapidly growing in the past twenty years. If projections hold, this share will rise to 36 percent by 2020. Effective measures considered to reduce transportation emissions include reducing vehicle travel needs, increased vehicle fuel efficiency, investment in low-carbon alternative fuels, and alternative transportation modes.

The ideal location of FTSSP has provided the Project with advantages from a sustainable transportation perspective. The potential passenger train station provides a transit-oriented development (TOD) setting. The preferred alternative has incorporated and optimized this TOD opportunity by creating a more compact neighborhood center with mixed community services within walking distance of the station. In addition, rail spurs that serve a great portion of the future industrial area provide opportunities for diverting a significant amount of truck traffic to rail, thus reducing vehicle miles travelled (VMT). The FTSSP shall prioritize attractive, rail-served industrial to be located in those rail-served parcels.

Further, the proposed Project would provide a well-connected pedestrian trail system, which would link most community facilities and primary destinations, and should feature treated intersections that calm traffic and create pedestrian activity. The design of the trail system is aimed to entice more pedestrian and bike traffic while providing future community residents with an attractive alternative route for daily shopping, service, and recreational trips. It is estimated that a 1 to 2 percent of VMT reduction would be expected.

The California Low Carbon Fuel Standard (CALCFS) is the result of the Executive Order signed by Governor Schwarzenegger. The CALCFS requires producers of gasoline and diesel fuels to reduce the "carbon intensity" of fuels sold in California. By 2020, the carbon intensity will be reduced by 10 percent from the 2010 level. Based on the current fleet mix of the City of Fairfield and County of Solano, it is estimated that the VMT related emissions will be reduced by 15 to 20 percent compared to a Business-as-Usual scenario without such standards.

8.3.4 Waste

Reducing waste leads to a decrease of greenhouse gas emissions that normally result from landfill gas and the manufacturing and transporting of products and waste. The modern solid waste management practice recommends a hierarchy of approaches: reduce, reuse, and recycle.

Many detailed strategies and measures have been identified through various research and best practices. Most, however, fall beyond the control of the current FTSSP provision. Future construction waste management and waste collection in the public realm is the primary focus of FTSSP today. The CALGreen code mandates that all new buildings must divert waste to recycling or salvage a minimum of 50 percent of nonhazardous construction waste and demolition debris generated onsite. This does not represent a significant change from the current standard since the voluntary tier one standard of a 65 percent diversion rate is considered to be achievable in most areas of California. Future construction management in the FTSSP area should aim to achieve this 65 percent diversion target. The development shall also provide proper recycling facilities, such as trash cans, for recyclable and non-recyclables.

8.3.5 Eco-Services and Open Space

The Project is in essence a conservation development with a large portion of the land being placed in permanent conservation and preservation.

8.3.6 Environment and Public Health

Preventing harmful pollutants that are found in the air, water, natural environment, and food supply is an important part of becoming a more sustainable community. Clean air, safe groundwater, and a healthy ecosystem helps to maintain the health of the community and its residents. Encouraging proactive responsibility in preserving healthy streams, wilderness areas, diversity of plant and animal populations, soils, and clean air for future generations is imperative for the FTSSP.

8.3.7 Green Building

The mandated CALGreen code has set a standard of quality for new buildings' sustainable performance. CALGreen Tier 1 and CALGreen Tier 2 municipalities are required to adopt the mandatory provisions but may also choose to incorporate additional voluntary measures as part of local building standards. The voluntary measures are more stringent than the regular code.

For example, CALGreen Tier 2 requires a 30 percent building energy reduction from the current standard. Should the City choose to adopt Tier 1 or Tier 2 standards, the building related emissions would be further reduced. In addition to CALGreen, other green building programs should be considered for implementation, where feasible. Built-it-Green, established in 2003, is a well-known green building program tested in California. USGBC and its LEED certification system





is the best known green building program, with a specific rating system tailored for residential homes, schools, and other non-residential building types. Some municipalities in California have formulated their own green building program as well. Should the City establish a green building program, it should be included in the implementation of FTSSP.





9.0 COMMUNITY SERVICES



9.0 COMMUNITY SERVICES

July 26, 2011



The FTSSP will develop facilities to address all of its community service needs and generate new capacity for residents. Table 9-1 summarizes the service providers to the FTSSP.

Table 9-1: FTSSP Service Providers

Service	Provider
Fire Protection & Emergency Services	City of Fairfield Fire Department
Police Protection	City of Fairfield Police Department
Schools	Travis Unified School District
Libraries	Solano County
Parks and Recreation	City of Fairfield

9.1 Fire Protection and Emergency Services

9.1.1 Existing Services

The Fairfield Fire Department (FFD) provides fire protection and emergency services to the City of Fairfield and has five staffed fire stations located throughout the City, as shown in Table 9-2. Located at 1975 Huntington Drive, Station #39 is the closest station to the Specific Plan area. In addition to these stations, the FFD uses the original main station on Union Avenue, Station 38, as a training and vehicle maintenance facility.

Table 9-2: Fairfield Fire Department Locations

Station	Address
Station 35	473 #A Edison Court, Fairfield
Station 37	1200 Kentucky Street, Fairfield
Station 38*	1633 Union Avenue, Fairfield
Station 39	1975 Huntington Drive, Fairfield
Station 40	2555 Hilborn Drive, Fairfield
Station 41	3200 N. Texas Street, Fairfield

* Station 38 is for training and vehicle maintenance only.



The FFD employs 57 career fire service professionals including six chief officers and 51 firefighters, with support from 13 members in the Reserve Firefighter Program. The FFD provides a variety of services including emergency medical response, fire inspections, code enforcement, public education, and special teams such as the Hazardous Materials Response Team and the Urban Search and Rescue Team. Medical emergencies account for approximately 80 percent of the total FFD responses, with all department members being either a paramedic or EMT-1 level certified.

The FFD responds to approximately 9,000 service calls per year, handled by a total of five engine companies and one truck company staffed on a daily basis. These service calls are primarily medical emergencies but also include structure and wildland fires, brush fires, vehicle accidents, trench or confined space rescues, hazardous material spills, and fire alarms.

9.1.2 Proposed Services

The City has adopted a fire station location policy in its General Plan Public Facilities Element (Policy PF 15.1). Staffing and fire stations must ensure that at least 80 percent of the residential dwelling units in any response area are located within 5 minutes maximum travel time of a station. According to a needs assessment conducted by Citygate (2010), the current fire station locations would not adequately serve the Specific Plan area, per Policy PF 15.1. A total of six stations will be needed at buildout of the General Plan to maintain fire response times of less than 5 minutes. However, according to a needs assessment study, Station 39 should be relocated easterly to better serve the Specific Plan area by complying with Policy PF 15.1. A new fire station will be constructed, to replace existing Station 39. A specific relocation site has not been determined. General relocation sites are: a) Huntington Drive, between Walters Road and Peabody Road and b) vicinity of the Peabody Road/Dobe Lane intersection. Relocation would not adversely affect other areas currently served by Station 39.

In addition, the FTSSP designates a potential location for the construction of a new Fire Training Center that will provide training for the fire department.

9.2 Police Protection

9.2.1 Existing Services

The Fairfield Police Department (FPD) currently provides police protection services for the City of Fairfield. The FPD is located in the Fairfield Civic Center at 1000 Webster Street. The FPD is composed of 114 sworn law enforcement officers supported by an additional 111 non-sworn employees. Functions of sworn officers include: Patrol,

Investigations, Traffic, Narcotics, Gangs, School Resource officers, Domestic Violence, K-9, Crime Free Multifamily Housing, and SWAT. Functions of non-sworn officers include: Code Enforcement, Community Service Officers, Crime Prevention, Juvenile Diversion, Records, Dispatch, Property and Evidence, Training, Fleet Maintenance, Crime Free Multifamily Housing and Crime Analysis.

9.2.2 Proposed Services

The City has adopted a police response policy in its General Plan Public Facilities Element (Policy PF 16.1), in which the FPD must maintain an average emergency response time of under 5 minutes and an average nonemergency response time of under 20 minutes.

The FPD will need to increase its staffing to provide services to the Specific Plan area. Currently, the Fairfield General Plan recommends a ratio of 1.13 to 1.20 sworn officers per 1,000 residents. At full buildout of all 6,800 dwelling units, the estimated population of the Specific Plan would be 19,277 residents. Based on the recommended ratio of the General Plan, an additional 23 officers would be needed. In conjunction with additional officers, additional police support staff, police vehicles and building area for police facilities would also be needed. It is anticipated that any additional building area for police facilities would occur in the vicinity of the existing Police Department building.



9.3 Schools

9.3.1 Existing Services

The Travis Unified School District (TUSD) serves the area of Fairfield north and east of Air Base Parkway and Walters Road (including Travis AFB), as well as portions of Solano County and southeast Vacaville. The Specific Plan area falls within the boundaries of the TUSD. The TUSD consists of one high school, one continuation high school, one middle school, and five elementary schools, as shown in Table 9-3.

Table 9-3: Schools in the Travis Unified School District

School	Grades	Address	Enrollment
Cambridge Elementary School	K-6	100 Cambridge Drive, Vacaville	530 students
Center Elementary School	K-6	3101 Markeley Lane, Fairfield	448 students
Foxboro Elementary School	K-6	600 Morning Glory Drive, Vacaville	760 students
Scandia Elementary School	K-6	100 Broadway Street, Travis AFB	450 students
Travis Elementary School	K-6	100 Hickam Avenue, Travis AFB	544 students
Golden West Middle School	7-8	2651 DeRonde Drive, Fairfield	883 students
Vanden High School	9-12	2951 Markeley Lane, Fairfield	1,500 students
Travis Education Center	10-12	2775 DeRonde Drive, Fairfield	86 students



9.3.2 Proposed Services

According to TUSD's 1997 Facilities Plan, there is a need for three additional elementary schools, one additional middle school, and one additional high school to accommodate development in Fairfield and Vacaville. The TUSD is proposing an elementary school site within the Specific Plan area. The City of Fairfield will work with the TUSD to determine available capacity to serve the project's student projections.

TUSD will pursue appropriate funding and financing mechanisms to construct the new elementary school to serve the project in PA2. This can include the use of State New Construction funding to build new and expand existing schools, as well as local revenue sources such as TUSD's existing school mitigation impact fees.

9.4 Libraries

9.4.1 Existing Services

Solano County adopted an update to their Library Facilities Master Plan in 2009, in which they identified immediate library needs and planned for future developments. Currently, eight libraries are in Solano County, including a law library. Two of the public libraries are located in the City of Fairfield, the Fairfield Civic Center Library and the Fairfield Cordelia Library.

As part of the Library Facilities Master Plan, the Fairfield Civic Center Library underwent renovation in 2002 and reopened on November 15, 2003, and now features a computer center, study rooms, the Friends of the Library Bookstore, a wireless network, and sixty public computer stations. The Fairfield Cordelia Library opened on December 14, 2006, and is the newest branch in Solano County. It features a 20-station computer center, a career planning center, and a wireless network.

9.4.2 Proposed Services

Two additional libraries are planned for the City of Fairfield, according to the Solano County Library Facilities Master Plan. One new 30,000-square-foot library is planned for the northern part of the City, and another is planned for the northeast portion of the City that falls within the FTSSP area. Each library will have dedicated space for children's storytelling programs, community meetings and events, computer center and technology training, and group study rooms.

PA4 within the FTSSP has sufficient area within its mixed-use district at the south end of the Lake to accommodate the new library. In accordance with the County's Master Plan, this library is planned for construction between 2011–2020. The mixed-use area has designated an approximate 1.5-acre site which would be sufficient in size to accommodate a library building up to the approximate 29,000-square-foot size planned by the County.

9.5 Parks and Recreation

9.5.1 Existing Services

The City of Fairfield currently maintains 23 parks and recreation facilities throughout the City, including four playground/tot lots (0.2 to 0.5 acre each), two community parks (37 to 48 acres each) and with a third under construction, 16 neighborhood parks (5 to 8 acres each), two public golf courses, a Community Arts Center, an Aquatics Complex, Sports Center, Senior Center, and a bike and hiking path system.

The City of Fairfield General Plan currently provides criteria for neighborhood parks and community parks. Policy OS 12.3 pertains to neighborhood parks and states that they should be planned at a size of approximately 5 to 7 acres, and should be established at a ratio of 1.5 acres of usable park land per 1,000 residents throughout the City. Neighborhood parks are intended to serve a half-mile radius area.

Policy OS 12.4 pertains to community parks and states that they should be planned at a size of approximately 40 acres, and should be established at a ratio of 2 acres of usable park lands per 1,000 residents throughout the City. Community parks are intended to serve a 2-mile radius. When calculating park land needs, the population assumptions for persons per household include, 1.9 for RH, 2.8 for RM, and 3.2 for RL.

The City of Fairfield Municipal Code (Article III Division 13) discusses park land dedication and provides standards for park lands. Parks and recreational facilities are described as parks, tennis courts, soccer fields, ball fields, swimming pools, and other like recreational facilities. The general standard adopted by the City requires that 3.5 acres of property for each 1,000 residents should be devoted to park and recreational purposes.

Table 9-4 shows the park requirement and dedication per planning area.

9.5.2 Proposed Services

The FTSSP includes a park system that is intended to provide residents and visitors a meaningful public space experience while promoting active participation and healthy lifestyles. The parks and recreational areas will serve as focal points within the Specific Plan area by offering safe opportunities for people to gather, recreate, exercise, rest, and play.

The FTSSP proposes a parks and recreation program that includes recreation centers as well as pocket parks, neighborhood parks, a Lake Park, and a community park (Great Park), as shown in Figures below. Paseos are also proposed for the Specific Plan area. Park and



Table 9-4: Park Requirement and Dedication Summary

Land Use	Dwelling Units	Population	Community Park		Neighborhood Park	
			Dedication Needed (Acres)	Provided (Acres)	Dedication Needed (Acres)	Provided (Acres)
Planning Area 1						
LR	462	1,478				
MR	196	549				
HR	392	745				
	1,050	2,772	5.5		4.2	5.0
Planning Area 2						
LR	56	179				
MR	182	510				
HR / CM	1,337	2,540				
	1,575	3,229	6.5		4.8	5.0
Planning Area 3						
LR	28	90				
MR	588	1,646				
HR	308	585				
	924	2,321	4.6		3.5	5.0
Planning Area 4						
LR	329	1,053				
MR	592	1,658				
HR	743	1,412				
	1,664	4,122	8.2		6.2	25.0
Planning Area 5*						
LR**	777	2,038				
MR	700	1,960				
HR	-	-				
	1,477	3,998	8.0		6.7	2.0
Planning Area 9						
LR	110	352				
MR	--	--				
HR	--	--				
	110	352	0.7		0.5	0.0
Project Total	6,800	16,794	33.6	50.0***	25.9	42.0

Notes:

LR assumes 3.2 people per household; MR assumes 2.8 people per household; HR / CM assumes 1.9 people per household
 Community Park requirement is 2 acres per 1,000 residents and Neighborhood Park requirement is 1.5 acres per 1,000 residents

* Part of the Neighborhood Park requirement for PA5 is met by the Lake Park located in PA4

** Includes 140 dwelling units on Vacaville Water Treatment Site

*** Great Park in PA7 provides 50 acres of Community Park, in compliance with City policy

recreation will be provided and maintained through a hybrid system combining publicly owned spaces operated and maintained by the City with privately owned spaces maintained by the Home Owners Associations (HOA) that may be public access or private only recreation spaces.

Parks/Open Space and Recreation

Great Park



The Great Park located in PA7 is an approximately 50-acre community park including active and passive recreation opportunities for the community (Figures 9-1 and 9-2). This community park will be:

- Constructed by City and/or by FTSSP developers as may be required through conditions of approval of subsequent development entitlements or terms of a development agreement
- Open to the general public
- Owned and operated by the City of Fairfield
- Maintained by the City through tax revenue from the proposed Open Space CFD and CFD 2006-1
- Eligible to receive credit for City's Quimby land dedication requirement for the 50-acre park acreage dedicated

Landscape elements and organization should reflect the agrarian history of the surrounding landscape and provide opportunities for park users to learn about the local natural environment. The amenities will provide activities for all ages and may include but are not limited to lit sports fields, sport courts, shade structures, picnic areas, activity lawn, skate node, jogging trails, trailhead with interpretive signage, orchard-style landscaping, amphitheater, children's play areas, security lighting, and concession/restroom/storage building.



Legend

-  Neighborhood Park
-  Service Area (1/4 Mile Radius)

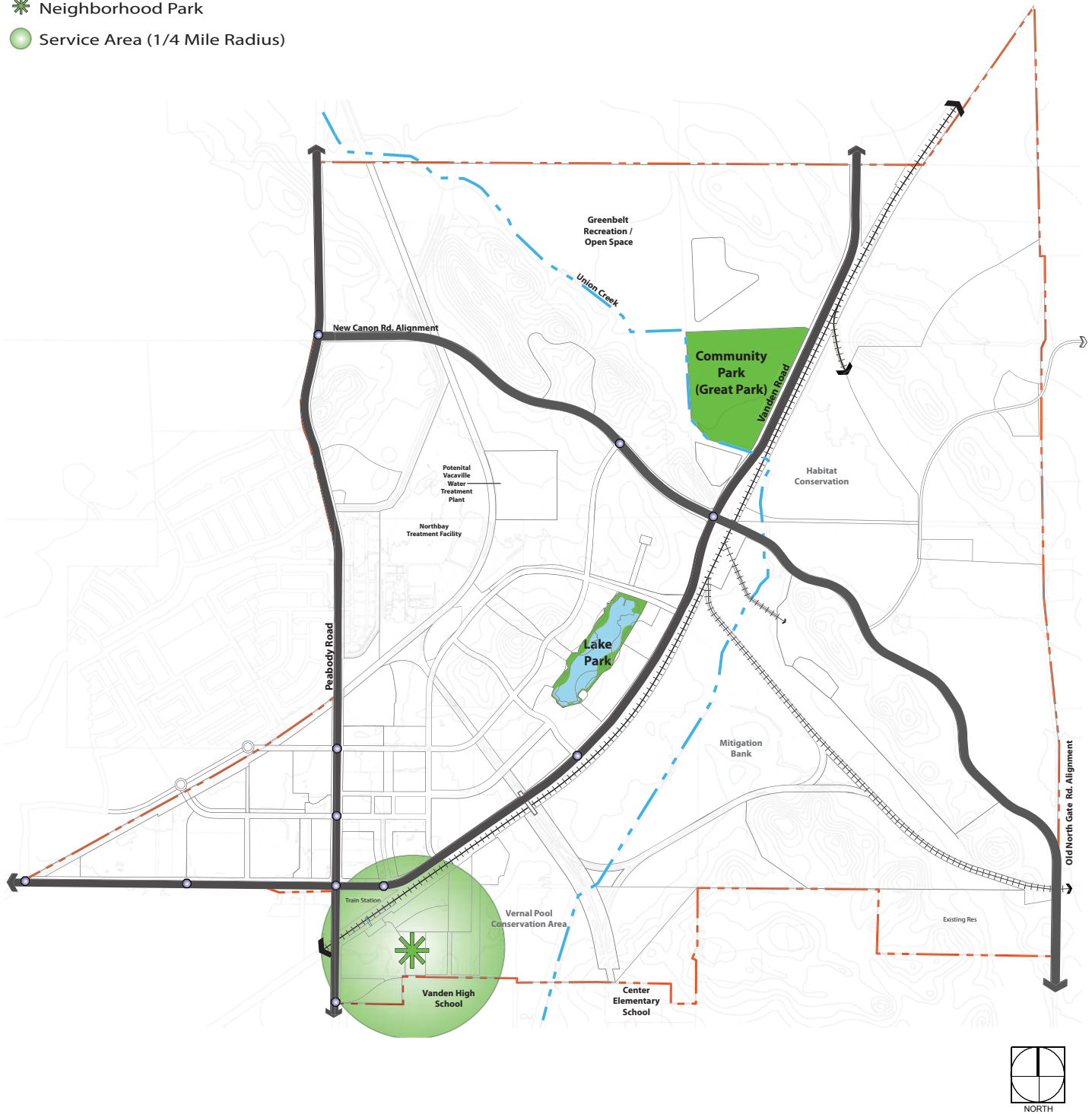


Figure 9-1: Community and Neighborhood Park Frequency Plan

k activities by the use of a tree lined promenade.
 portunities to learn about wetland environments with the
 n of an agricultural lake and the Union Creek restoration.
 s with "Airplane Hill" and nearby communities.
 k access along Vanden road for the neighboring
 as with the parking orchards.
 network of measured soft surface walking/jogging trails.



Figure 9-2: Illustrative Great Park



Linear Park

The 54-acre public Linear Park is proposed as a major open space linkage and bicycle/transportation trail connecting residential neighborhoods to open space recreation areas (Figures 9-3 and 9-4). This Linear Park will be:

- Constructed by City and/or by FTSSP developers as may be required through conditions of approval of subsequent development entitlements or terms of a development agreement
- Open to the general public
- Owned and operated by the City of Fairfield
- Maintained by City through tax revenue from a Lighting Landscaping Maintenance District(s)

This space should be designed as a vibrant and attractive space that includes active and passive recreational uses and is integrated with the storm water drainage system.

The goal of the Linear Park landscape area is to provide a visual and functional extension of the historic and cultural landscape of the region as informal recreational opportunities with pedestrian and bicycle pathways. Thoughtful blending of cultural and landscape architectural elements will span from the open space buffer landscape of PA7 into the PA2 Town Center and through PA1.

Functionally, the Linear Park landscape will provide for drainage and water restoration and is considered to be the spine of Fairfield's green infrastructure. The plant palette will consist primarily of native and drought-tolerant materials. A 10-foot-wide, city standard concrete trail coupled with fitness stations, informal seating areas, and security lighting will provide a trail system connecting to all parts of the City of Fairfield.

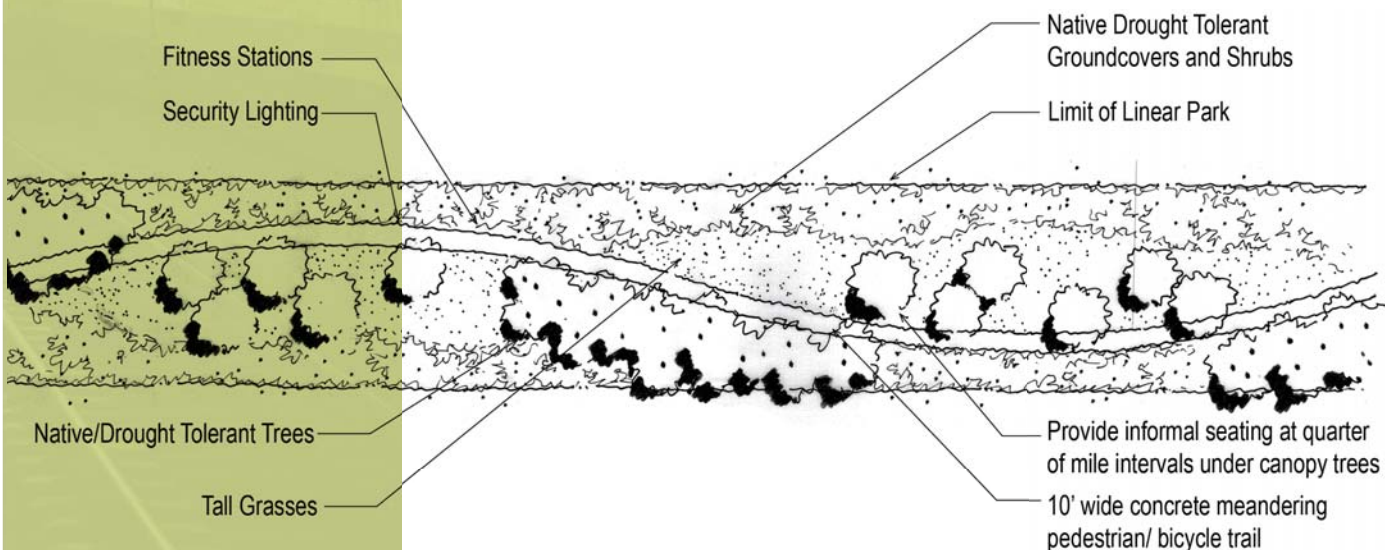


Figure 9-3: Linear Park Diagram



Figure 9-4: Linear Park Frequency Plan

The pedestrian pathway network at the FTSSP consists of two primary components: sidewalks and paths. This network will provide not only social and recreational opportunities for the residents but also an alternative mode of transportation. Hence, the two systems are planned and integrated seamlessly to provide a meaningful pedestrian connection to the regional network.

The sidewalk system is typically located adjacent to streets. Sidewalks may be separated from the curb by a landscape parkway or, in the case of more urban areas, the sidewalk may extend all the way to the curb. Sidewalk widths will vary depending on the classification of the streets and the development intensity of the built environment. The provision of street trees, appropriate signage, lighting, and pedestrian crossings are all important elements that greatly enhance the visual experience and safety of the sidewalk systems.

The off-road paths provide a different type of experience for the residents and visitors. They will typically be located in the green belts, parks, and other open space areas and accommodate uses such as walking, jogging, and biking. Path users tend to desire routes that include view corridors, scenic diversity, interesting land forms, and natural features. Facilities along the paths may include benches, trash receptacles, dog-waste bag stations, fitness stations, lighting, and appropriate signage.

Lake Park

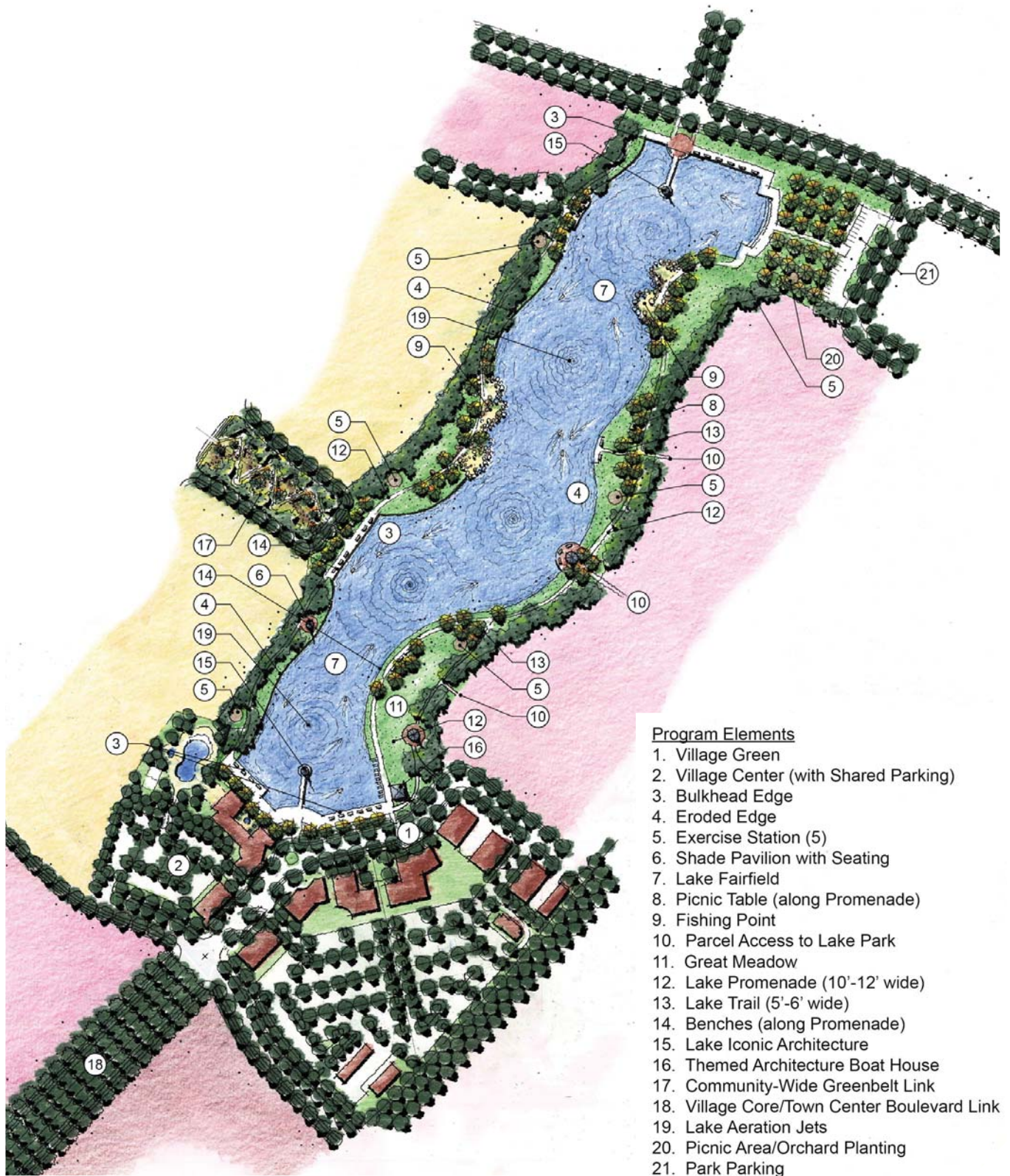
The Lake Park is an approximately 22-acre public amenity proposed to be centrally located within the FTSSP in PA4, accessible to all members of the community, and will offer both visual serenity and a variety of leisure activities (Figures 9-1 and 9-5). The Lake Park will be:

- constructed by City and/or by FTSSP developers as may be required through conditions of approval of subsequent development entitlements or terms of a development agreement
- open to the general public
- owned and operated by the City of Fairfield
- land portion of park shall be maintained by City through tax revenue from CFD 2006-1
- water portion of park shall be maintained by City through tax revenue from a Lighting Landscaping Maintenance District(s)
- eligible to receive credit for City's Quimby land dedication requirement only for the land portion (approximately 11 acres) of the park acreage dedicated

The Lake Park is comprised of approximately 11 acres of water feature surrounded by approximately 11 acres of public park land for a total of approximately 22 acres of recreation.

Iconic structures such as pergolas, pavilions, or public works of art will further enhance the overall character of the park and offer a





Note: Lake Fairfield (9 acre water feature) shall not count toward Quimby Act or Park Development Impact Fee credit

Figure 9-5: Illustrative Lake Park

memorable experience. The amenities may include, but are not limited to, promenade, parking, lake trail, seating plaza, benches, great meadow, amphitheater, iconic architecture, themed gazebo, fitness stations, fishing and pathway connections to future parcels.

Neighborhood Park

A public neighborhood park of 5 acres is proposed within PA3, south of the Train Station (Figures 9-1 and 9-6). This Neighborhood Park will be:

- constructed by City and/or by FTSSP developers as may be required through conditions of approval of subsequent development entitlements or terms of a development agreement
- open to the general public
- owned and operated by the City of Fairfield
- maintained by City through tax revenue from CFD 2006-1
- eligible to receive credit from City's Quimby land dedication requirement for the 5 acres of park acreage dedicated

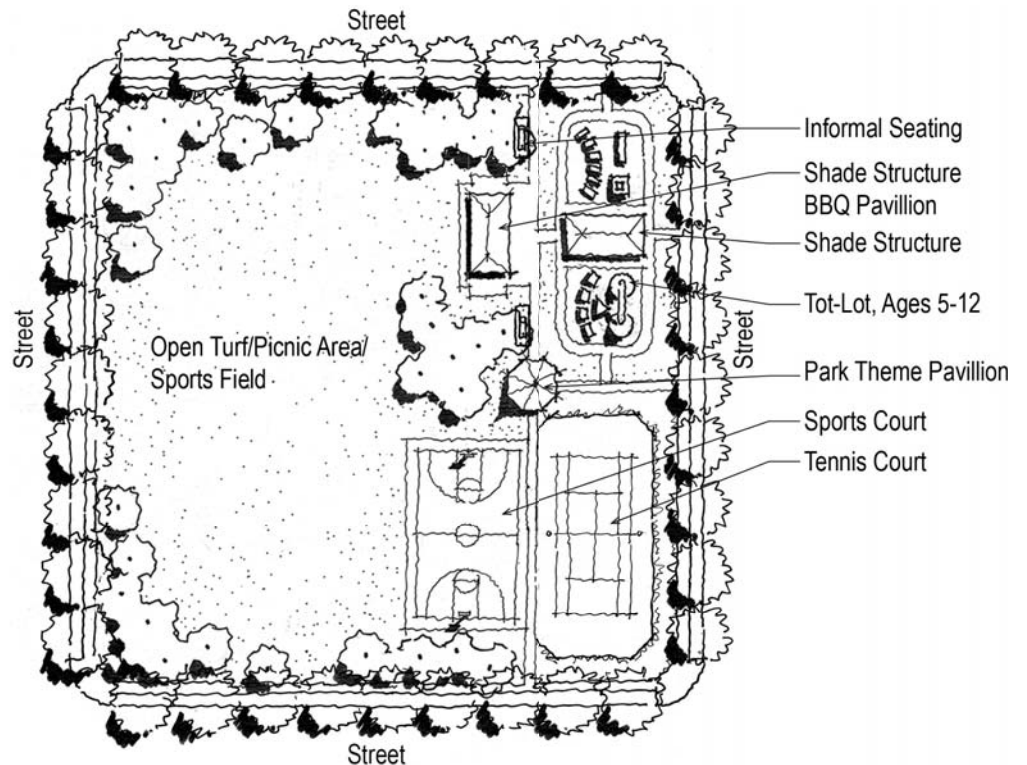


Figure 9-6: Neighborhood Park Diagram

It may be programmed for active field sports, sport courts, picnic areas, play structures, tot lots, trash receptacles and security lighting. The Neighborhood Park may be sited as a stand-alone facility or co-located with a school facility. Shade structures, park benches, and trash receptacles will be included in the neighborhood park. Parking for the neighborhood park will generally be accommodated by on-street parking around the park.

Private Recreation Centers

Private recreation centers will provide a central amenity for the neighborhoods in PA1, PA4 and PA5 (Figure 9-8). They will act as a social activity nexus and will vary in size, program, and character. Recreation centers of a larger scale may include meeting rooms, multi-purpose rooms, or special event areas. Private recreation centers will be:

- privately developed
- privately accessible for FTSSP/adjacent neighborhood residents
- owned and operated/maintained by the HOA

Pocket Parks

For the purposes of FTSSP, Pocket Parks will be programmed as 0.5-acre to 3-acre parks of a more modest scale and passive in character (Figures 9-7 and 9-9). A General Plan Amendment is being processed concurrently with FTSSP, which would allow parks less than 5 acres to be considered as “Neighborhood Parks” for City requirement calculations per the following conditions:

- park area shall not be less than 0.5 acre of usable space
- minimum dimension shall not be less than 100 feet of usable space
- shall be privately developed, owned and maintained by HOA
- park areas that are publically accessible shall count 100 percent of the park area toward Neighborhood Park requirements

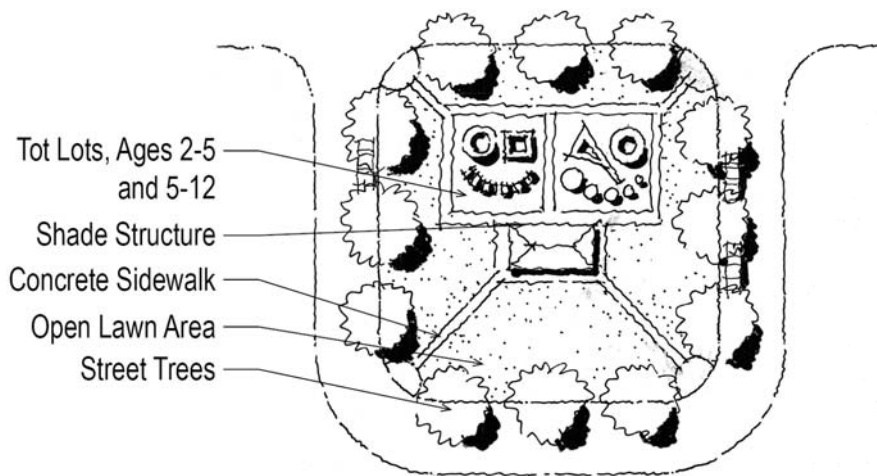
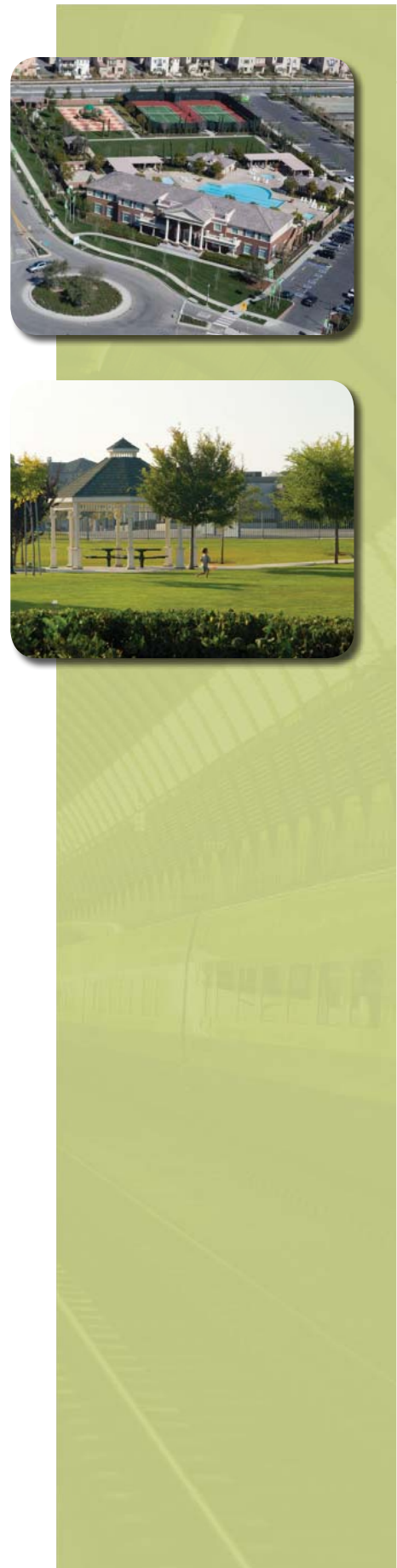


Figure 9-7: Pocket Park Diagram



Legend

- * Recreation Center
- Service Area (1/4 Mile Radius)

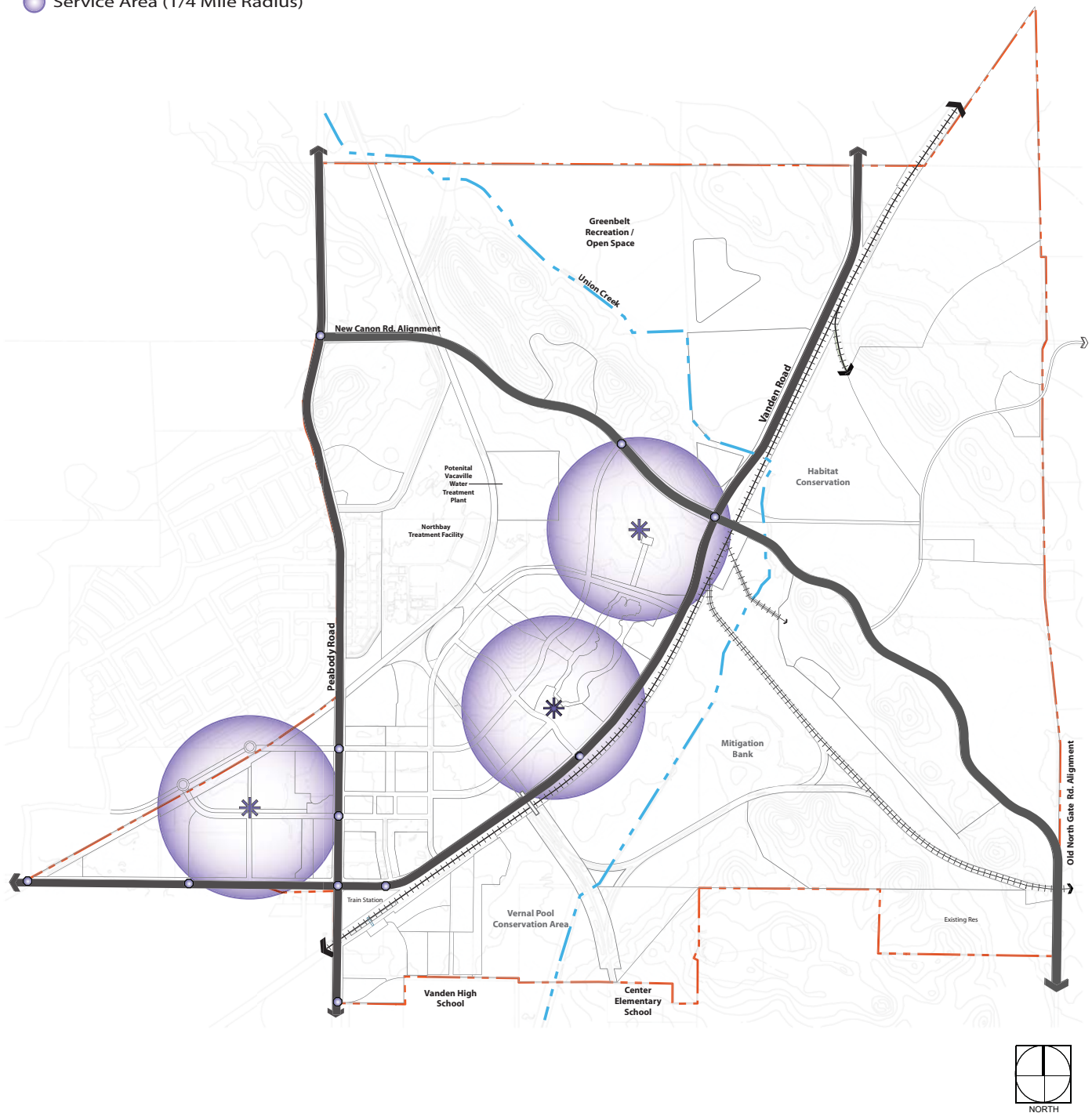




Figure 9-8: Recreation Center Frequency Plan

Legend

-  Pocket Park
-  Service Area (1/8 Mile Radius)

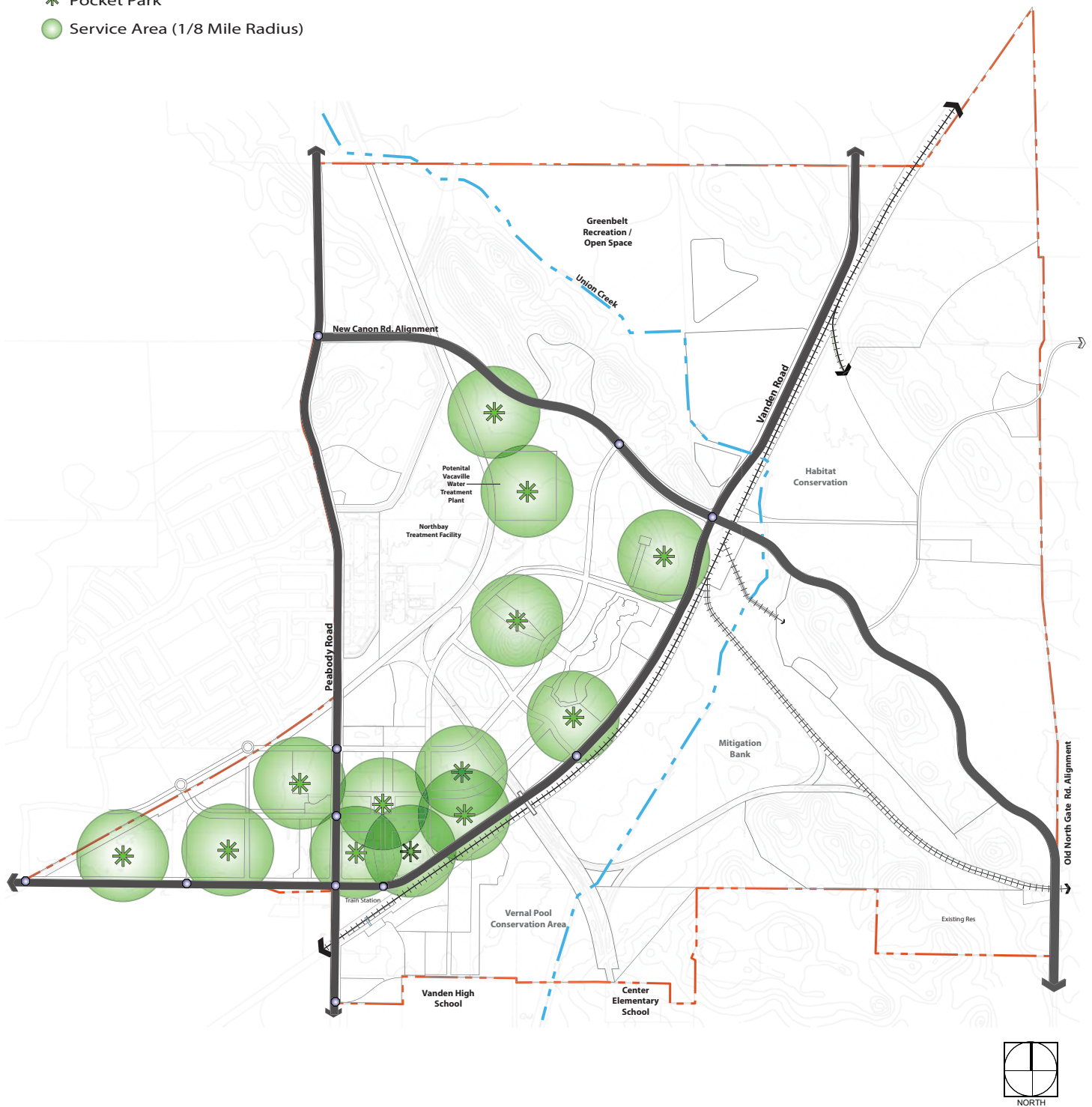


Figure 9-9: Pocket Park Frequency Plan



Pocket Parks will provide flexible play areas, picnic areas, play structures, and/or tot lot areas at the local neighborhood level. They will also play an important role in creating individual neighborhood identity and “place making.” Fronting homes onto these parks whenever possible will help frame the park space and promote safety by increasing surveillance from adjacent homes. Incorporation of one-way streets around the pocket parks will help to create a more intimate scale of space and reduce the chance of conflicts between pedestrian and vehicular traffic. Strategic distribution of pocket parks will encourage walking and biking to these amenities and minimizing vehicular parking requirements. Parking for the pocket parks will generally be accommodated by on-street parking around the park.

9.6 Public Services Policies

9.6.1 Fire and Police Protection

Policy 9-1

Through Community Facilities Districts, new development in FTSSP shall provide funding to the City for fire, police, and emergency services to meet the needs of its residents, at levels consistent with adopted policies of the City. See Chapter 14, Implementation and Administration, for detail.

Fire Protection

Policy 9-2

The City shall select a site for the relocation of Station #39, substantially in accordance with the general relocation areas shown in City Needs Assessment Report, concurrent with its approval of the initial Master PUD Permit within PA4 or PA5, whichever occurs first.

Policy 9-3

Concurrent with the initial final map approval within PA4 or PA5, whichever occurs first, the landowner(s) for PA4 and PA5 shall:

- a. acquire and convey at no cost to the City the site for the relocation of Station #39; and
- b. construct a new fire station building up to a maximum of 5,000 square feet of building area, including the garage.

Policy 9-4

Until the new fire station has been constructed and deemed operational by the Fire Chief, the City shall:

- a. not issue any building permits for the construction of dwelling units within Planning Area 5; and
- b. issue building permits for a maximum of 500 dwelling units for Planning Area 4.

Policy 9-5

To provide appropriate fire apparatus for higher density, mixed-use development, new development in the FTSSP shall provide funding for the difference in the cost between a Quint fire apparatus and fire engine. This funding may occur through a Backbone Infrastructure Community Facilities District or through the FTSSP Impact Fee.

9.6.2 Schools

Policy 9-6

Landowners and City shall coordinate with TUSD in its development of the proposed elementary school.

9.6.3 Library

Policy 9-7

Landowners and City shall coordinate with Solano County in its development of the proposed branch library facilities, consistent with the Solano County Library Facilities Master Plan. City will allow shared parking in the mixed-use area to facilitate library construction.

9.6.4 Parks and Recreation

Policy 9-8

For every residential single-family subdivision of 100 dwellings and attached product site plan of 150 dwellings a minimum of 1 place-making element located in a highly visible location shall be developed. Place-making elements may include but are not limited to: pocket park, paseo, play area, plaza, square, water feature, iconic structures, and/or other features approved by the Director.

Policy 9-9

Within each Planning Area, neighborhood parks shall be provided through the Master PUD Permit process to meet the requirements of Table 9-1.

Policy 9-10

Additional acreage beyond the minimum requirements of Table 9-1 by neighborhood type may be necessary to meet the Neighborhood Park requirements based on specific planning configurations at the time of Master PUD Permit.

Policy 9-11

A minimum 1 acre of park land shall be provided in PA1 in each of the locations shown on the land use plan (separate from the open space required within an individual development) in accordance with the Park Requirement and Dedication Summary Table (Table 9-4).





Policy 9-12

Pocket parks shall be programmed into residential neighborhoods/ parcels. These HOA maintained spaces are intended to be modest in nature and passive in character providing an intimate open space amenity and flexible play/picnic area for immediate residents. Location and configuration of pocket parks should be based on specific site planning details but shall be provided at a frequency of one per 2 acres. Only those pocket parks consistent with Policy 9-13 shall be eligible to receive credit for the land dedication requirement of the City's Quimby ordinance.

Policy 9-13

Parks less than 5 acres in size ("pocket parks") may be classified as neighborhood parks, provided they meet the following criteria:

- Minimum net area is 0.5 acre and minimum dimension is 100 feet;
- Area shall be improved with recreational facilities or amenities, appropriate to the size and location of the park;
- City determines that adequate public or private recreation facilities exist or are planning in the area;
- Park is maintained by a HOA; and
- Park is appropriately located within a residential neighborhood.

Policy 9-14

Each development project shall provide parks and recreational areas in accordance with the City of Fairfield Municipal Code Article III Division 13.

Policy 9-15

The FTSSP park and recreational program shall be consistent with the City of Fairfield General Plan except where modified by this document.

Policy 9-16

All recreational facilities shall be landscaped and contain irrigation systems, in accordance with the Landscape Plan in Chapter 12.0 of this Specific Plan.

Policy 9-17

Maintenance of public parks, landscaping in public rights-of-way and open space areas shall be provided in accordance with the policies in Chapter 14.0.

Policy 9-18

City shall maintain the Great Park through tax revenue from the proposed Open Space CFD and CFD 2006-1, paid by new development within the Specific Plan area. Until sufficient tax revenue from these two CFDs is generated from new development within the Specific Plan area for this purpose, City may require such development to maintain the Great Park.

Policy 9-19

City shall allocate the revenue collected within Specific Plan area from developers' payment of AB 1600 Park Impact Fees and 25 percent of the Bedroom Tax to complete the construction of parks within Specific Plan boundaries.



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10.0 TRANSPORTATION PLAN



10.0 TRANSPORTATION PLAN

July 26, 2011




10.1 Overview

This transportation plan is designed to accommodate the daily travel patterns of the FTSSP residents and visitors, as well as the regional circulation needs. The FTSSP development creates a compact project that provides a diverse mix of housing, shopping, employment, cultural, and recreational opportunities to encourage walking, bicycling, and other alternative transportation trips. The plan creates opportunities for residents to live, work, and play within the same community.

Fairfield is midway between San Francisco and Sacramento on I-80. The FTSSP development is designed to take advantage of the new Fairfield-Vacaville Multi-Modal Train Station, which has been planned collaboratively by the Cities of Fairfield and Vacaville, and the Solano Transportation Authority. The station will include a new Capitol Corridor passenger rail station, bus transfer facility, and park-and-ride lot to improve accessibility to the Capitol Corridor intercity train service. One of the goals of the Specific Plan is to provide sustainable development that would minimize the dependency on automobiles and encourage the use of alternative modes of transportation.

10.1.1 Regional Circulation

I-80 and SR-12 are the major roadways that provide circulation to the FTSSP area. I-80 is a major east-west freeway originating in the San Francisco Bay Area to the west, continuing east toward Sacramento, and terminating in New Jersey. I-80 crosses the cities of Fairfield and Vacaville in a southwest-to-northeast direction. In the cities of Fairfield and Vacaville, I-80 provides four mixed-flow lanes in each direction, with a posted speed limit of 65 mph. A fifth lane for high-occupancy vehicles (HOVs) is provided from Air Base Parkway to Red Top Road, and a sixth “auxiliary lane” is provided between I-680 and SR-12 East.



SR-12 is an east-west state highway in the project vicinity that extends from SR-49 in San Andreas to a junction with SR-1 near Bodega Bay in Sonoma County. Near the FTSSP area, SR-12 is a four-lane expressway with infrequent signals and a 50-mph speed limit. West of the FTSSP area, SR-12 joins I-80 for a segment of approximately 2 miles before splitting off to the west toward Napa County. SR-12 serves as an important commute route between I-80 and points to the east and provides access to the Sacramento-San Joaquin River Delta. For more information on regional circulation, please see the Traffic Report included as an appendix to the FTSSP EIR document.

10.1.2 Proposed Roadway Network

The proposed FTSSP roadway network includes multiple connections to the key arterials traversing the site, Peabody Road and Cement Hill Road/Vanden Road, and an internal network of two-lane roadways (with additional turn lanes where needed at intersections) that will efficiently distribute traffic while providing a high level of mobility for vehicles, bicycles, pedestrians, and transit vehicles (see Figure 10-1). Another key element of the proposed roadway network is the realignment of Canon Road, and grade separation of New Canon Road and the railroad tracks in order to provide sufficient roadway and capacity between the eastern industrial area and the western portion of the Specific Plan area, as well as external trips from the industrial area to Vacaville and to the rest of Fairfield and the region.

The realigned New Canon Road would also be extended in a northwesterly direction to Peabody Road, providing a new connection between Vanden Road and Peabody Road that would serve both internal project trips and trips between the Specific Plan Area and external destinations such as Vacaville and Travis AFB. This extension would be four lanes for a portion of the roadway nearest Vanden Road, then transition to two lanes for the rest of the roadway west to Peabody Road. McCrory Road would be extended to the southwest from its current terminus at North Gate Road, to intersect New Canon Road, providing access to the new industrial development, and North Gate Road would also be realigned to intersect New Canon/McCrory.

Figure 10-2 depicts the streets within the Specific Plan area that do not allow for driveways serving individual residential or industrial lots.



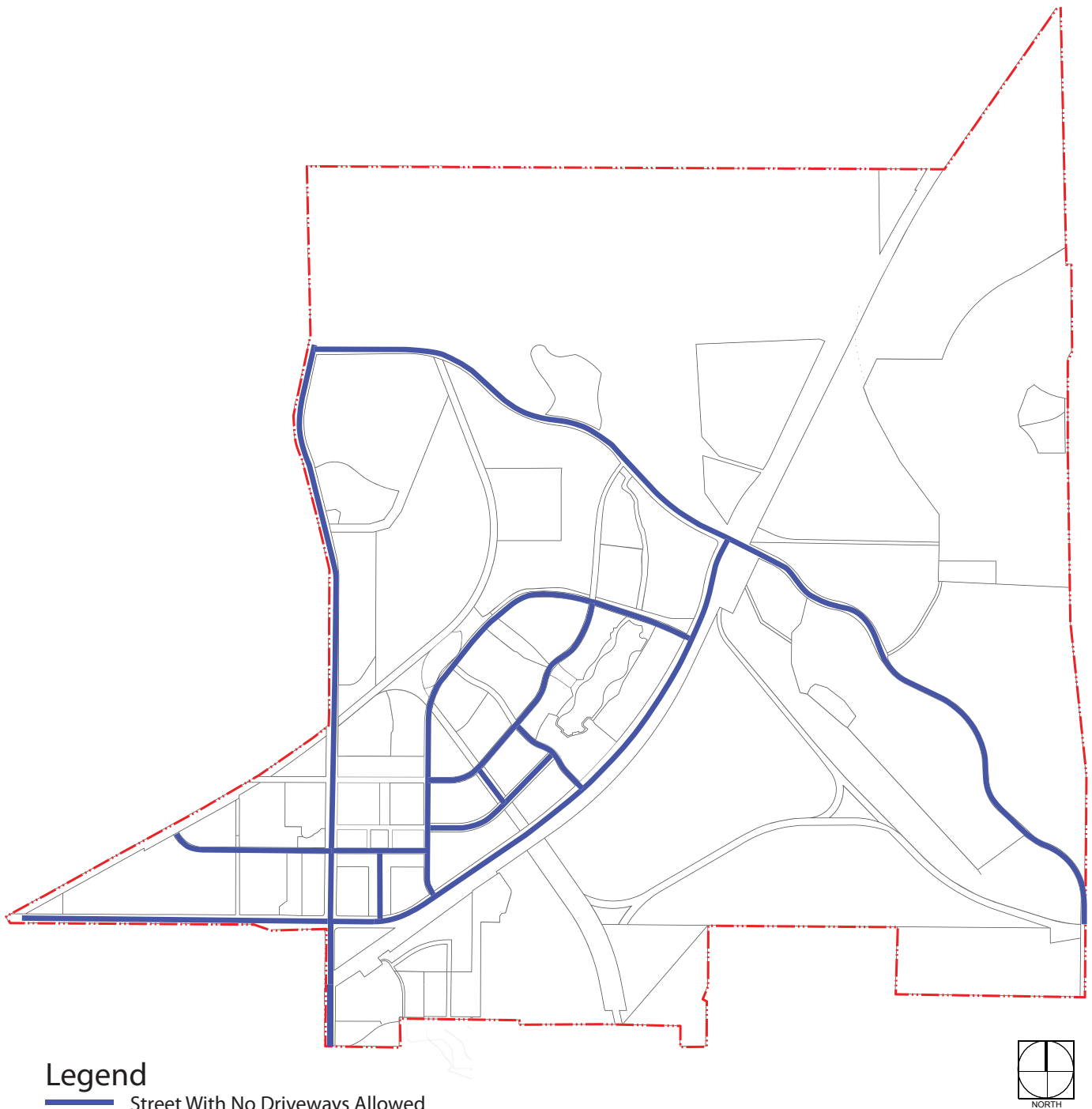
Legend

- ● ● Vanden Road
- ■ ■ Vanden Road between Peabody and E St
- ▤ ▤ ▤ ▤ Cement Hill / Peabody Road - Six Lanes
- ● ● ● Cement Hill / Peabody Road - Four Lane
- ■ ■ ■ Industrial Collector - Four Lanes
- ■ ■ ■ New Canon - Four Lanes
- ■ ■ ■ New Canon - Two Lanes
- ■ ■ ■ Connector with Enhanced Median
- ■ ■ ■ Industrial - Two Lanes
- ● ● ● Thematic Street
- — — Connector
- — — Connector with Multi-Use Trail
- ● ● ● Local Residential
- Signalized Intersection



1. Streets alignments shown on the plan are conceptual. Alignments may be revised based on design refinement at the time of Master PUD Permit development.
2. Street cross-sections shown in Figures 10-3 through 10-26 do not depict intersections. Number of lanes at intersections will be based on eIR and subsequent traffic studies.
3. Right-Of-Way includes both City easements and fee title portions.

Figure 10-1: Roadway Section Key Map



1. Streets alignments shown on the plan are conceptual. Alignments may be revised based on design refinement at the time of Master PUD Permit development.
2. A limited number of driveways serving industrial parcels on New Canon Road may be allowed by the City on a case-by-case basis.

Figure 10-2: Streets with No Driveways

10.2 Roadways and Vehicular Circulation

The goal of the transportation plan is to provide convenient vehicular access to the various land uses through a safe and efficient hierarchy of roadway systems. The following roadways are designed to fit the FTSSP community:

10.2.1 Arterials

Arterial roadways are designed to carry high volumes of traffic and to allow for efficient movement of vehicles through major intersections. On-street parking is generally not permitted.

Vanden Road

Vanden Road is an arterial roadway that traverses the Specific Plan area northeast to southwest. It is designed adjacent to development with an ultimate right-of-way (ROW) of 159 feet and a minimum median of 16 feet (see Figure 10-3 and 10-4). It includes four traffic lanes, and a 10-foot regional trail. A 10-foot landscape parkway would be included between the travel way and the railroad ROW.

Cement Hill Road (Manuel Campos Parkway)/Peabody Road

Another arterial intersection within the Specific Plan area is Cement Hill Road (Manuel Campos Parkway)/Peabody Road. Peabody Road runs north-south along the western boundary of the project site and intersects Cement Hill Road (Manuel Campos Parkway) near the southwest corner of the project site. They are designed with an ultimate ROW of 156 feet and a median of 16 feet (see Figure 10-5). They include six traffic lanes and 10-foot multi-use trails. Additional Peabody Road sections are provided in Figure 10-6 and 10-7.

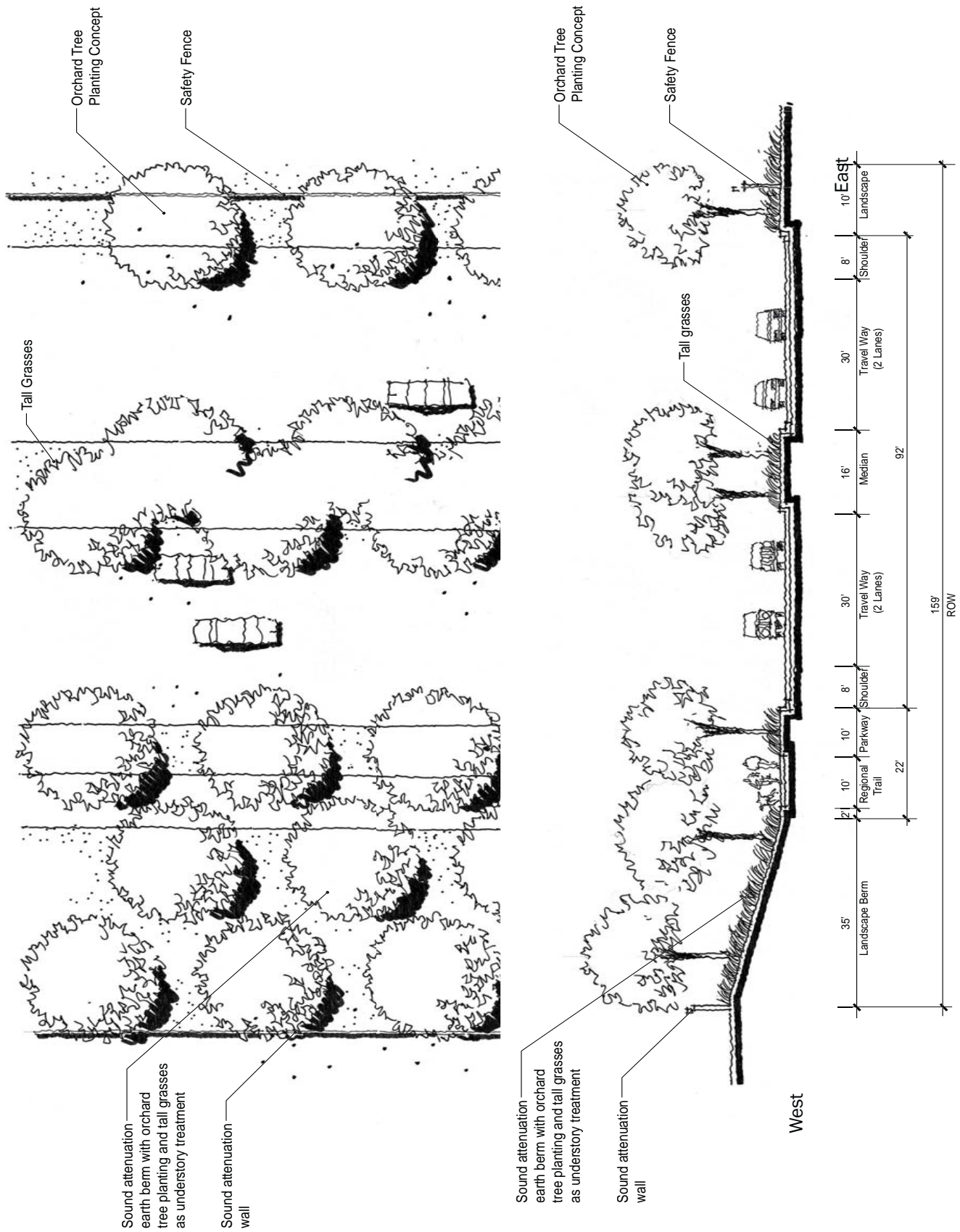
New Canon Road

New Canon Road is a proposed arterial that will traverse the Specific Plan area northwest to southeast, intersecting with Vanden Road and crossing the railroad ROW. This arterial varies in number of lanes, from two to four, depending upon proximity to Vanden Road (see Figures 10-8 and 10-9). The portion of New Canon Road that intersects with Vanden Road and crosses the railroad ROW is a four-lane arterial with an ultimate ROW of 110 feet minimum that has a median of 16 feet and extends to the industrial collector. It includes 5-foot bike lanes and a 6-foot sidewalk on the development side.

As New Canon Road extends past the intersection to the northern industrial/employment area, it becomes a two-lane arterial with an ultimate ROW varying with 74 feet minimum (see Figure 10-8). It includes 5-foot bike lanes and a 6-foot sidewalk adjacent to development.

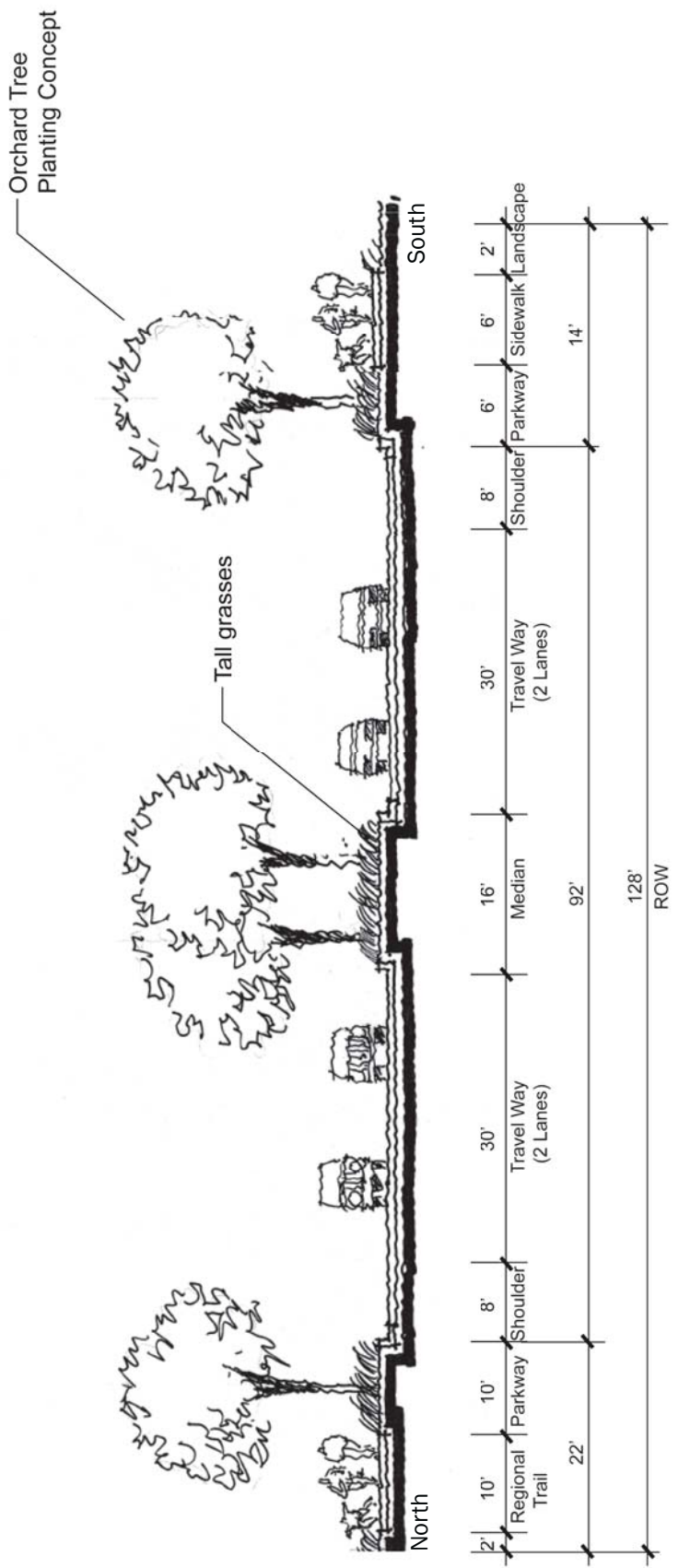
New Canon Road shall not have access driveways, Figure 10-2; except a limited number of driveways serving industrial parcels taking access from New Cannon Road may be allowed by the City subject to site plan review on a case-by-case basis.





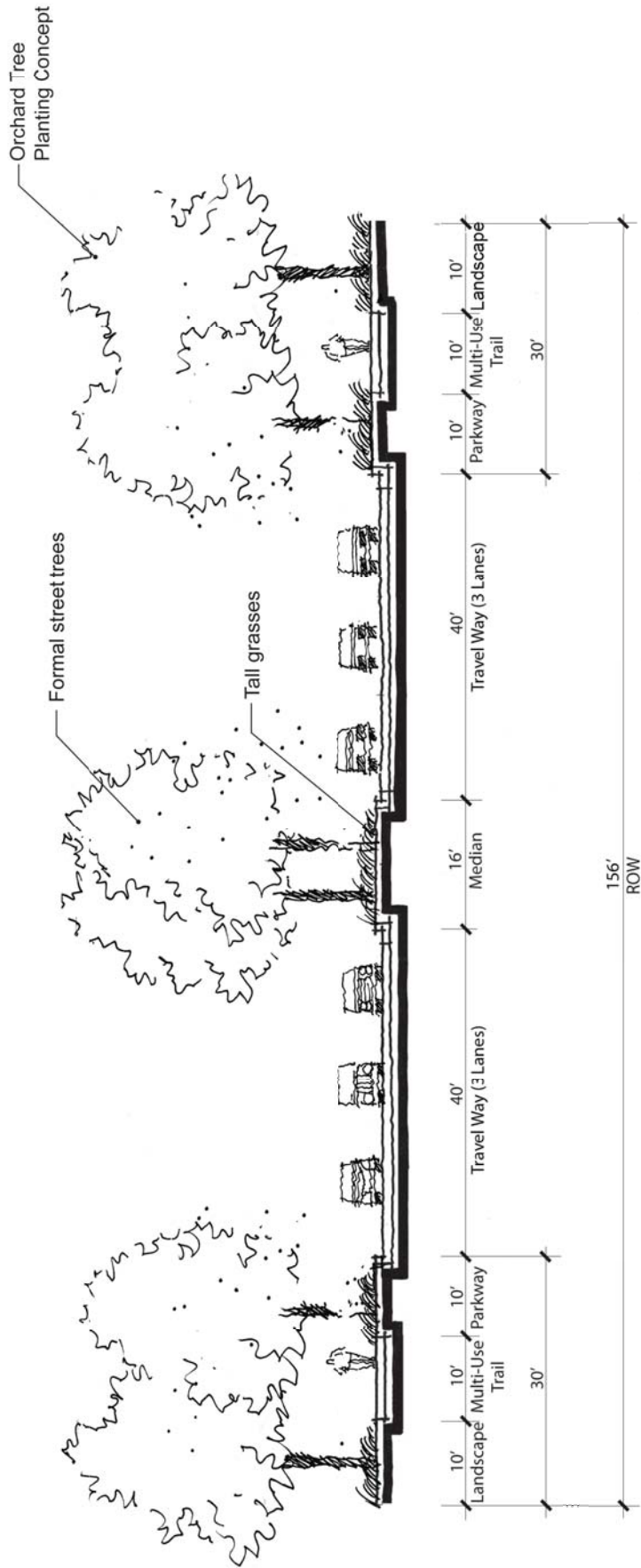
1. Additional noise attenuation conditions (without berm) are included in Chapter 12.0 Landscape.

Figure 10-3: Vanden Road 4-Lane and Plan View



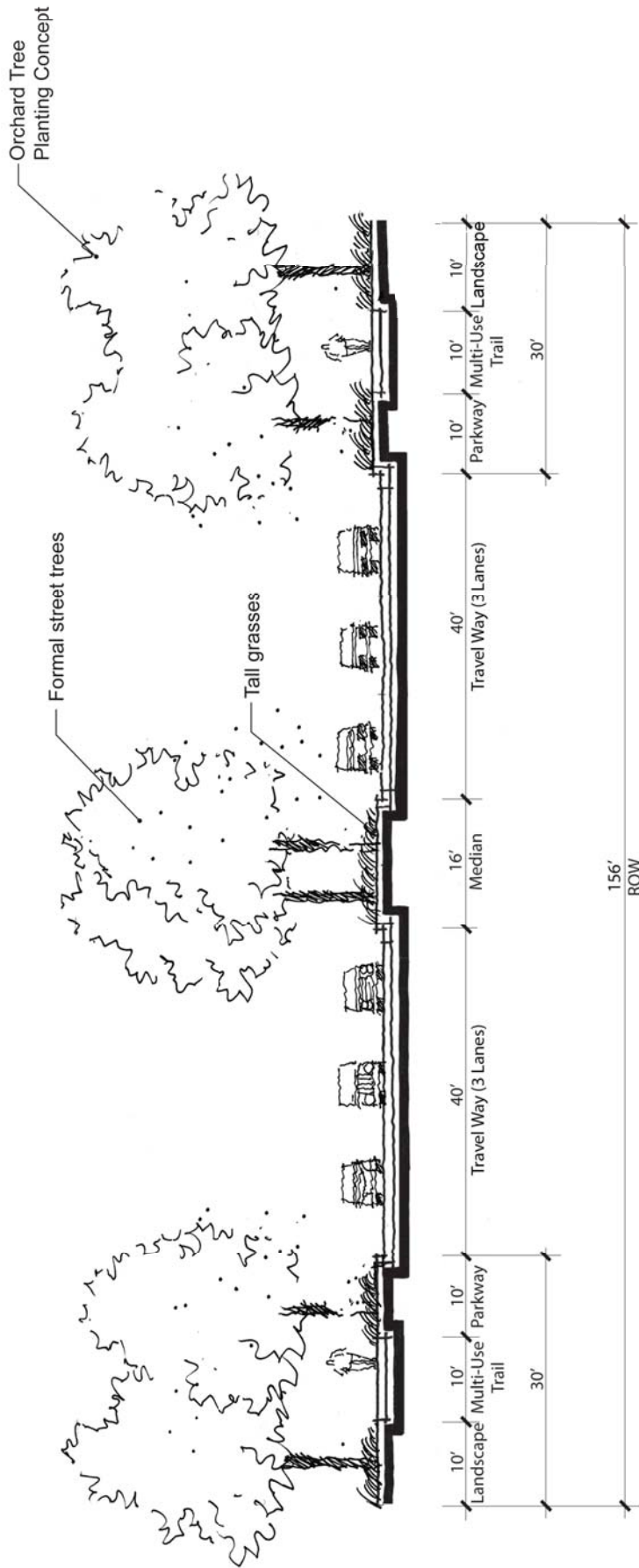
1. Additional noise attenuation conditions (without berm) are included in Chapter 12.0 Landscape.
2. Vanden Road segment from Connector Street to pedestrian overcrossing may have berm as shown in Figure 10-3, as determined through Master PUD Permit approval.

Figure 10-4: Vanden Road: Peabody Road to Connector Street



1. Right-Of-Way may vary due to North Bay Aqueduct structure near Manuel Campos/Walter Road Intersection

Figure 10-5: Cement Hill (Manuel Campos Parkway)



1. Peabody Road shall be a 6 travel lane road from Air Base Parkway to "A" Street intersection. Cross-section show is the maximum width of right-of-way. Right-of-Way will vary along this segment and will be reduced in certain areas due to existing and abutting development, bridge structure, and other constraints.
2. Peabody Road shall transition from 6 travel lanes to 4 travel lanes from "A" Street to Linear Park Trail. Any new street improvements from Linear Park Trail to New Canon Road shall conform to existing construction and approved plans.
3. A 10 foot multi-use trail shall be constructed on the east side of Peabody Road to New Canon Road intersection.
4. Informal clusters of street trees shall be placed where Peabody Road abuts open space.

Figure 10-6: Peabody Road: Air Base Parkway to "A" Street

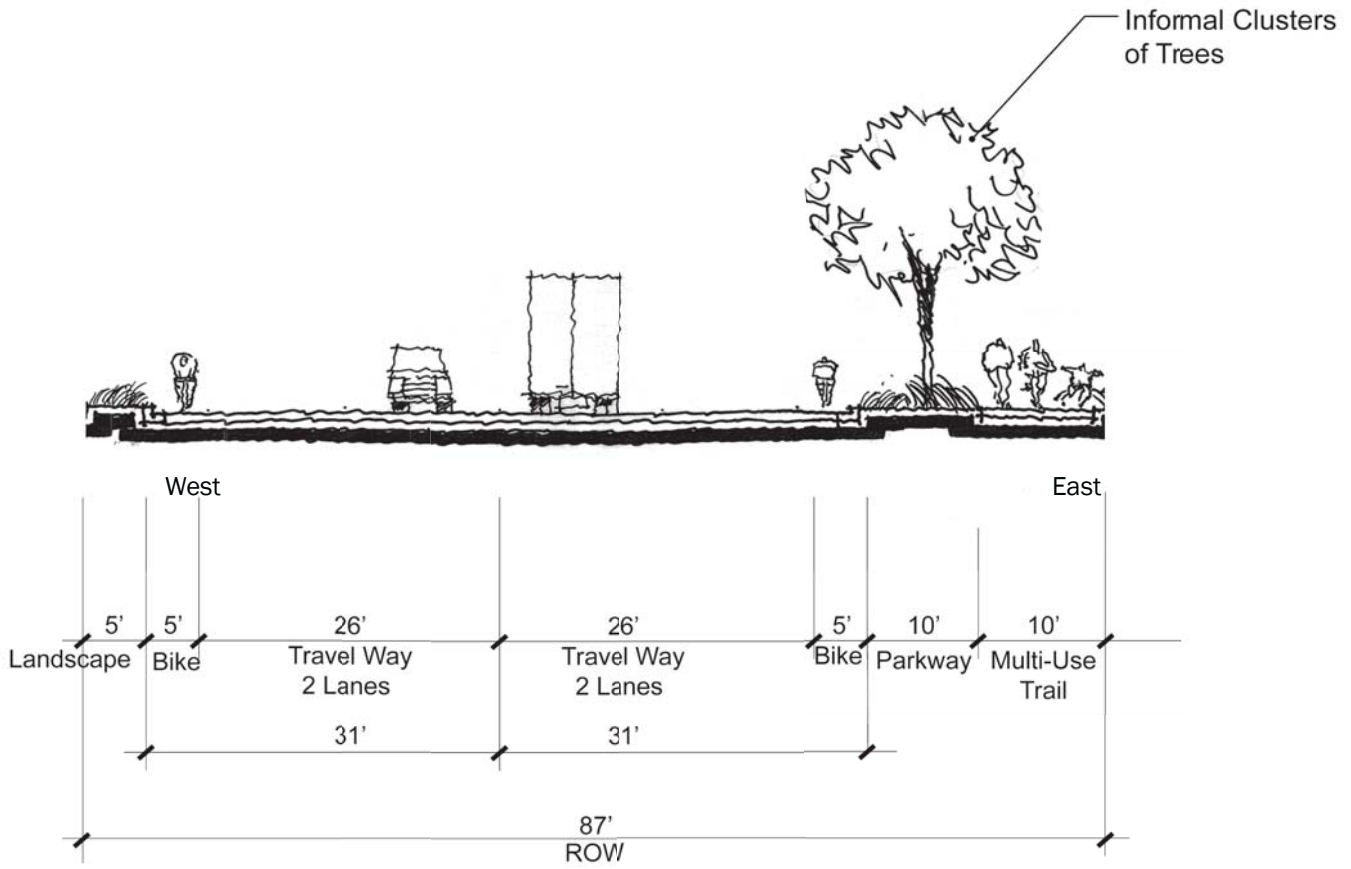
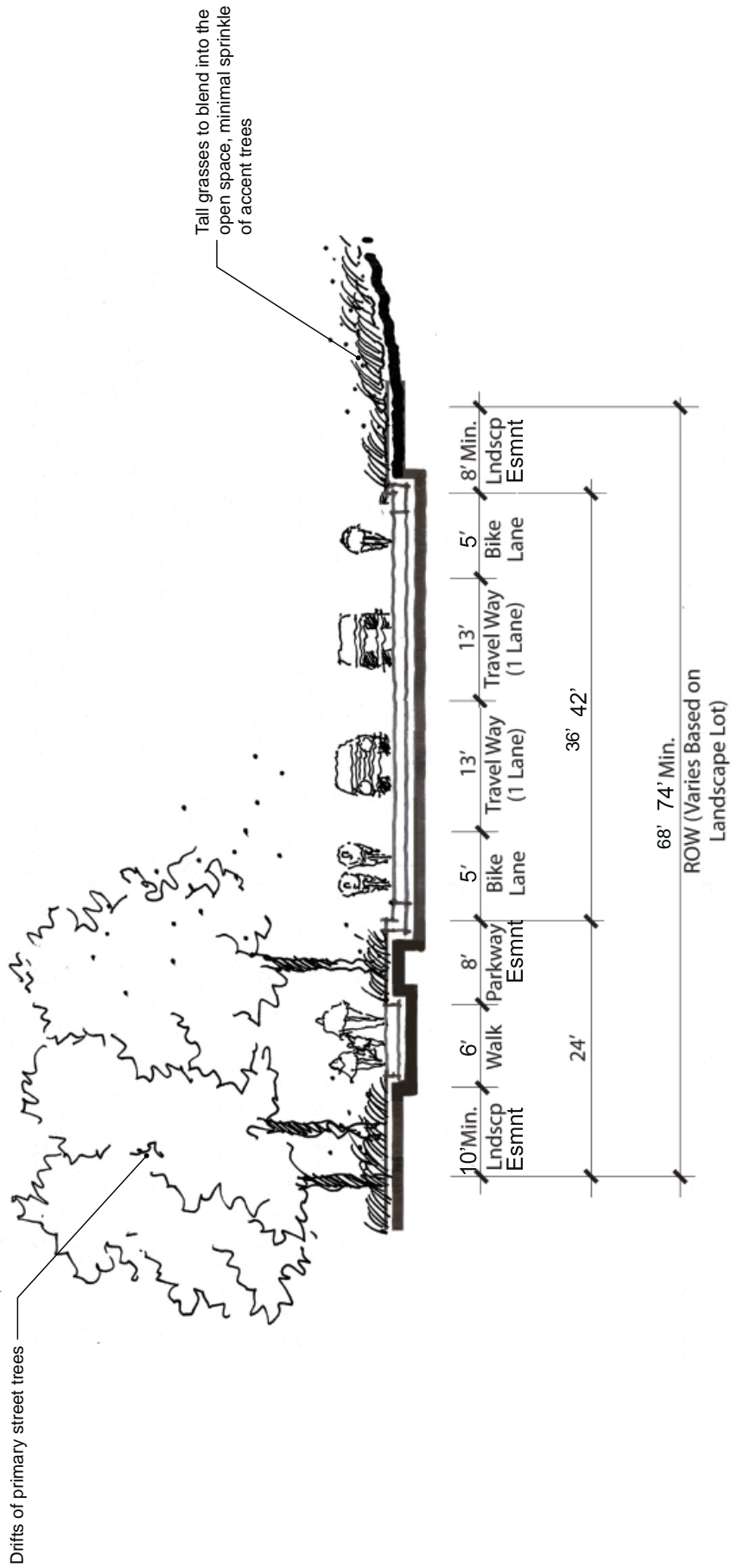
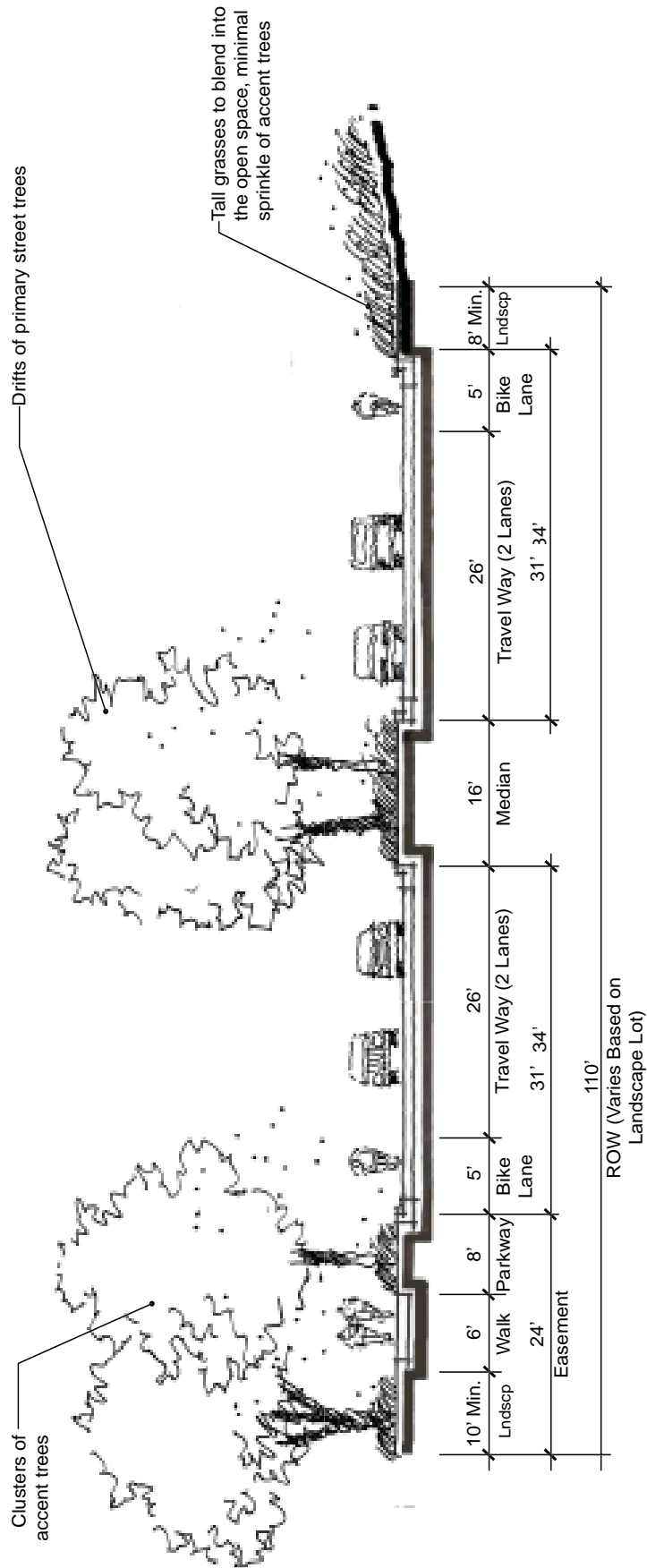


Figure 10-7: Peabody Road, North of New Canon Road



1. Segments of New Canon Road between Linear Park and Peabody Road shall have 10 foot multi-use trail in lieu of 6 foot sidewalk.
2. Ag fencing shall be provided along the right of-way where abutting open space.
3. Masonry wall shall be provided along right of-way where abutting residential.

Figure 10-8: New Canon Road: 2-Lane

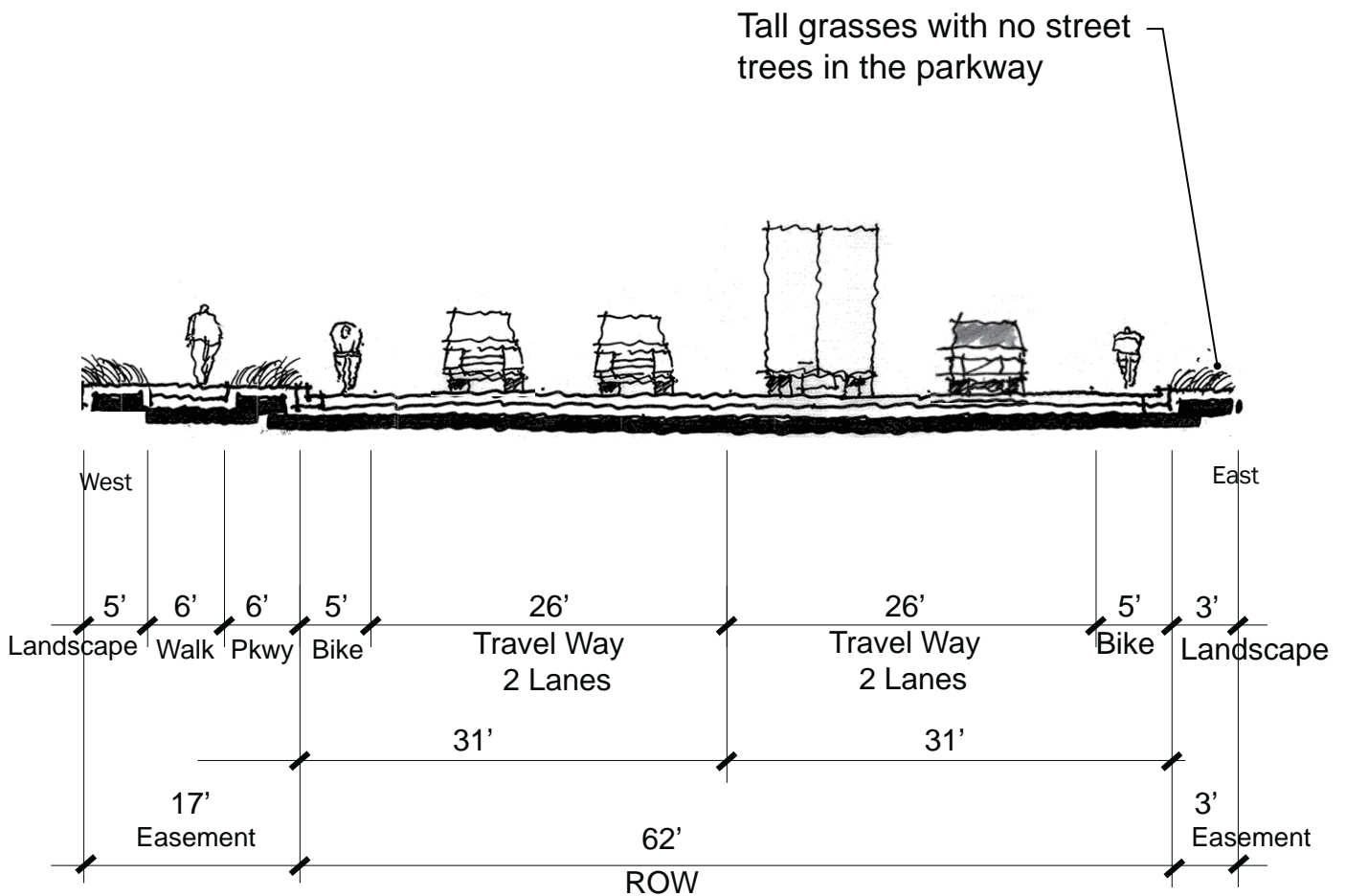


1. Ag fencing shall be installed along right-of-way where abutting open space.
2. Masonry wall shall be installed along right-of-way where abutting residential.

Figure 10-9: New Canon Road: 4-Lane

10.2.2 Industrial Collector

Collectors are designed to serve as intermediate routes handling traffic between arterials and connector streets. They are primarily designed to move traffic but also provide access to abutting properties. A collector road will connect New Canon Road to the industrial/employment area in the eastern portion of the Specific Plan area. It is designed with an ultimate ROW of 62 feet when adjacent to open space. The 62-foot ROW corresponds to the 4-travel lane configuration shown in Figure 10-10 below. A 50-foot ROW is required for a 3-lane configuration, and a 40-foot ROW is required for the 2 travel lane configuration.



1. Easement area includes public access easement, public utility easement and public landscape easement.
2. Ag fencing shall be installed along easement abutting open space.

Figure 10-10: Industrial Collector Adjacent to Open Space

10.2.3 Connectors

Connectors are lesser volume streets providing access off of collectors into residential neighborhoods, such as in PA2 and PA3. On-street parking is generally permitted. A connector is designed with an ultimate ROW of 74 feet with two traffic lanes, 7-foot parking aisles, and an additional 2-foot setback from property line to wall for required side-on conditions (see Figure 10-11).

Street B within PA2 shall be a Connector.

Connector with Multi-Use Trail

A connector with a multi-use trail is designed with an ultimate ROW of 82 feet with two traffic lanes, 10-foot multi-use trails, 7-foot parking aisles, and an additional 2-foot setback from property line to wall for required side-on conditions (see Figure 10-12). This type of connector can be found in PA2 and PA4 and extend into PA5. No driveways are allowed on streets with a 10-foot multi-use trail.

Street D within PA1 west of Peabody Road shall be a Connector with Multi-Use Trail.

Street E within PA2 shall be a Connector with Multi-Use Trail.

Connector with an Enhanced Median

A connector with an enhanced median is designed with an ultimate ROW of 152 feet with two traffic lanes, 8-foot bikes lanes, and a 60-foot median with activity nodes (see Figure 10-13). A connector with an enhanced median is planned for the Linear Park Trail northwest of the Civic Boulevard within the old train ROW.

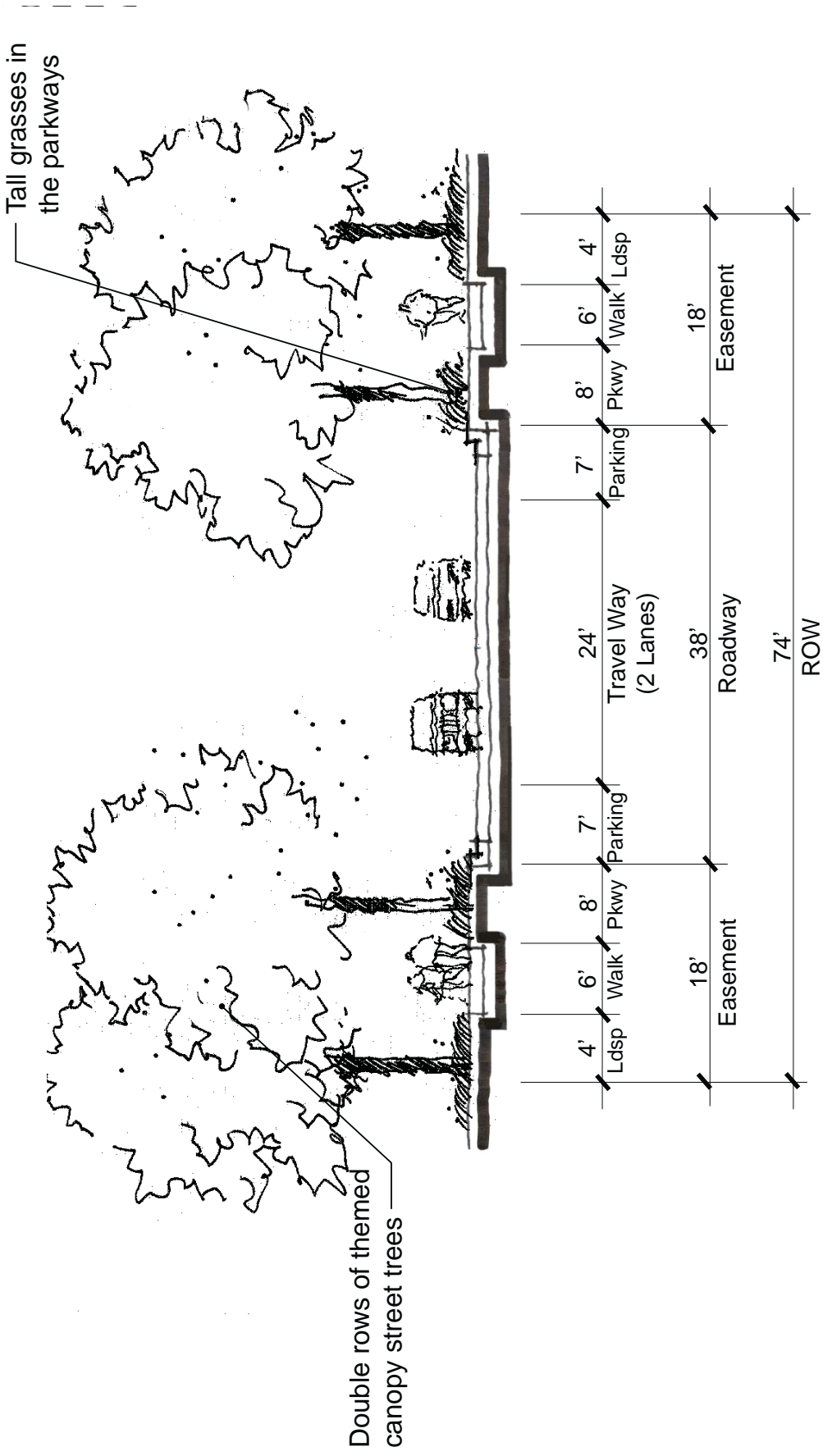
Street H within PA2 shall be a Connector with Enhanced Median.

Industrial Connector

Within the Industrial/Employment area of the Specific Plan area, an industrial connector will provide access off of the industrial collector. An industrial connector is designed with an ultimate ROW of 76 feet with two lanes and a 6-foot wall setback (see Figure 10-14).

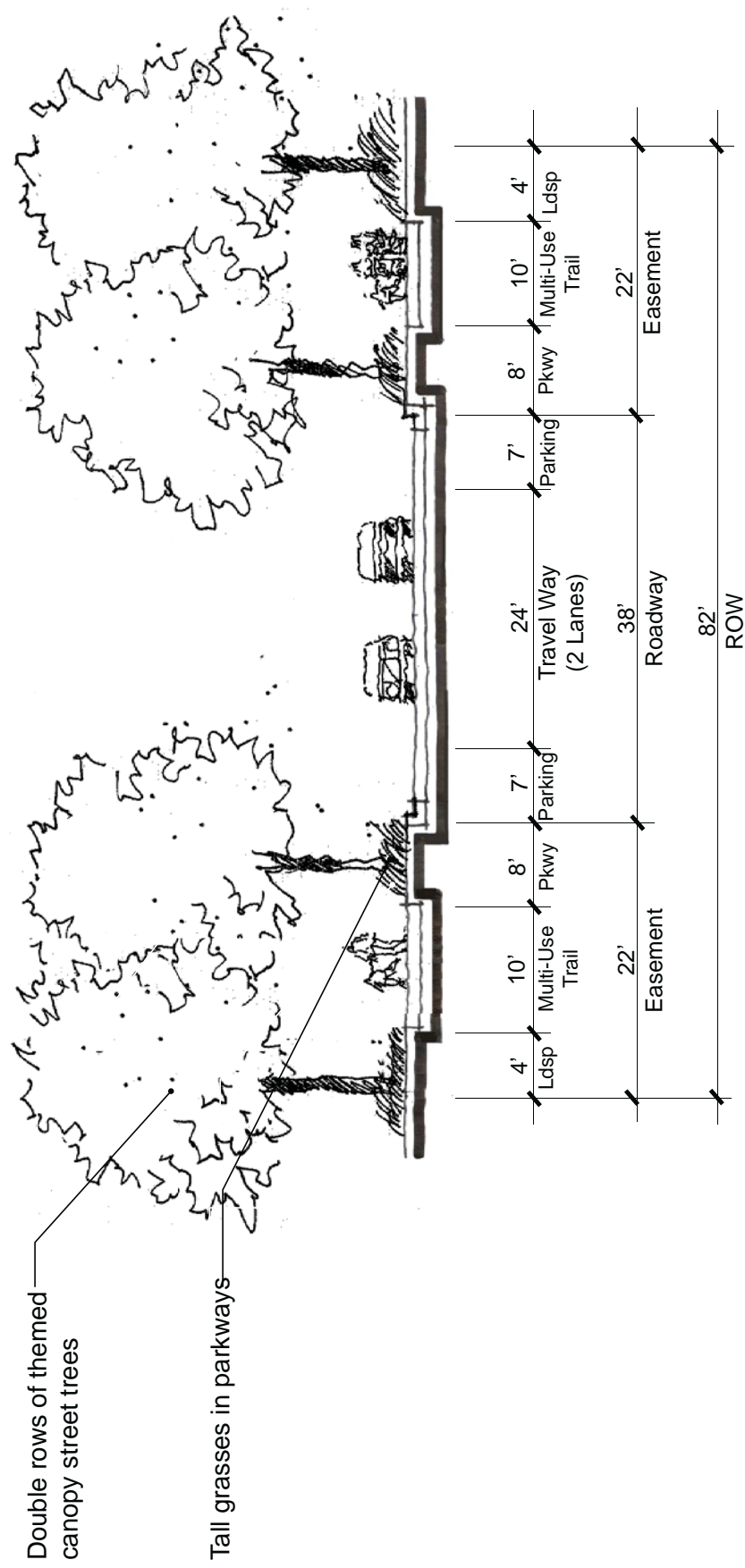
10.2.4 Civic Boulevard

The Civic Boulevard is a Thematic Street intended to serve as a symbolic roadway within the FTSSP community. It is the roadway linking the Town Center to the Lake Park and features iconic landscaping and enhanced building setbacks. On-street parking is generally permitted. The Civic Boulevard is designed with an ultimate ROW of 114 feet and a minimum median of 24 feet (see Figure 10-15). It includes 10-foot multi-use trails, 7-foot parking aisles, and an additional 2-foot setback from property line to wall for required side-on conditions.



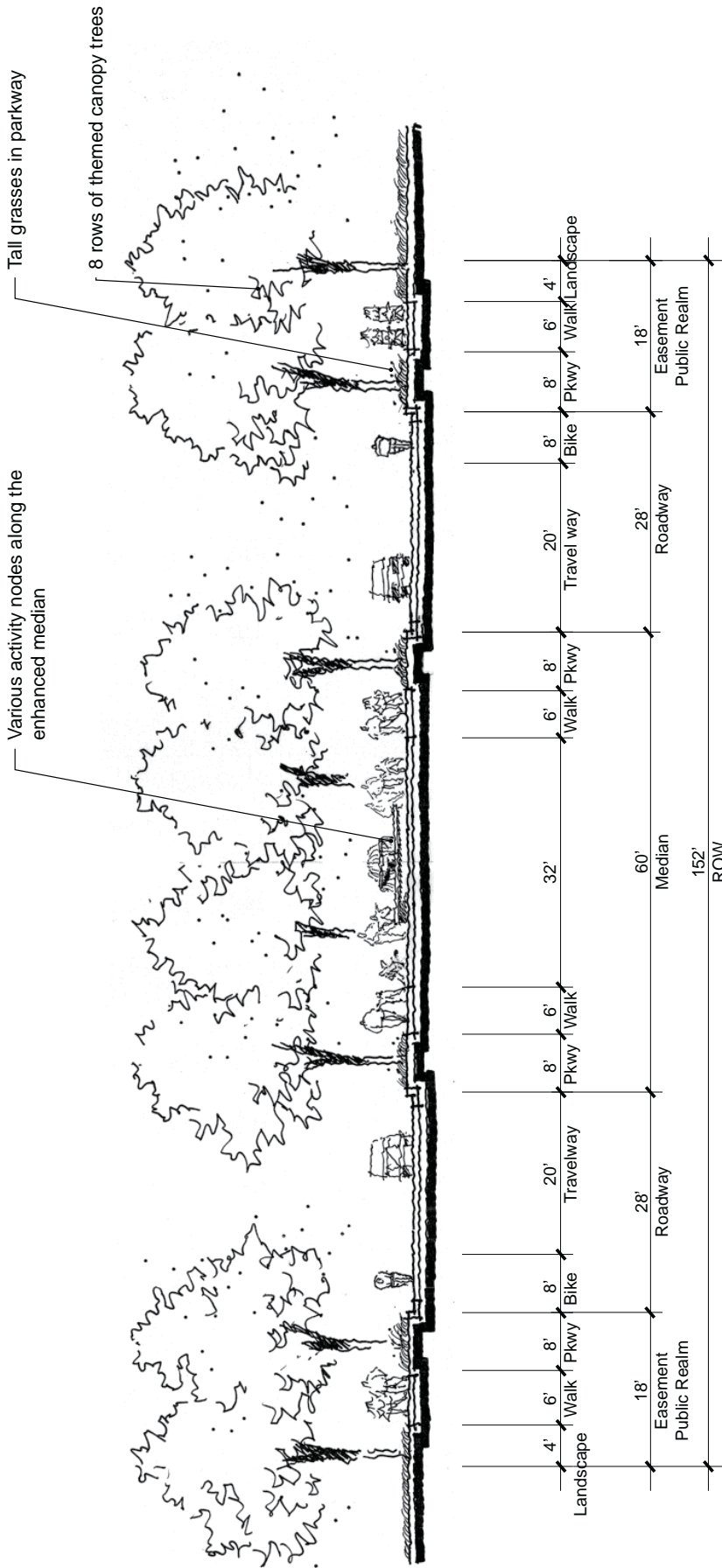
1. Easement area includes public access easement, public utility easement and public landscape easement.
2. Additional 2-foot setback required for side-on conditions.

Figure 10-11: Connector



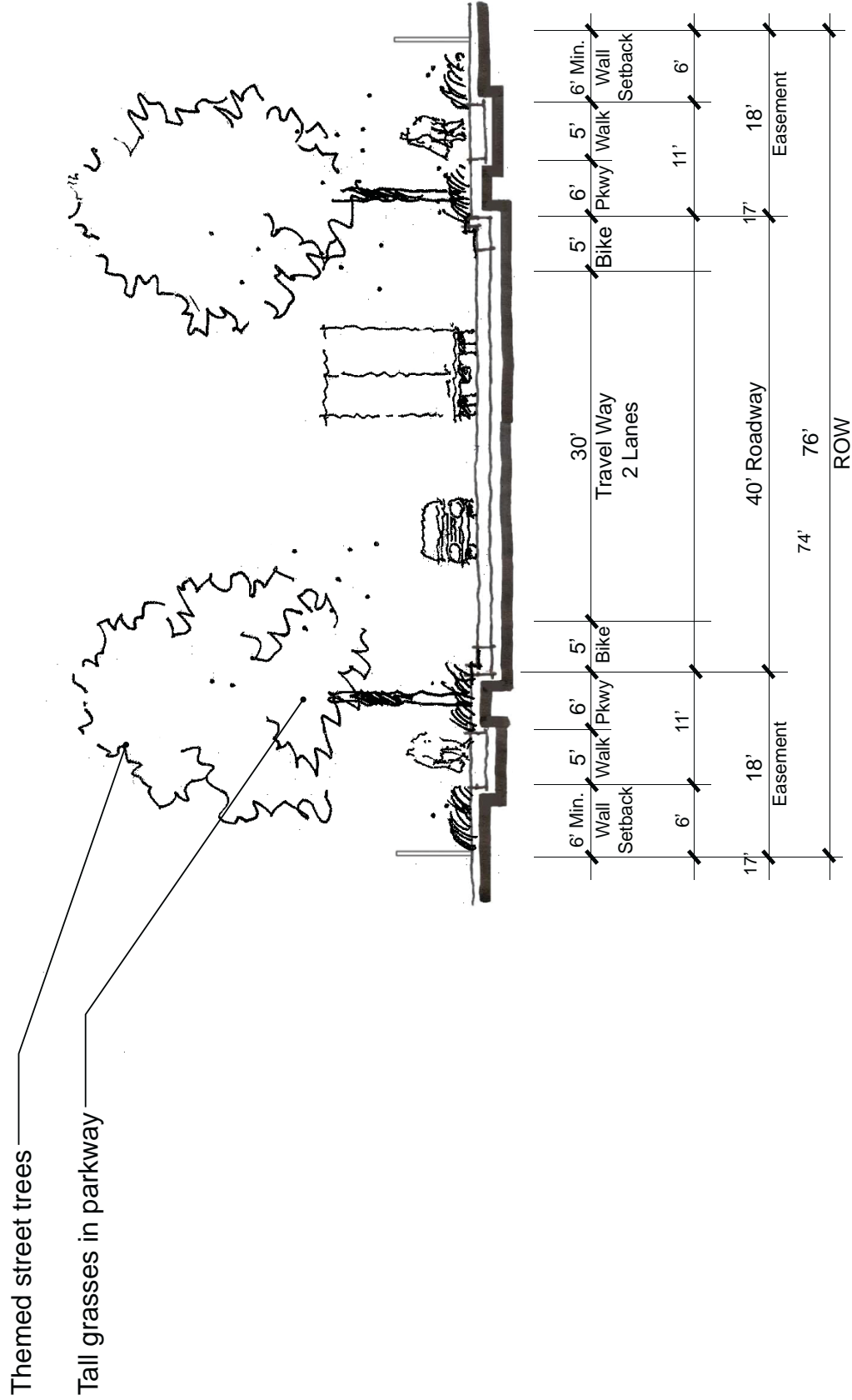
1. Easement area includes public access easement, public utility easement and public landscape easement.

Figure 10-12: Connector with Multi-Use Trail



1. No on-street parking allowed.
2. Easement area includes public access easement, public utility easement and public landscape easement.

Figure 10-13: Connector with an Enhanced Median - Street H PA2



1. Easement area shall include public access easement, public utility easement and public landscape easement.

Figure 10-14: Industrial Connector

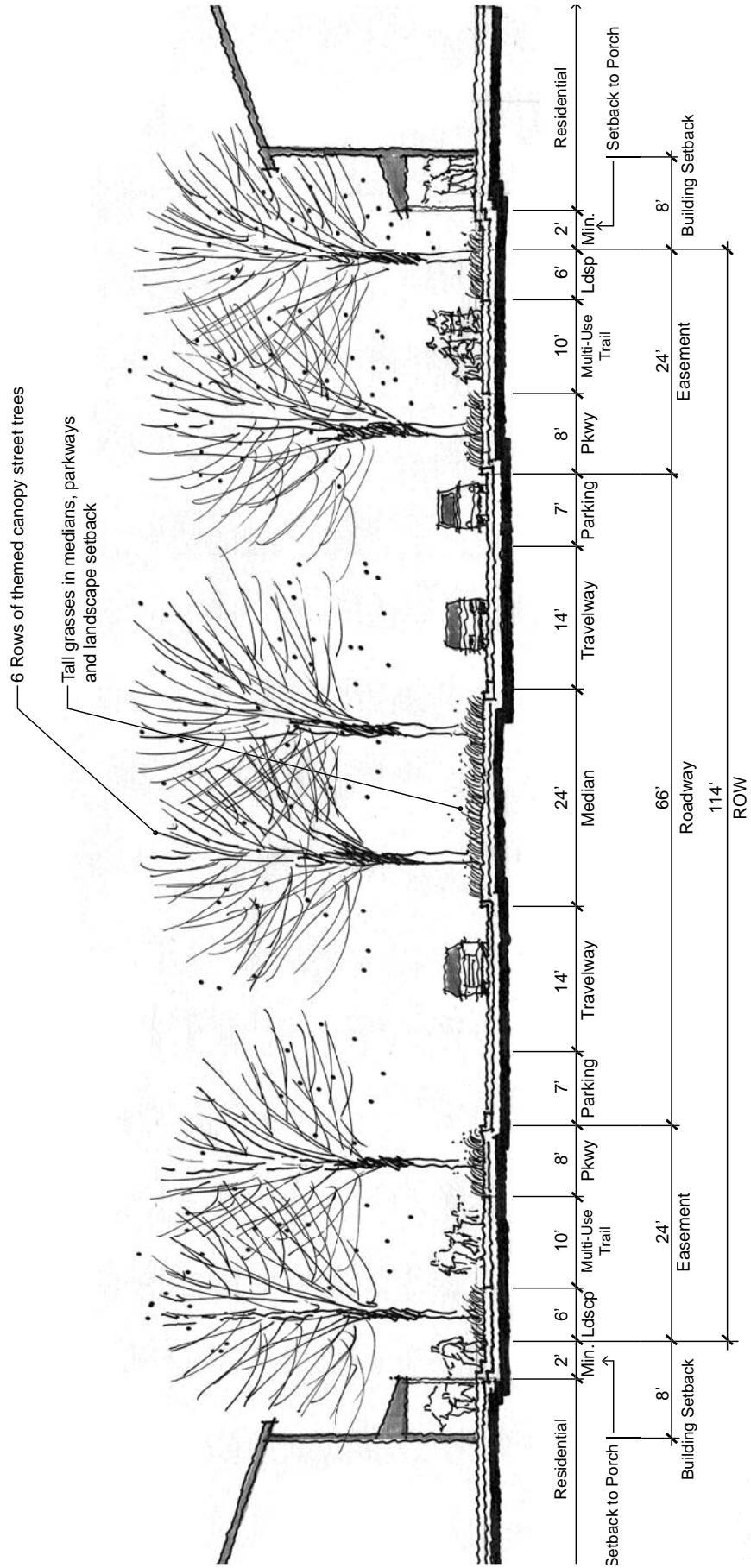


Figure 10-15: Civic Boulevard



10.2.5 Main Street

A Main Street is designed to provide access into the downtown of the Project. On street parking, parallel or angled, is permitted.

Main Street within PA2 has improved sections of Figure 10-16 and Figure 10-17.

Main Street with Parallel Parking

This roadway is designed with an ultimate ROW of 70 feet and includes 8-foot parking aisles and 15-foot walkways with tree wells (see Figure 10-16).

Main Street with Angled Parking

This roadway is designed with an ultimate ROW of 96 feet and includes 21-foot parking aisles and 15-foot walkways with tree wells (see Figure 10-17).

Street G within PA4 shall utilize Figure 10-17, Main Street with Angled Parking.

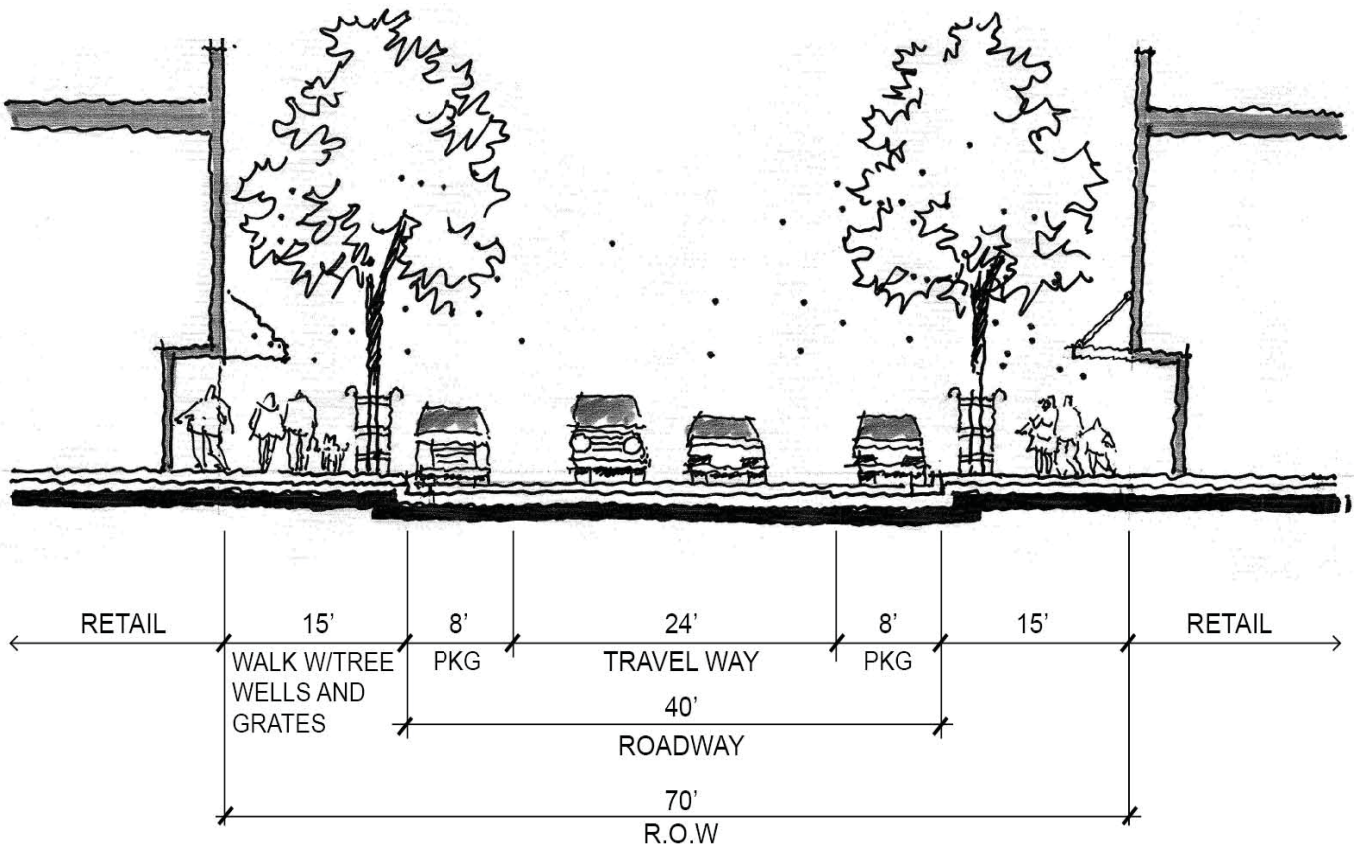
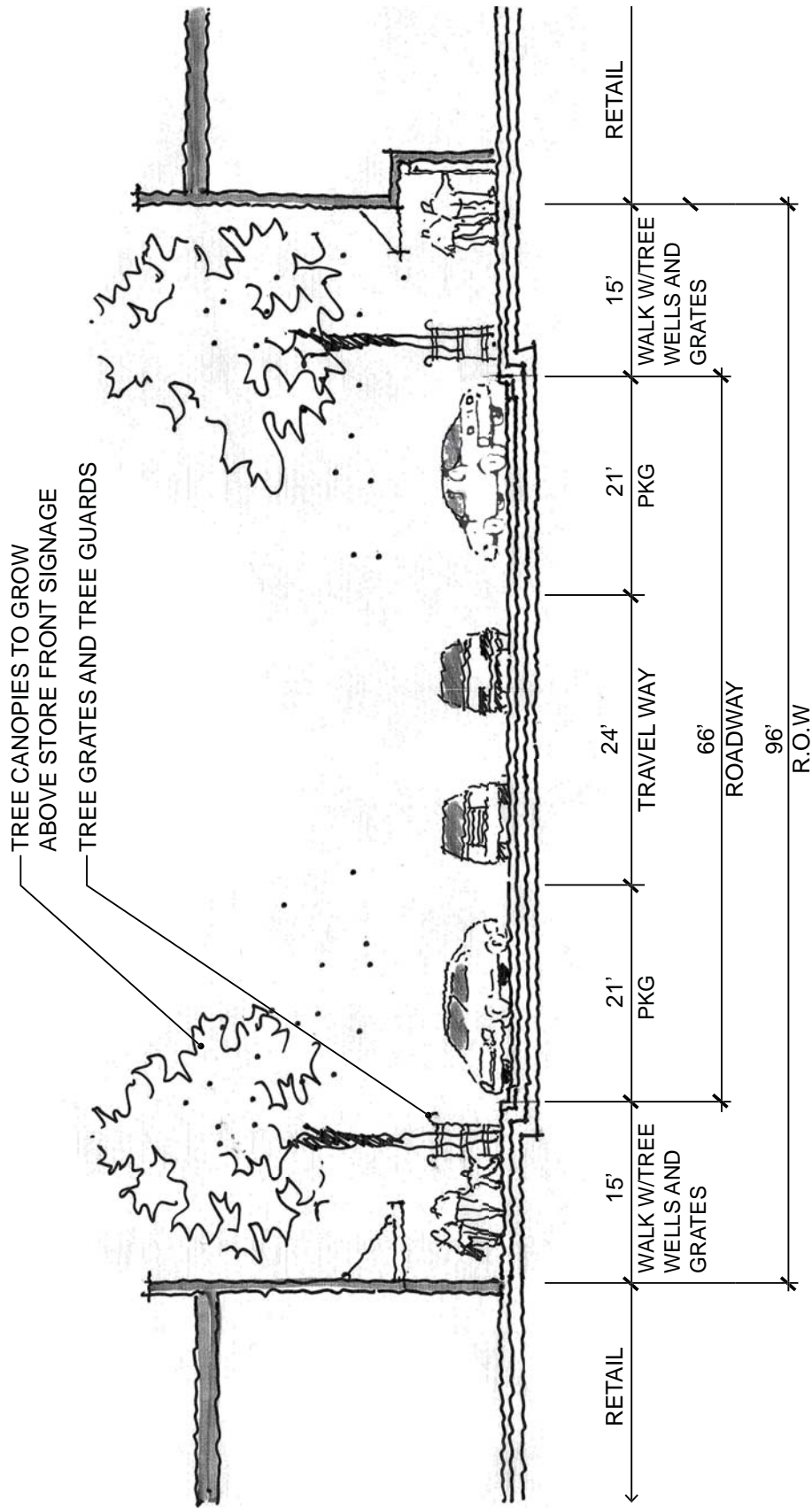


Figure 10-16: Main Street with Parallel Parking



1. On-street parking shall be installed at 60 degree angle.

Figure 10-17: Main Street with Angled Parking



10.2.6 Thematic Streets

Thematic Streets play a part in creating civic space and pulling a neighborhood together. The goal is to orient living activity towards the street, emphasizing architectural detail and including interactive architecture including porches, courtyards, entries, windows and second-story balconies. Several thematic streets have been incorporated into the urban design framework to create a more diverse, interesting and memorable urban neighborhood. These streets are planned to provide a unique identity that will help 'brand' the surrounding neighborhoods (see Figures 10-15 through 10-21).

A Street

A Street is a Thematic Street within the Lake District envisioned as a romantic boulevard that sits at the base of the hill overlooking the Lake Park (see Figures 10-18 and 10-19).

B Street

The north/south pedestrian axis through PA2 is continued by Thematic Street B Street. This connection also provides the opportunity for ground floor commercial uses, live/work and/or Home Based Business (see Figure 10-20).

D Street

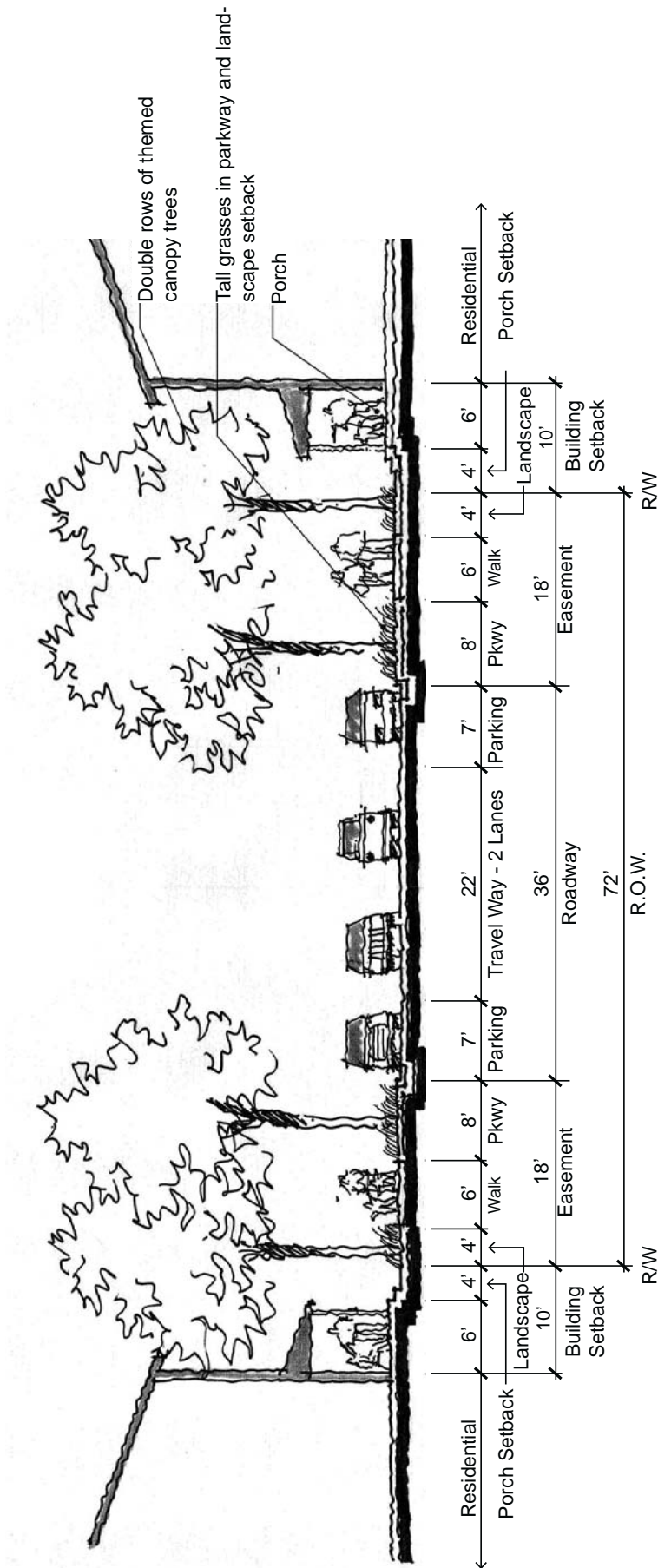
Thematic Street D transitions from Main Street to the Civic Boulevard (see Figures 10-21 and 10-22).

E Street

The north/south pedestrian axis from the Train Station up to the Town Square through PA2 is established by Thematic Street E Street. This connection provides the opportunity for ground floor commercial uses, mixed-use, live/work and/or high density residential in proximity to transit and the activities of the Town Center (see Figure 10-23).

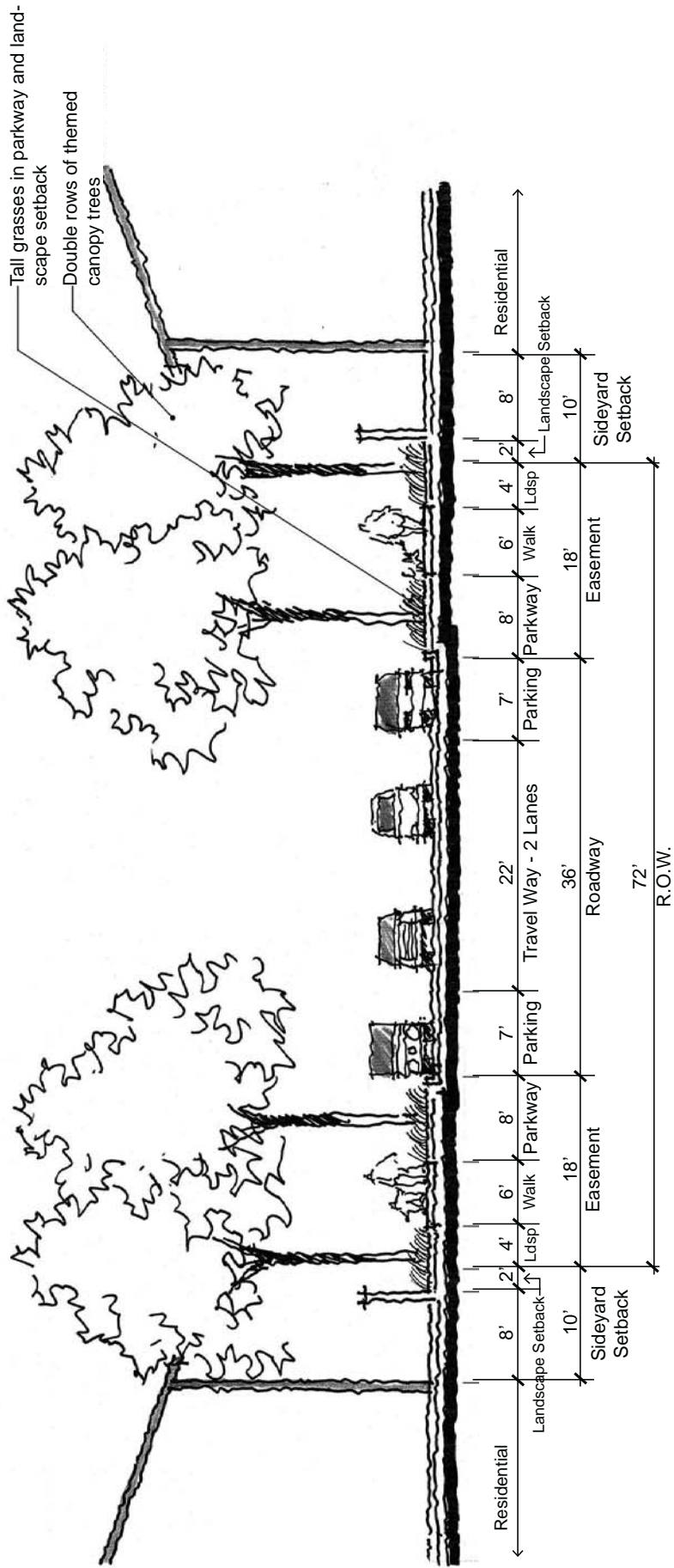
G Street

G Street will mimic the Main Street within PA2, bringing angled parking, pedestrian scaled environments and urban setbacks to the retail/commercial setting of the Village Core (see Figure 10-24).



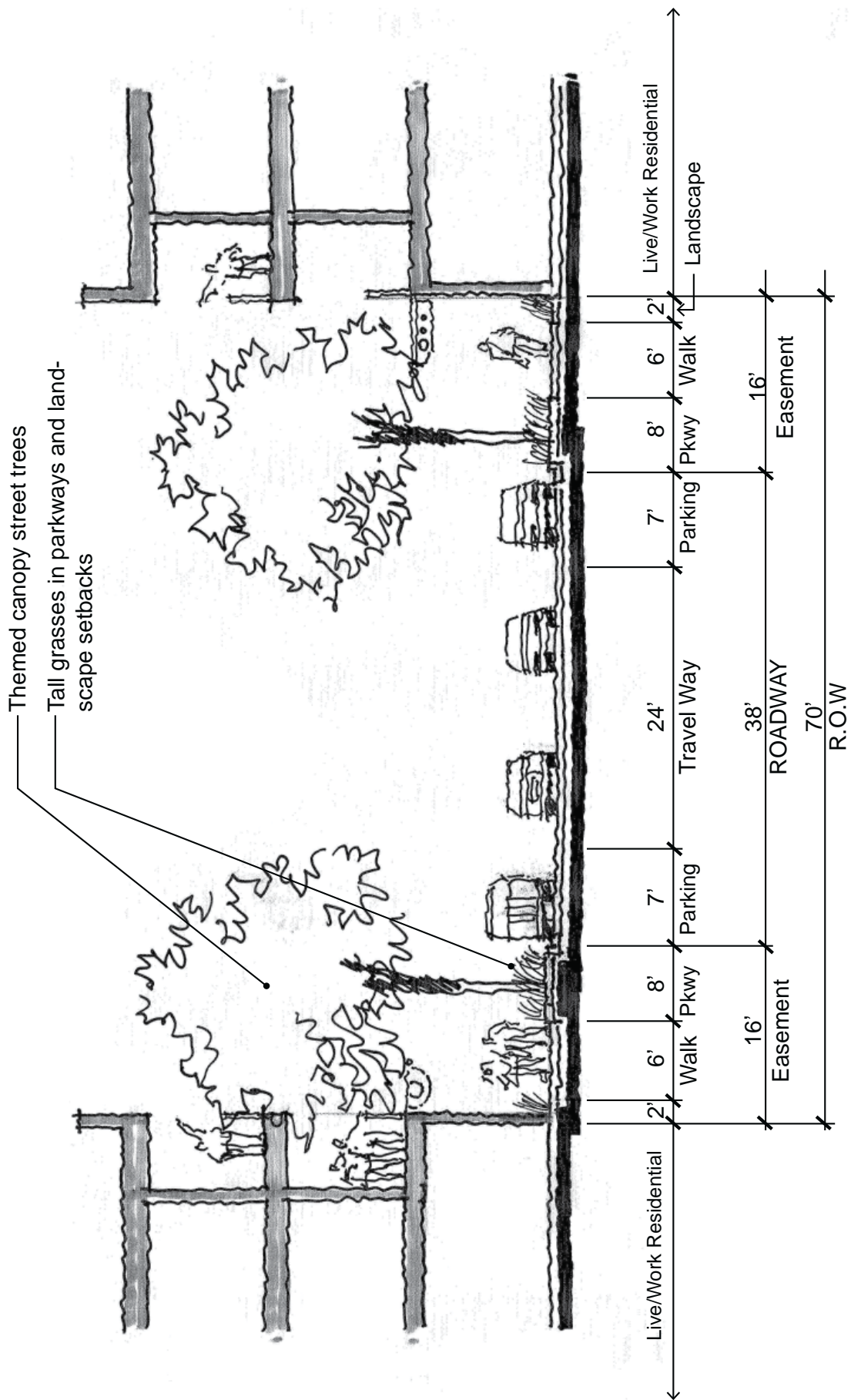
1. Easement area shall include public access easement, public utility easement and public landscape easement.

Figure 10-18: Street A - Alt 1 PA4



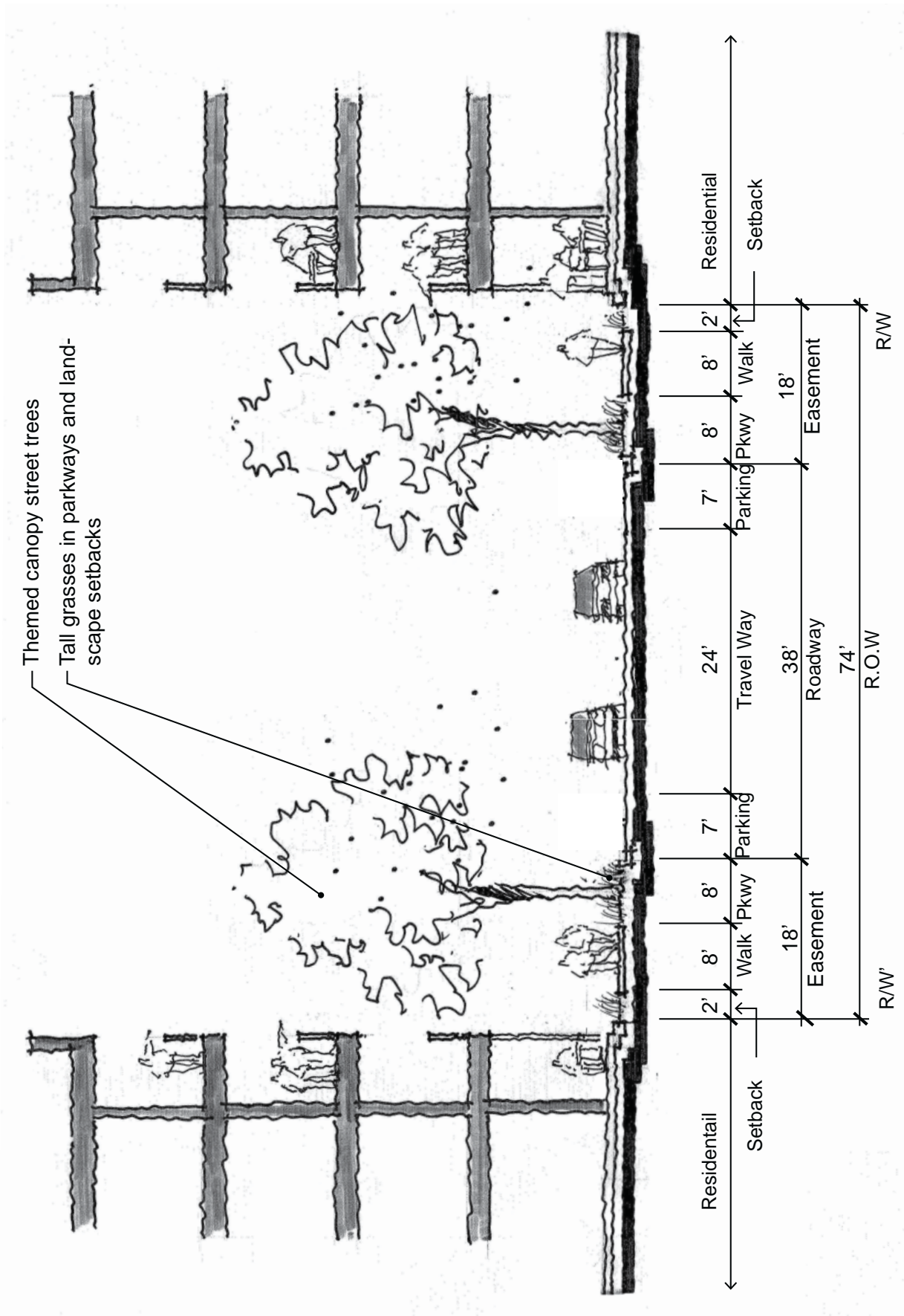
1. Easement area shall include public access easement, public utility easement and public landscape easement.

Figure 10-19: Street A - Alt 2 PA4



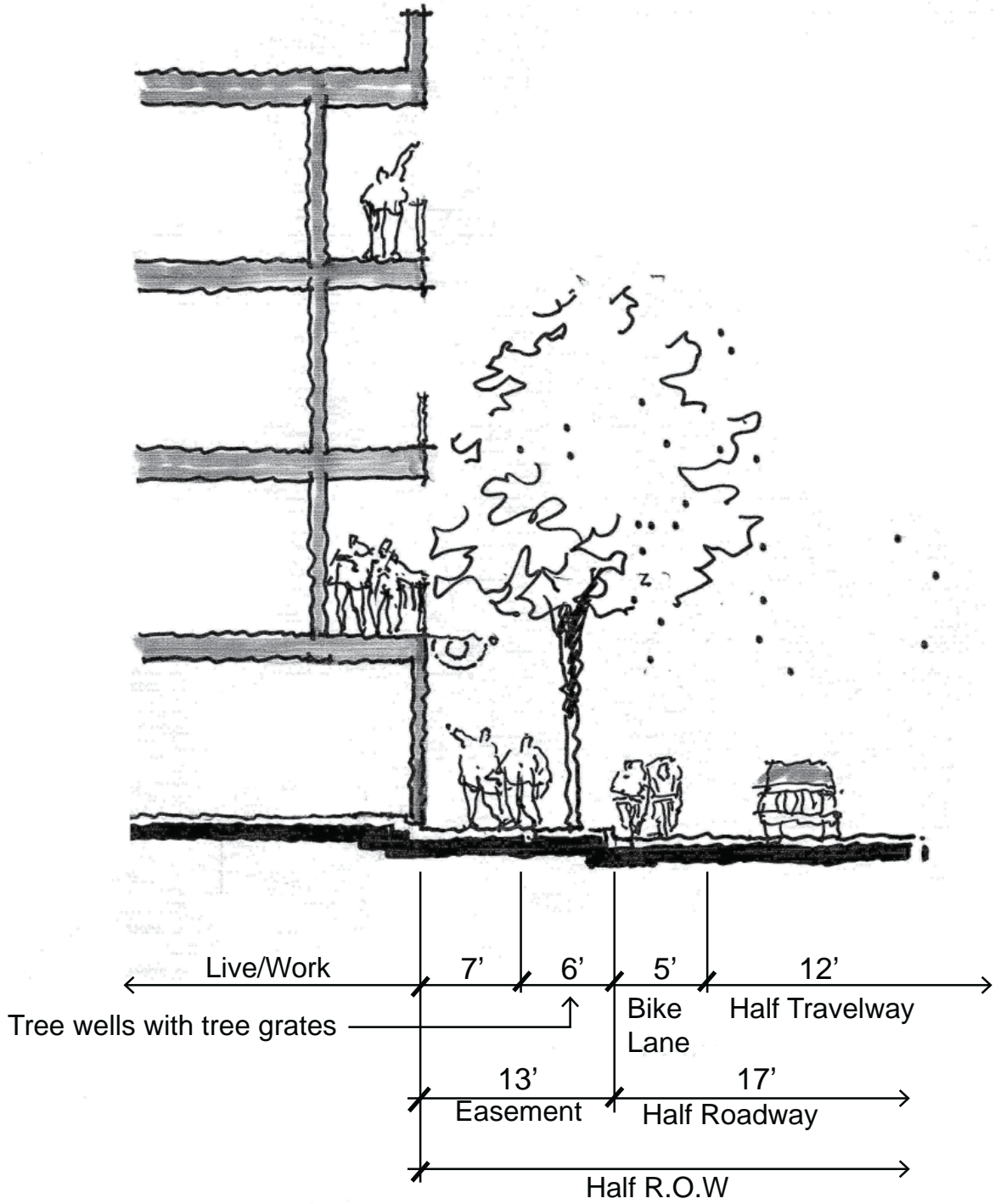
1. Easement area shall include public access easement, public utility easement and public landscape easement.

Figure 10-20: Thematic B Street - Live/Work Section



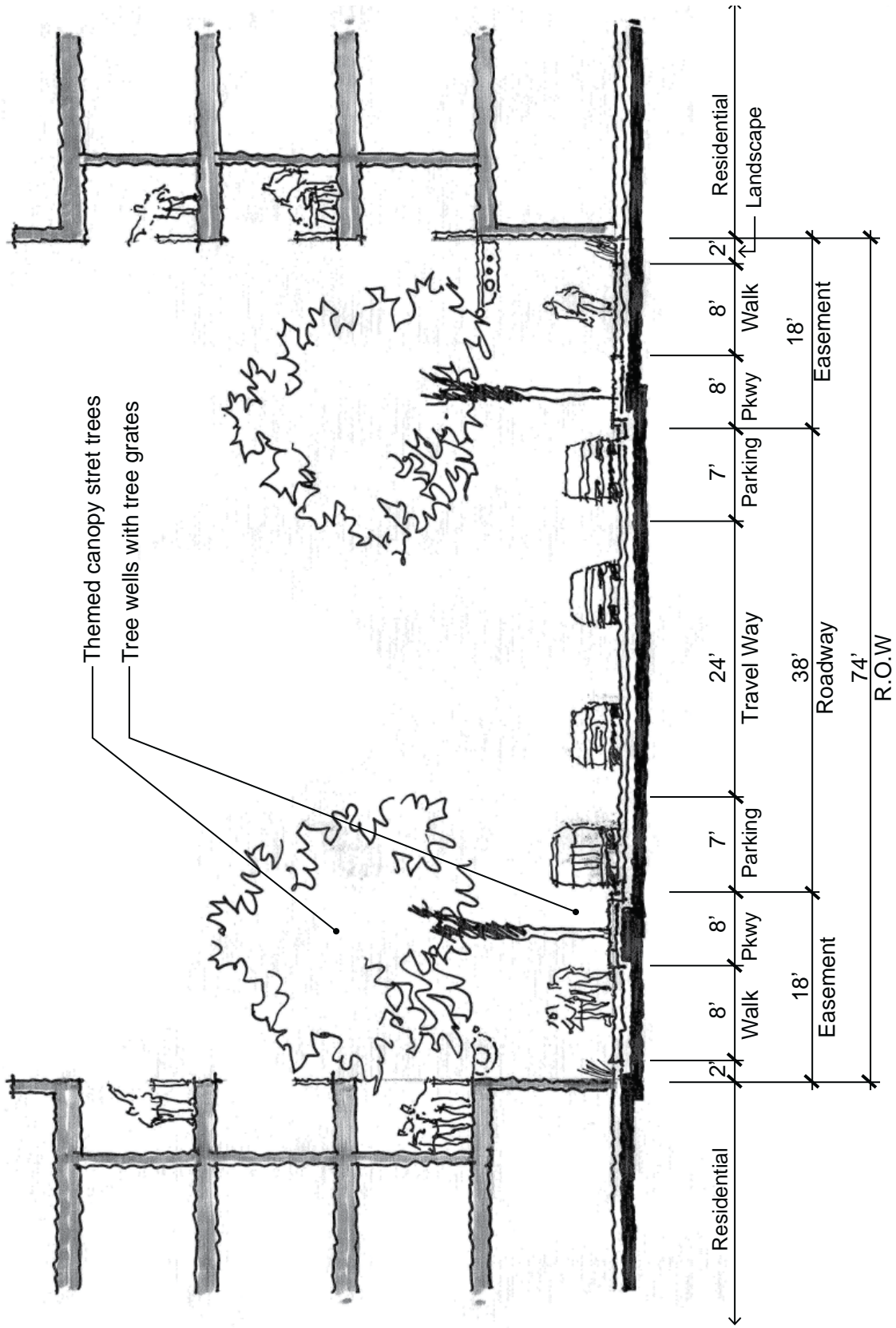
1. Easement area shall include public access easement, public utility easement and public landscape easement.

Figure 10-21: Thematic Street D - Residential Section



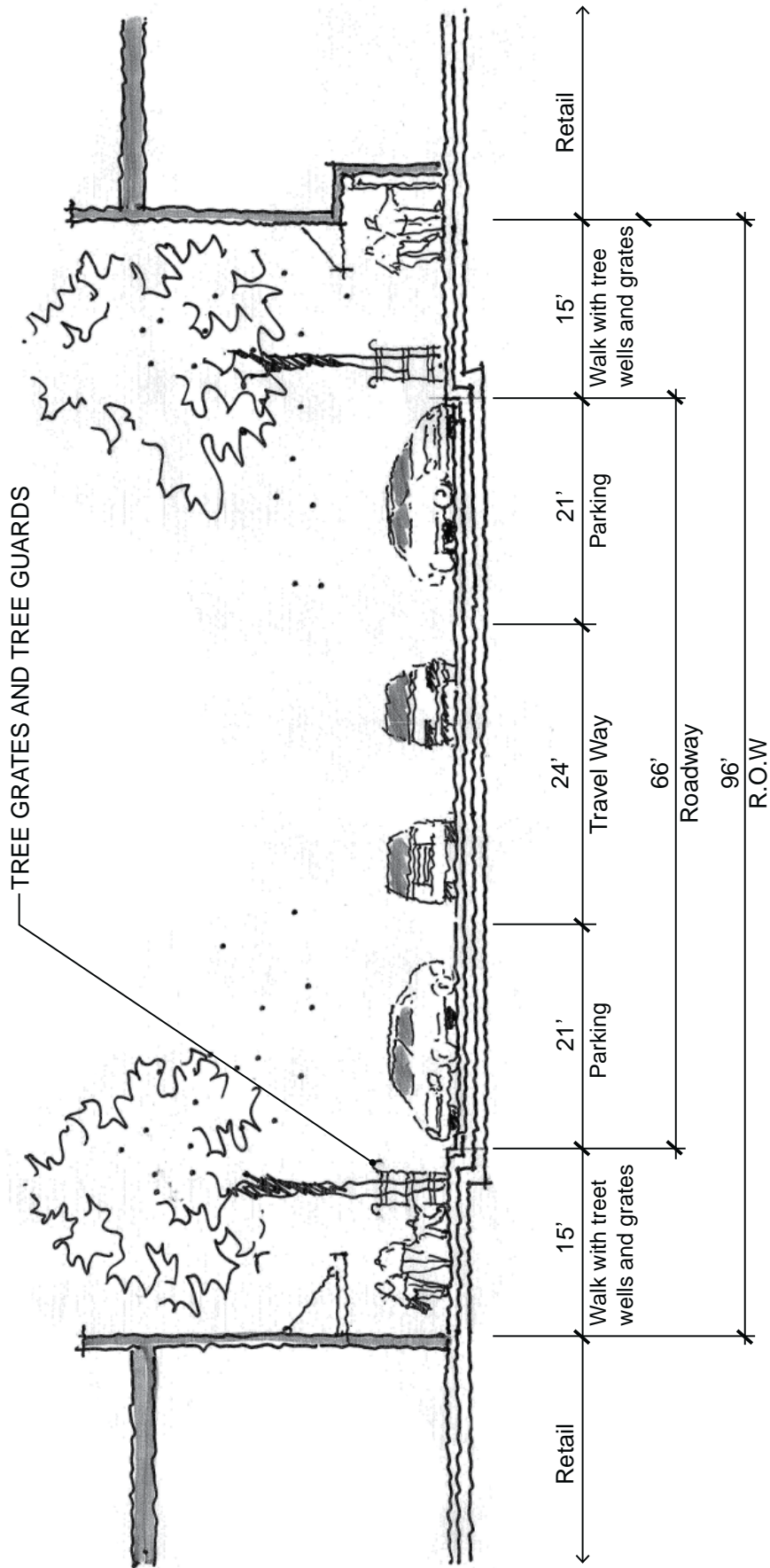
1. On-street parking shall be provided at a 60 degree angle.

Figure 10-22: Thematic Street D - Non-Residential Section



1. Easement area shall include public access easement, public utility easement and public landscape easement.

Figure 10-23: Thematic E Street - Train Station Connector Section



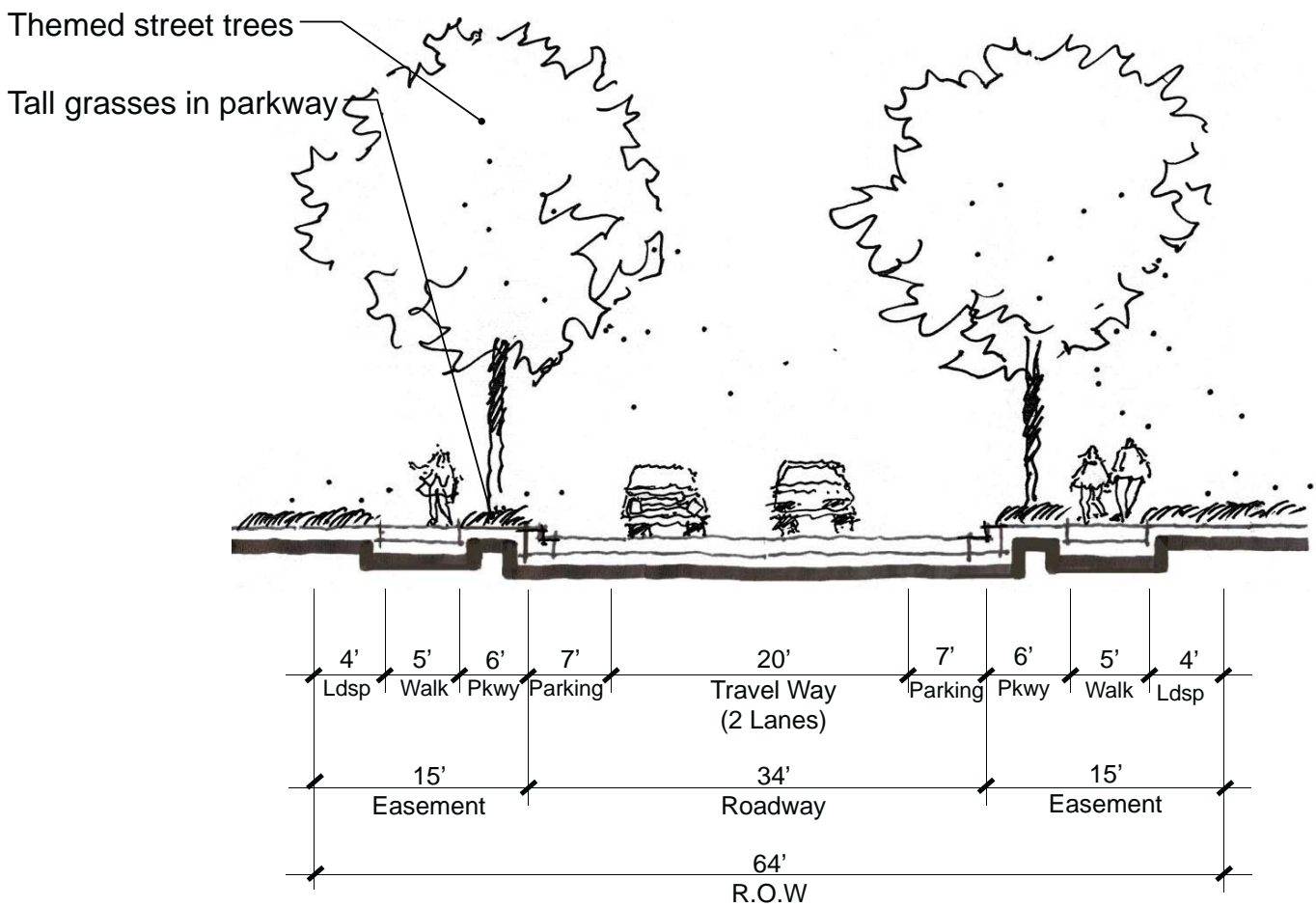
1. Easement area shall include public access easement, public utility easement and public landscape easement.

Figure 10-24: Thematic Street G - Main Street with Angled Parking



10.2.7 Icon Street

Icon streets are designed to serve as iconic roadways within the FTSSP community and will be planned on a case-by-case basis. They would generally be used to connect two neighborhoods with terminus on either end. On-street parking is generally permitted. An icon street is designed with an ultimate ROW of 64 feet and includes 7-foot parking (see Figure 10-25).

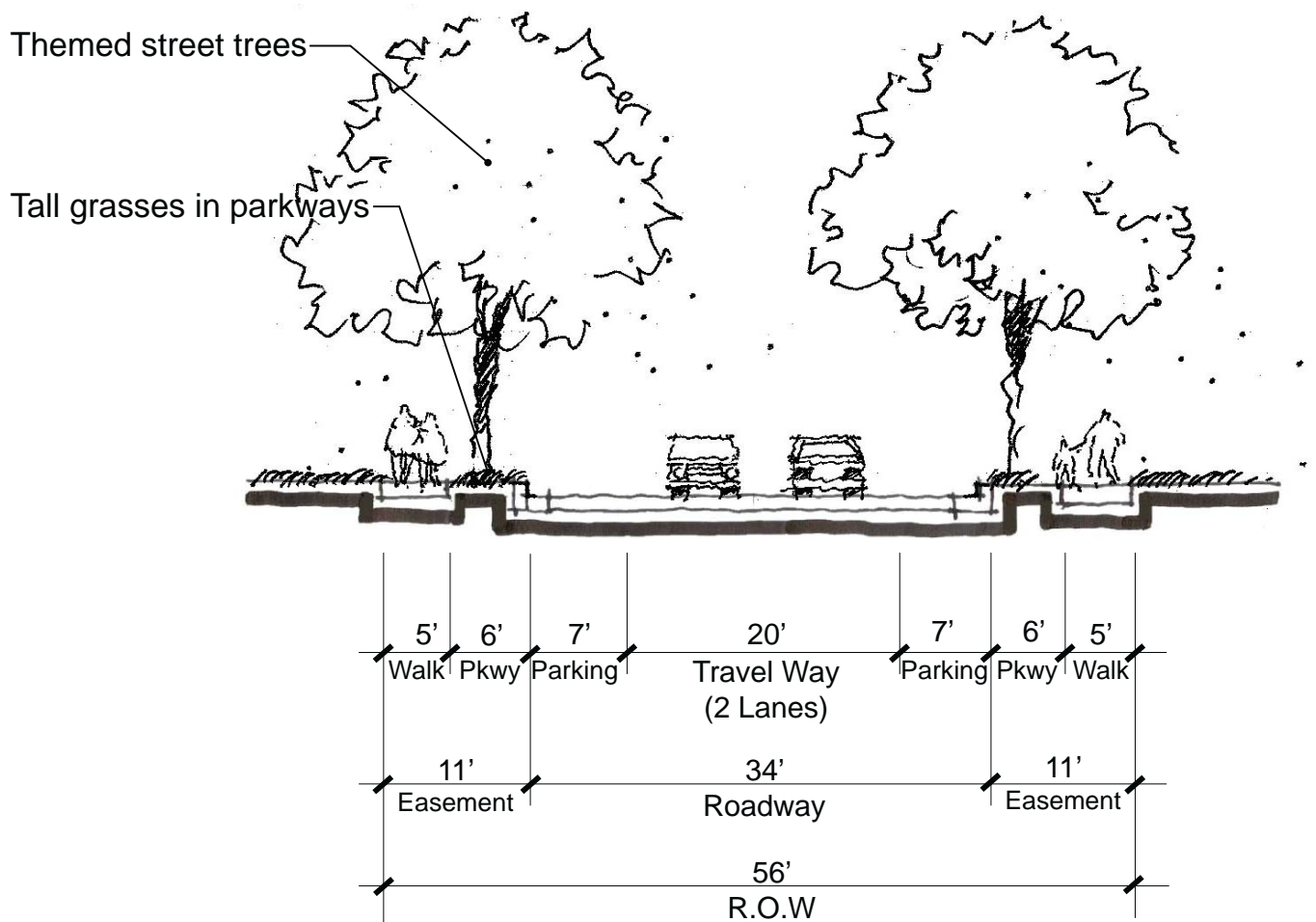


1. Easement area shall include public access easement, public utility easement and public landscape easement.

Figure 10-25: Icon Street

10.2.8 Residential Street

Residential streets are designed as low-speed, low-volume roadways to provide access to individual parcels in neighborhoods. On-street parking is generally permitted. These roadways are designed with an ultimate ROW of 56 feet and include 7-foot parking aisles, 5-foot sidewalks, and an additional 2-foot setback from property line to wall for required side-on conditions (see Figure 10-26).



1. Easement area shall include public access easement, public utility easement and public landscape easement.

Figure 10-26: Residential Street



10.2.9 Alley

Alleys are designed to provide secondary access to abutting properties. These privately owned and maintained roadways are designed with an ultimate ROW of 30 feet and a 20-foot travel lane and 5-foot setbacks (see Figure 10-27).

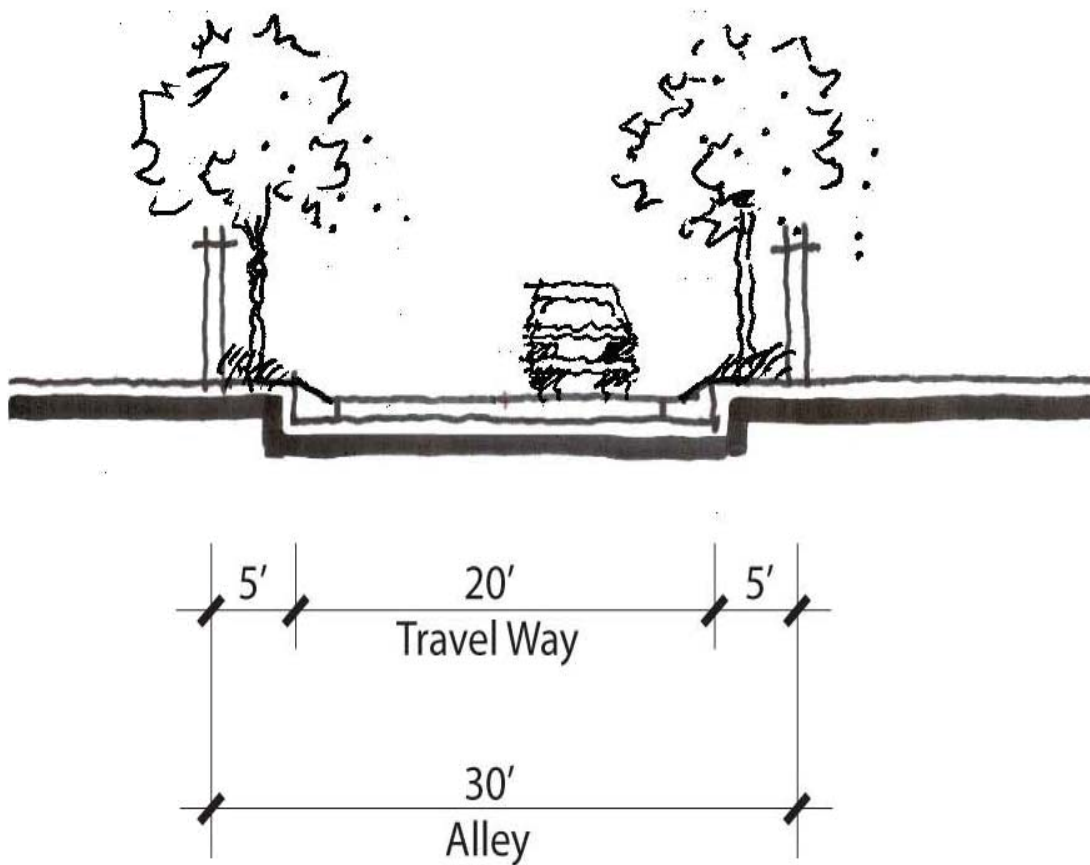


Figure 10-27: Alley

10.3 Pedestrian and Bicycle Network

Solano Transportation Authority (STA) incorporated a Countywide Pedestrian Plan (2004) and a Countywide Bicycle Plan (2004), which are currently in the process of being updated, as a part of their Comprehensive Transportation Plan.

Existing Countywide Pedestrian and Bicycle Plans

The purpose of the Countywide Pedestrian Plan is to support and encourage walking as a means of transportation in Solano County. The Countywide Pedestrian Plan encourages incorporation of pedestrian access and amenities in all new major projects.

According to the Countywide Pedestrian Plan, the major pedestrian routes in Fairfield are along West and North Texas Street and Travis Boulevard. These routes provide pedestrian access to high-density residential developments, commercial districts, public buildings, parks, schools, and the Fairfield Transportation Center. Other major north-south pedestrian routes include Pennsylvania Avenue, Union Avenue, and Dover Avenue. Major east-west pedestrian routes include Atlantic Avenue/Cement Hill Road and Tabor Avenue. Pedestrian facilities such as sidewalks, crosswalks, and pedestrian signals are intermittent throughout the Specific Plan area.

The purpose of the Countywide Bicycle Plan is to develop a bikeway network that provides connections throughout Solano County. The Countywide Bicycle Plan discusses Class I, Class II, and Class III bicycle paths. Class I bike paths are paved trails that are separated from roadways. Class II bike paths are lanes on roadways designated for use by bicycles through striping, pavement legends, and signs. Class III bike paths are designated roadways for bicycle use by signs only and may not include additional pavement width for cyclists.

Currently, the Fairfield Linear Park Trail is an off-street bike path that extends from Solano Community College to a point east of North Texas Street and north of East Tabor Avenue. City's General Plan calls for extension of the Linear Park Trail into the Specific Plan area.

Class II bicycle facilities are widespread in Fairfield. Marked bicycle lanes are present along Cement Hill Road between Dover Avenue and Clay Bank Road, Air Base Parkway, East Tabor Avenue, and Walters Road.

Proposed Pedestrian and Bicycle Plans

The FTSSP development will include an extensive network of pedestrian pathways and a cohesive bicycle path system that will

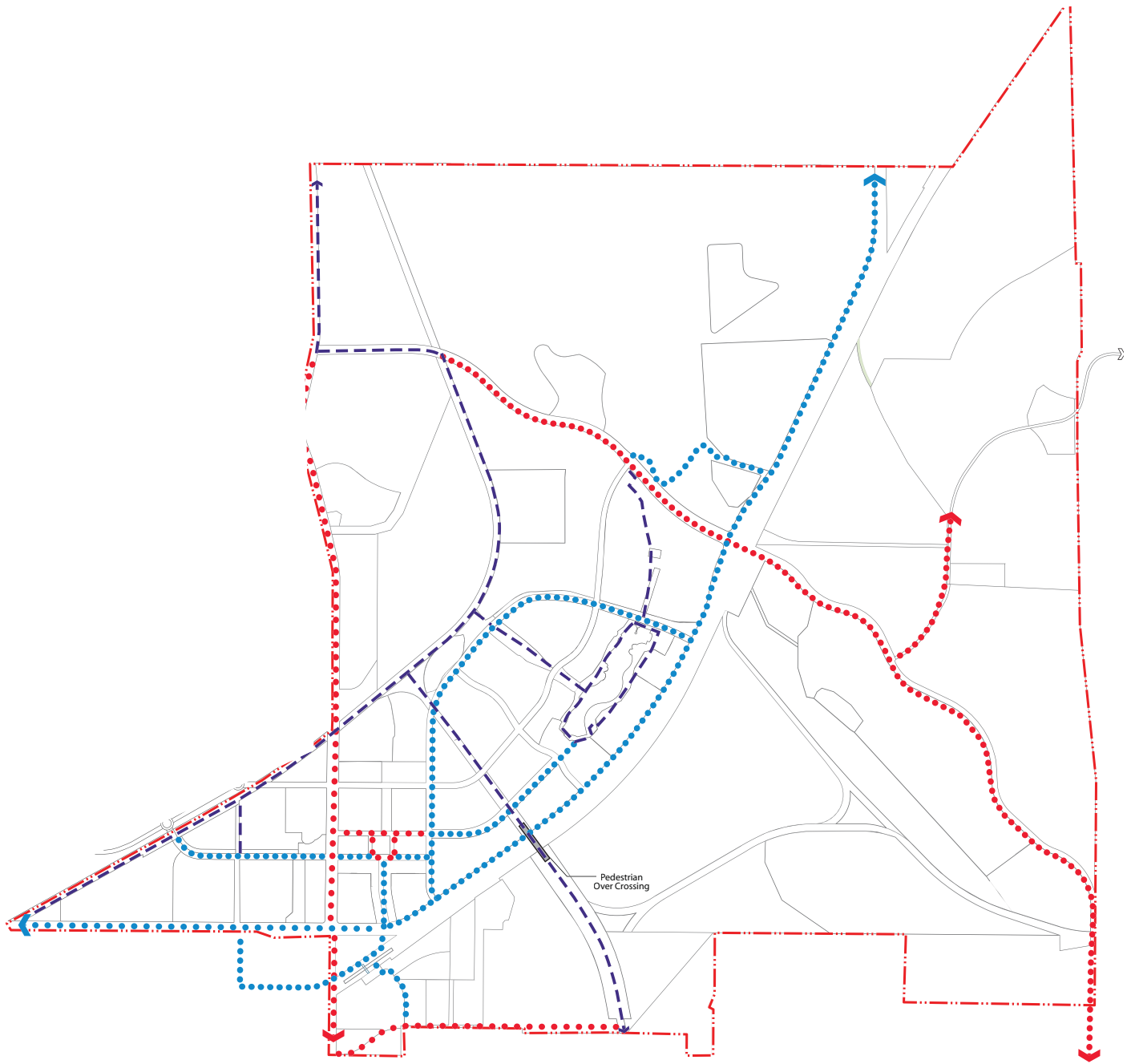


enable residents to access the project's public transportation facilities, the Lake Park, the numerous parks, and other public amenities, consistent with the safety and sensitive resources protection priorities. Figure 10-28 depicts the proposed pedestrian and bicycle plans for the FTSSP area.

On-street bike paths are planned along the following arterials: New Canon Road and Peabody Road. They will also extend into the industrial areas of the Specific Plan area, as well as from Center Elementary School to Vanden High School. Off-street bike paths are planned along Vanden Road as well as on many of the connector roads throughout the FTSSP area.

A multi-use trail will be constructed within the Linear Park Trail. It will run along the northern boundary of PA1 and PA2 and will continue along the roadway alignment until New Canon Road. A multi-use trail is proposed to extend from the northern portion of PA5, circle the Lake Park, and connect back to the Class I bike trail. The multi-use trail will also extend from the Class I bike trail through the PA2/PA4 boundary toward Center Elementary School.





Legend

- On-Street Bike Path
- Off-Street Bike Path
- - - Multi-Use Trail



Figure 10-28: Proposed Pedestrian and Bicycle Network



10.4 Public Transit

The public transit system includes both bus and rail passenger components. The bus and rail systems provide local and regional connections to Fairfield residents. The transit system operating within the City of Fairfield includes the following services:

- Local fixed-route bus service, regional express bus service and paratransit service operated by Fairfield and Suisun Transit
- Regional express bus service operated by Vallejo Transit
- Regional express bus service operated by Rio Vista Delta Breeze
- Regional passenger rail service operated by Amtrak

10.5 Park and Ride

Park-and-ride lots provide parking for commuters to leave their vehicles to meet carpools, vanpools, or access transit. There are 16 formal park-and-ride locations in Solano County. The following four park-and-ride lots are in proximity to the Specific Plan area:

- Fairfield Transportation Center
- Leisure Town Road and I-80
- Bella Vista Road and I-80
- Davis Street and I-80

10.6 Transportation Plan Policies

Policy 10-1

Circulation design shall be efficient and safe, accommodate vehicular traffic and addresses public safety, security, and public transportation needs.

Policy 10-2

Residential neighborhoods shall be served by local residential and other roadways, as appropriate.

Policy 10-3

The FTSSP development shall provide a safe, convenient and efficient pedestrian and bicycle network which includes on-street and off-street bike paths and multi-use trails.

Policy 10-4

The FTSSP pedestrian and bicycle network shall be compatible with the Solano County Comprehensive Transportation Plan, including the Solano Countywide Pedestrian Plan and the Solano Countywide Bicycle Plan.

Policy 10-5

New development within the Specific Plan area shall construct all road improvements identified in the Specific Plan, unless the City agrees to accept responsibility to construct certain roadway segments of city-wide or regional significance.

Policy 10-6

The reconstruction and widening of Vanden Road from Peabody Road to Leisure Town Road is the responsibility of Solano Transportation Authority, as part of Jepson Parkway. The City of Fairfield is responsible to pay 50 percent of the cost of those improvements as its local share for that portion of Vanden Road which is ultimately annexed to the City. New FTSSP development shall contribute the City's 50 percent share through a combination of land dedication for road right-of-way, mitigation of biological impacts, payment of development impact fees, and other financial contribution.

Policy 10-7

The reconstruction and widening of Cement Hill Road to 4 lanes from Peabody Road to Walters Road extension is the responsibility of Solano Transportation Authority. New FTSSP development shall pay the City's 50 percent share of the 4-lane road through a combination of land dedication for road right-of-way, mitigation of biological impacts, payment of development impact fees, and other financial contribution. New FTSSP development is responsible for the financing and construction of lanes 5 and 6.

Policy 10-8

The construction of Walters Road extension from Cement Hill Road to Air Base Parkway is the responsibility of Solano Transportation Authority. The City of Fairfield is responsible to pay 50% of the cost of those improvements. New FTSSP development shall pay its proportionate share of the City's 50 percent share of the road through a combination of land dedication for road right-of-way, mitigation of biological impacts, payment of development impact fees, and other financial contribution.

Policy 10-9

New development within FTSSP shall pay its proportionate share of the construction of the off-site widening of Peabody Road to six travel lanes from the southern limit of work of the Peabody Road railroad overcrossing improvements, south to Air Base Parkway. Its proportionate share shall be the cost of the fifth and sixth travel lanes.



Policy 10-10

The City may approve enhanced local streets through approval of a tentative subdivision map, dependent upon the average daily traffic (ADT) volume for the local street. Enhanced local streets would have an ROW of 58 to 60 feet, a curb-to-curb of 38 feet, and a 16-foot expanded parkway on one side, and would allow for parking on both sides.

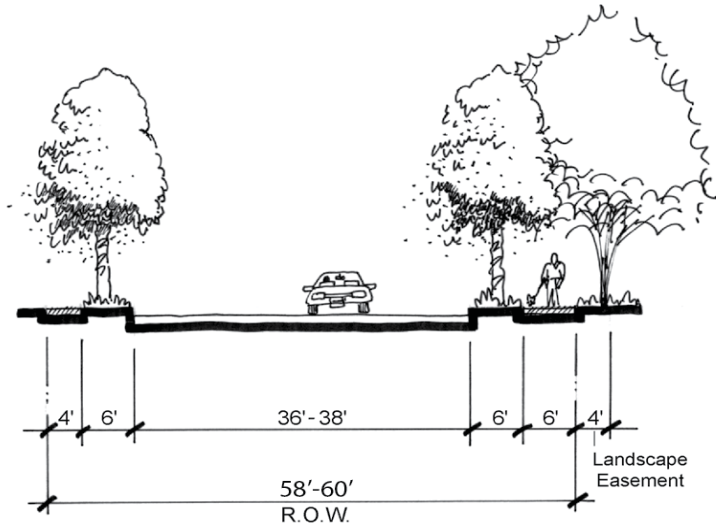


Figure 10-29: Enhanced Local Cross Section

Policy 10-11

The City shall allow a reduced radius cul-de-sac provided there is one on-street parking space per single-family lot. Such determination shall be done through tentative subdivision map approval. Reduced radius cul-de-sacs shall have a diameter of 76 feet and a radius of 38 feet to curb.

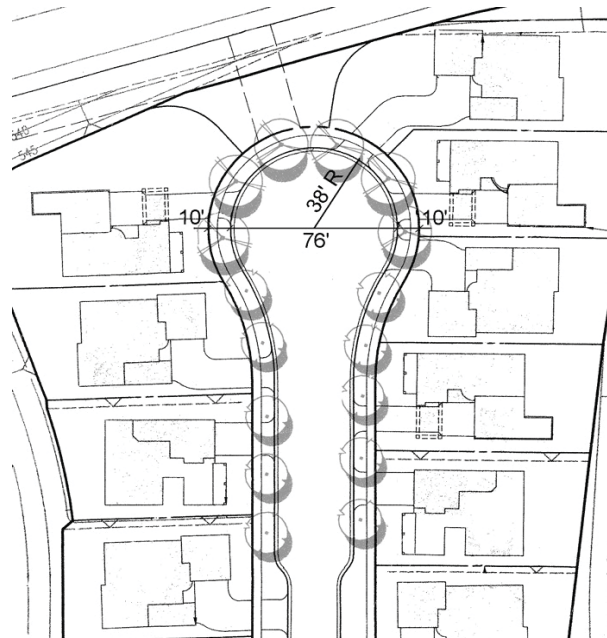


Figure 10-30: Typical Cul-de-sac with Curb Separated Sidewalk

Policy 10-12

The FTSSP development shall support raised landscape medians within residential streets, contingent upon approval by the City on a case-by-case basis, if public benefit is demonstrated.

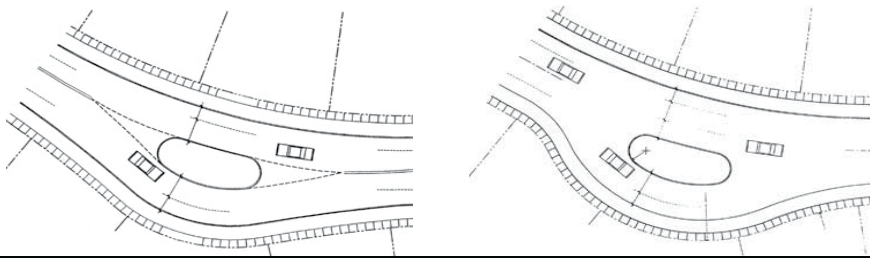


Figure 10-31: Double Loaded Street with Median Landscape

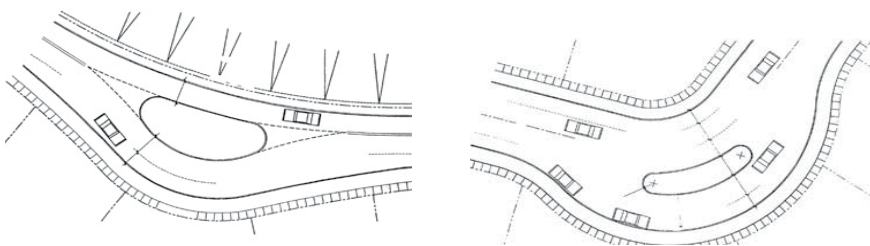
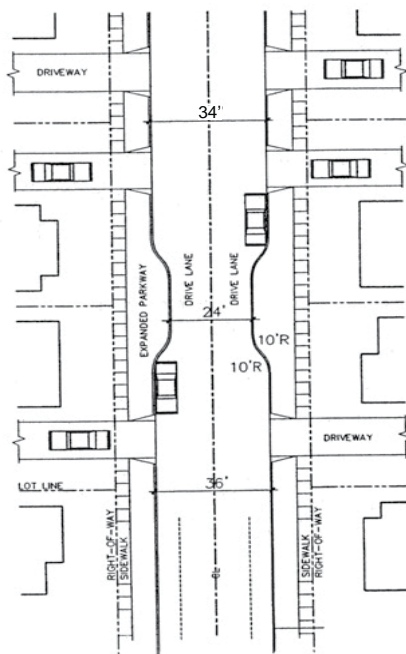


Figure 10-32: Single Loaded Street with Median Landscape

Policy 10-13

The City shall allow tapered streets (see Figure 10-33) and other traffic calming measures provided there is one off-street parking space per single-family lot. The location and design of such tapered streets shall be determined by the City through tentative subdivision map approval.



Notes:

- 1) Typical for larger lot neighborhoods
- 2) Meets all parking criteria with parking within 150' of each residence

Figure 10-33: Typical Tapered Street

Policy 10-14

The City shall allow alternative residential intersections such as tapered intersections provided there is one off-street parking space per single-family lot. Such determination shall be done through tentative subdivision map approval.

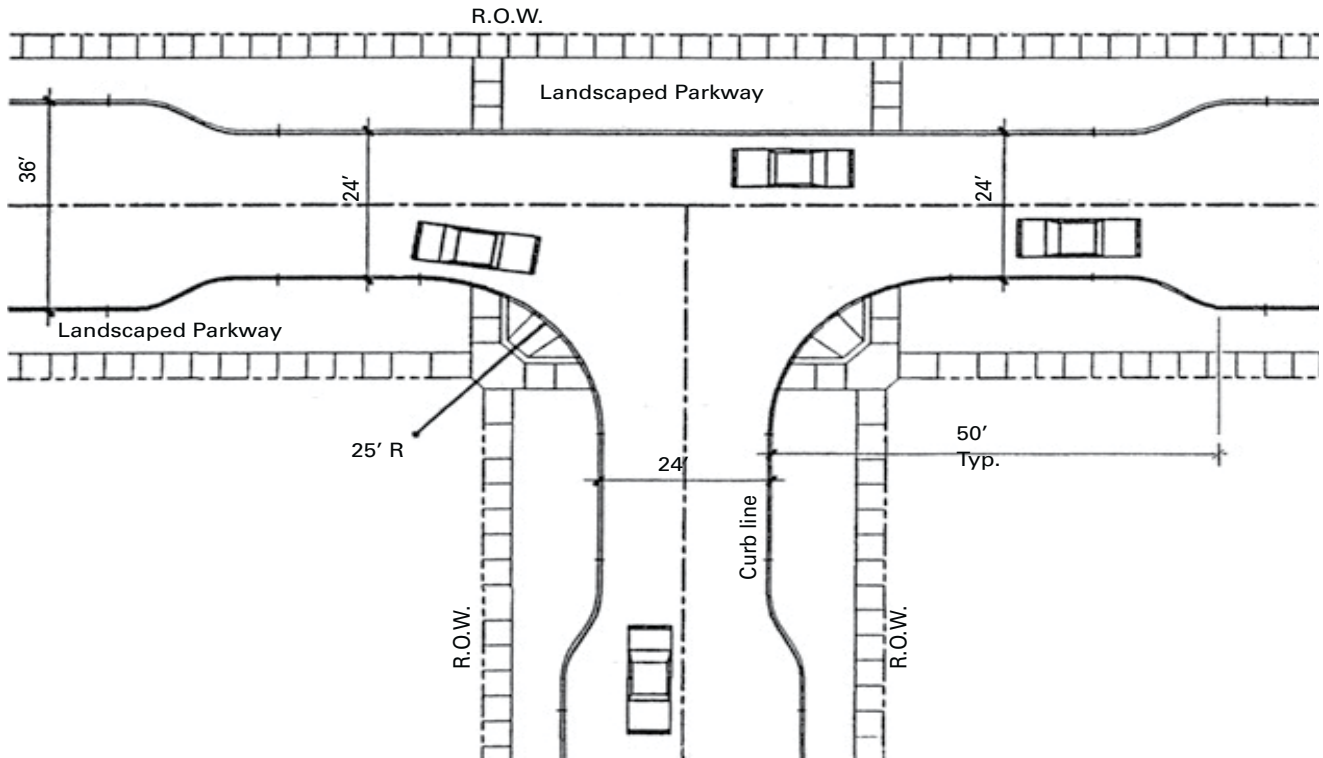


Figure 10-34: Typical Tapered Intersection, 3-Way

Policy 10-15

All alleys within Specific Plan area shall be privately owned and maintained by a Homeowners Association. The City shall allow rolled curbs for alleys, with either a 4-inch rolled curb and gutter or a 5-inch rolled curb and gutter. Such determination shall be done through tentative subdivision map approval.

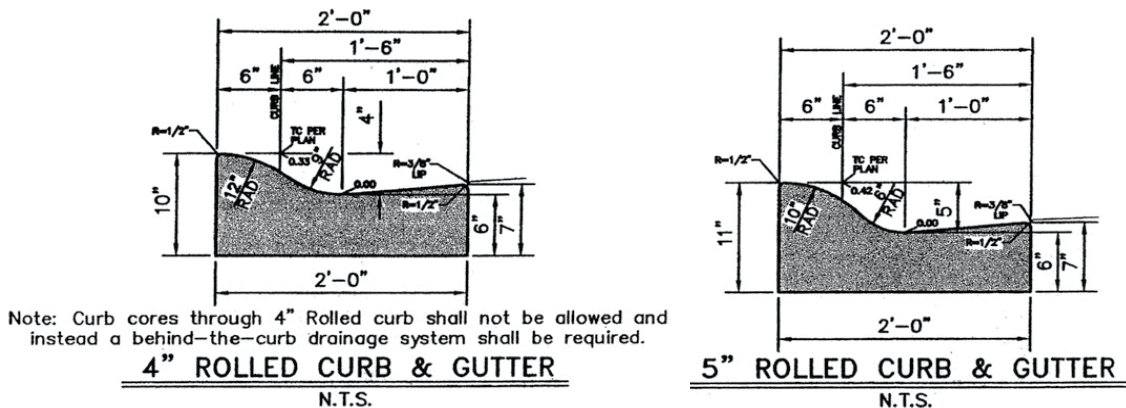


Figure 10-35: Rolled Curb and Gutter

Policy 10-16

The minimum, curb-to-curb width of an alley shall be 20 feet. Through conditions of approval of a tentative subdivision map, City may allow this width to be reduced to 16 feet if: :

- There are no private or public underground utilities within the alley;
- Alley is designed for one-way traffic; and
- Alley design is adequate for garbage service.”

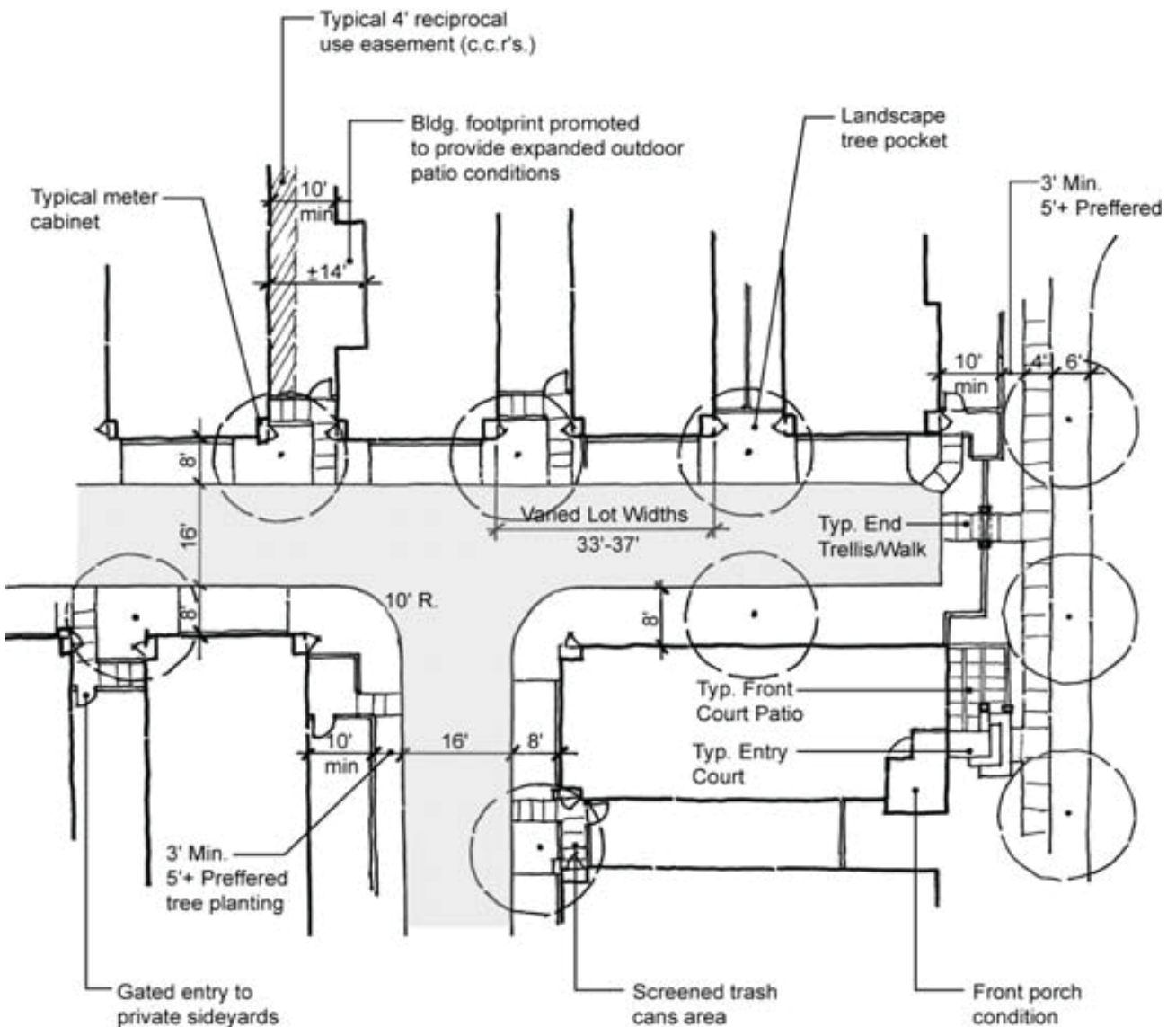


Figure 10-36: Alley Conditions



Policy 10-17

The location of signalized intersections shall be limited to those shown in Figure 10-1.

Policy 10-18

The number of nonsignalized intersections on the following roads shall be minimized:

- Peabody Road
- Vanden Road
- Cement Hill Road
- New Canon Road

The location and design of such nonsignalized intersections shall be determined through the City's approval of Master PUD Permits.

Policy 10-19

All streets shall be designed and constructed in accordance with the Street Sections shown in this chapter and the City's Standard Specifications for Public Improvements. Where the FTSSP Street Sections vary from the Standard Specifications, the FTSSP shall prevail.

Policy 10-20

Property owner responsibility to construct and finance the roadway network within the Specific Plan shall be in accordance with the provisions of Chapter 14.0, Implementation.

Policy 10-21

New development shall be responsible to construct and/or finance certain off-site road improvements, as identified in the EIR. The allocation of those costs and or construction obligations shall be in accordance with the provisions of Chapter 14.0, Implementation.

Policy 10-20

The construction of a new, off-site bridge connecting Joseph Gerevas Drive and Walters Road extension shall be in accordance with the provisions of Chapter 14.0, Implementation.



11.0 UTILITIES PLAN



11.0 UTILITIES PLAN

July 26, 2011



11.1 Overview

This chapter describes the existing utilities infrastructure, the required improvements, and the proposed new infrastructure to support the needs of the development of the FTSSP. Table 11-1 summarizes the utility providers to the FTSSP development.

Table 11-1: FTSSP Utility Providers

Utility	Provider
Water	City of Fairfield
Wastewater	Fairfield-Suisun Sewer District
Drainage and Flood Control	City of Fairfield
Solid Waste	City of Fairfield (Solano Garbage Co.)
Electric Service	Pacific Gas and Electric
Natural Gas	Pacific Gas and Electric
Communications	AT&T

Utility infrastructure will be constructed and dedicated, and easements provided consistent with this Specific Plan, the project development agreements, and all applicable standards and requirements.

11.2 Wet Utilities

11.2.1 Water

Water Supply and Demand

The City of Fairfield provides potable water to users within its City limits, except for Travis AFB, which receives water supplies from groundwater wells owned by the Base and from the City of Vallejo. The City or Solano Irrigation District (SID) provides nonpotable water for irrigation at several locations. The City receives water supplies from the Solano Project (through the Solano County Water Agency [SCWA]), the State Water



Project (SWP), the California Department of Water Resources (DWR) settlement, and various contracts with the SID. In 2005, the City had a total available water supply of 52,330 acre-feet per year (afy), including 49,730 afy in potable supplies and 2,600 afy in nonpotable supplies. In 2010 (a drier year than 2005), the City is estimated to have a total of 48,560 afy in available supplies, including 46,160 afy in potable supplies and 2,400 afy in nonpotable supplies.

In 2010, the demand for City water is expected to be 26,620 afy, not including demands from Travis AFB, which has independent water supplies and infrastructure. Of this, 16,990 afy was for residential use, 6,100 afy was for nonresidential use, 1,400 afy was for golf course irrigation, and 2,130 afy was unaccounted for. With 48,560 afy of available supply, the 2010 reserve (surplus) is estimated at 21,940 afy. Therefore, City water demand is at present substantially less than available water supplies.

For the City General Plan buildout without the FTSSP development, the ultimate total projected water demand would be 43,350 afy, compared to an available supply of 46,700 afy for a multiple dry year condition. This would leave a reserve of 3,350 afy without the need to obtain additional supply. Buildout with the FTSSP development would result in an additional water demand of 2,810 afy over existing demand projections. This would reduce the City's reserve to 540 afy but would still be within the City's available supplies. The City would have enough water supply to serve the proposed project in addition to existing and planned development under the multiple dry year condition.

Water Conveyance and Facilities

The Specific Plan area includes some existing water conveyance infrastructure, primarily located in the southwestern portion within Peabody and Cement Hill Roads. A 16-inch water line extends from the Specific Plan area's southern boundary just south of the Union Pacific Railroad (UPRR) tracks and continues north within Peabody Road to connect with existing residential development just west of the Specific Plan area. A 16-inch water line is also located within Cement Hill Road, which enters the Specific Plan area at its westernmost point and extends east until it connects with the 16-inch water line in Peabody Road and continues east to the UPRR track as a 12-inch water line. Another 16-inch water line also extends north from Cement Hill Road and is located just outside the Specific Plan area to serve the existing residential uses just west of the Specific Plan area. See Figure 11-1 for a conceptual diagram of the existing and proposed water conveyance infrastructure within and near the Specific Plan area.

There are two water treatment facilities located within the City that filter and disinfect raw water for municipal uses:

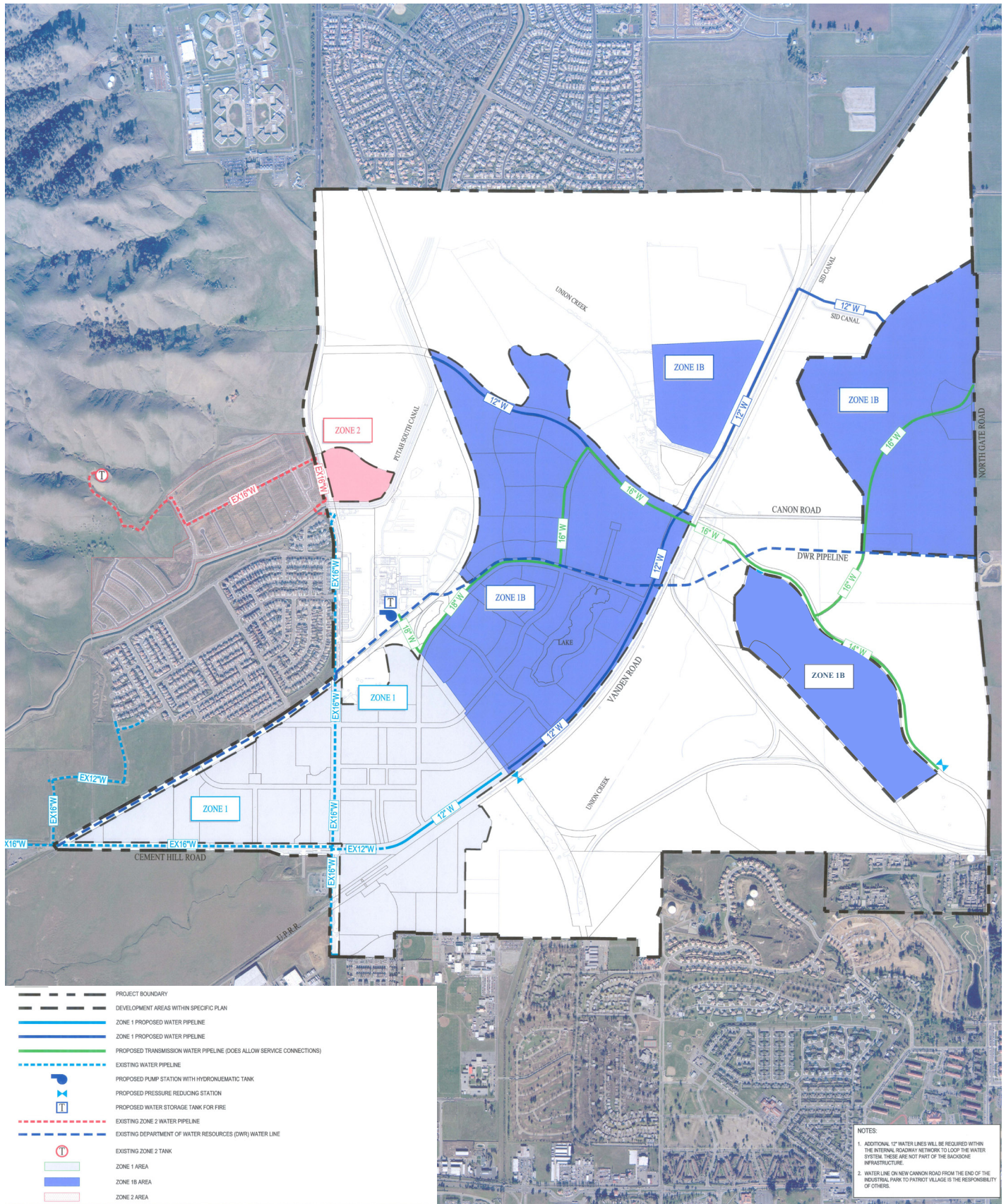


Figure 11-1: Conceptual Water System - Existing and Future



- Waterman Water Treatment Plant (WWTP) has a treatment capacity of 30 million gallons per day (mgd).
- North Bay Regional Water Treatment Plant (NBR WTP) is located within the Specific Plan area boundary and will continue operation during construction and implementation of the FTSSP development. It has a total water treatment capacity of 40 mgd, with approximately 26.7 mgd allocated for the City of Fairfield and 13.3 mgd for the City of Vacaville. The NBR WTP may eventually be expanded to 90 mgd (60 mgd for the City of Fairfield and 30 mgd for the City of Vacaville), though a maximum capacity of 60 mgd is more probable.

In addition, the City owns and operates a water system that includes 11 water storage reservoirs, which have a total storage capacity of more than 76 million gallons; it is in the process of developing more storage reservoirs to meet the City's treated water storage needs for the next 10 to 20 years. The City also owns and maintains 15 pump stations with a total capacity more than 30 mgd and 350 miles of water pipelines.

11.2.2 Wastewater

The Specific Plan area currently falls within the Fairfield-Suisun Sewer District (FSSD), which provides wastewater collection and treatment services for the City of Fairfield, as well as Suisun City, Travis AFB, the unincorporated area of Cordelia, parts of Suisun Valley, and some other unincorporated areas adjacent to these cities and community. The Specific Plan area is located within the Suisun basin.

Wastewater Facilities

The FSSD wastewater collection system is composed of 70 miles of sewer conduits ranging in size from 12 to 48 inches in diameter and a total of 12 pump stations. The FSSD service area is drained by four sewage basins that drain by gravity into the four major pump stations. FTSSP area is in the Suisun Basin and will be served by the Suisun Pump Station. The Suisun Pump Station's current maximum pumping capacity is 31.7 million gallons per day ("mgd"). Both the Suisun and Central basins do not currently have the capacity to accommodate planned growth east of Peabody Road, which includes most of the Specific Plan area. For all new annexations in the area, the City requires developers to prepare sewer master plans to indicate how growth will be accommodated.

FSSD operates the Fairfield-Suisun Subregional WWTP, located on 150 acres on Chadbourne Road southeast of I-80. The WWTP provides tertiary treatment and recently completed its second of two expansion projects in the summer of 2010. This most recent expansion increased the plant's capacity to 23.7 mgd average dry weather flow. The plant expansion does not have a declared wet weather capacity, but it is in excess of 52 mgd.

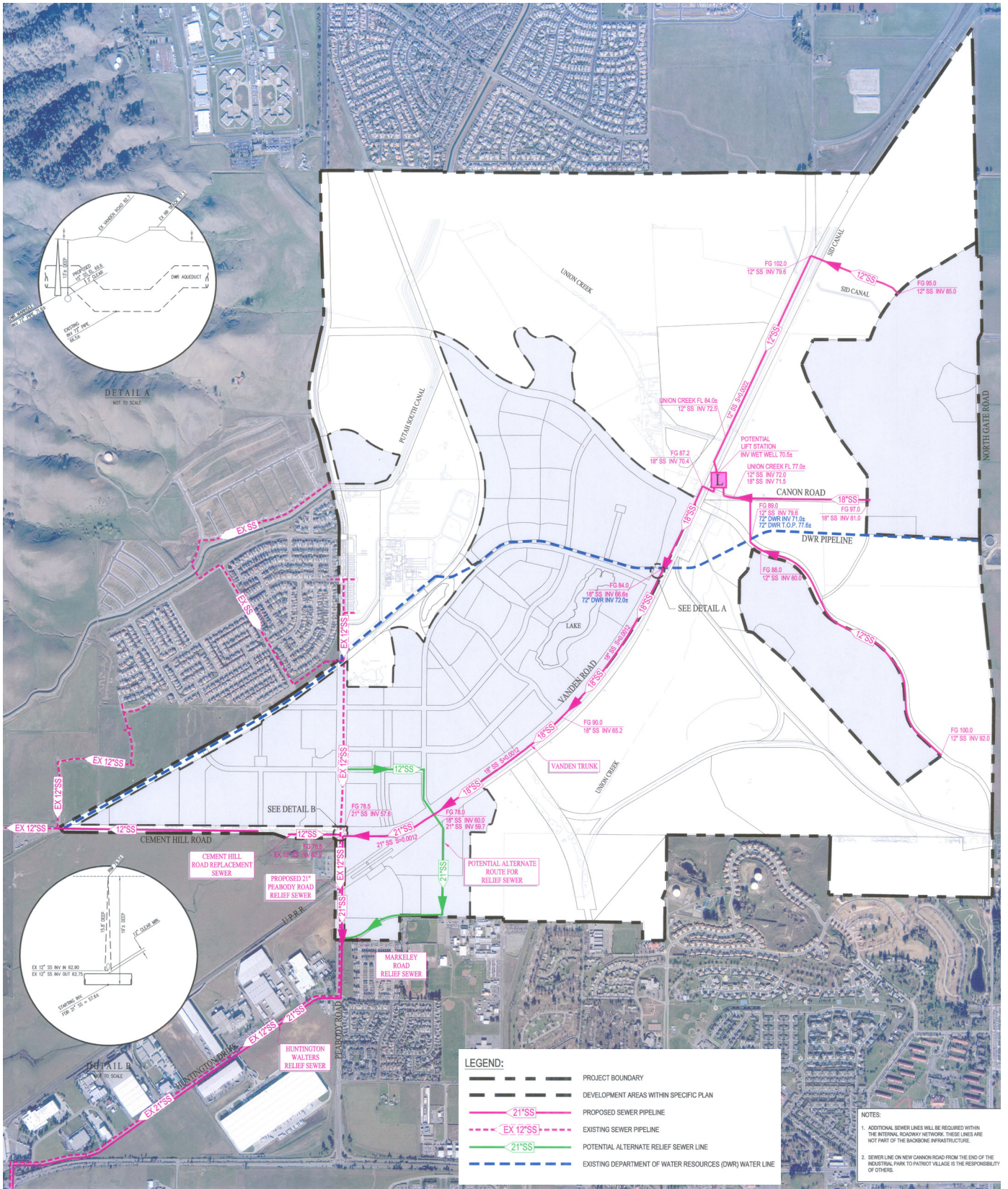


Figure 11-2: Conceptual Sewer System - Existing and Future



The Specific Plan area contains one existing 12-inch sewer pipeline located beneath Peabody Road, generally adjacent to the existing uses currently developed within the site (see Figure 11-2). There are also minor sewer pipelines and associated minor infrastructure connecting individual properties to this pipeline. Planned improvements associated with new sewer infrastructure would be placed within most of the project's major roadways and along the existing railroad easement and would connect to existing sewer connections at the project boundary, when adjacent to existing development. A lift station is proposed near the intersection of New Canon Road and Vanden Road, to the east of the railroad tracks, dependent on the final sewer design. A new trunk sewer within Peabody Road, Huntington Drive and Walters Road connecting to the existing collection system at East Tabor Avenue, is anticipated to serve the FTSSP area.

11.2.3 Drainage and Flood Control

Groundwater

There are four groundwater basins within Solano County as defined by the California Department of Water Resources ("DWR"): the Napa-Sonoma Lowlands subbasin within the Napa-Sonoma Valley basin, the Suisun-Fairfield Valley basin, and the Solano and Yolo Valley subbasins within the Sacramento Valley basin. The northeast portion of the FTSSP project site overlies the Solano basin, the northwest corner overlies an undefined aquifer, and the remainder of the project site overlies the Suisun-Fairfield basin. Groundwater is not used for domestic or irrigation purposes in Fairfield and is not considered a viable source for domestic water due to tidal inflows that impact water quality. Groundwater is not used within the project site.

Surface Water Resources

The Specific Plan area is within the Suisun Hydrologic Unit within the San Francisco Bay Hydrologic Region, which falls within the jurisdiction of the San Francisco Bay Regional Water Quality Control Board (RWQCB), and the Valley Putah-Cache Hydrologic Unit, which falls under the jurisdiction of the Central Valley RWQCB. The Suisun Hydrologic Unit drains approximately 157 square miles. Suisun Bay and Suisun Marsh are the receiving waters. The Valley Putah-Cache Hydrologic Unit drains approximately 305 square miles, and the Sacramento Delta is the receiving waters. The Specific Plan area has been further delineated into 11 subwatersheds for storm water planning purposes.

Union Creek flows south through the Specific Plan area to Suisun Marsh and Bay, the receiving waters for the creek. The northwest portion of the Specific Plan area is traversed by the Putah South Canal. SCWA is responsible for operating and maintaining the 33-mile-long canal under an agreement with the Bureau of Reclamation. Water for the Canal comes from the Putah Diversion Dam located on Putah

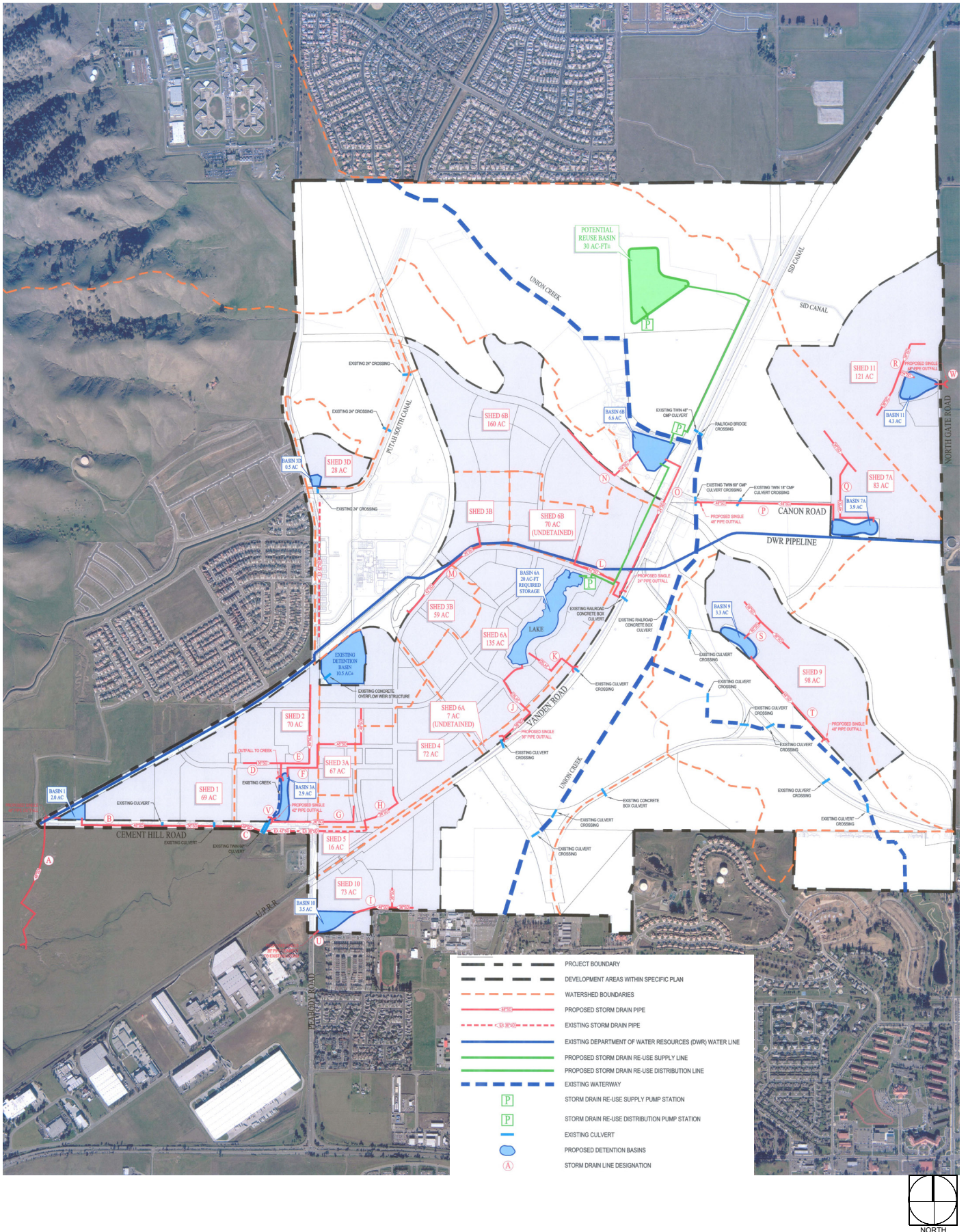


Figure 11-3: Conceptual Drainage System - Existing and Future shed and Re-use

Creek, approximately 6 miles below Monticello Dam and Lake Berryessa. Putah South Canal starts at Putah Diversion Dam and ends near Cordelia, approximately 30 miles south.

A segment of a SID irrigation canal traverses the northeast corner of the Specific Plan area. The portion of the canal that traverses the industrial area will be undergrounded. The alignment may vary from its current alignment to minimize the depth of the pipe. A SID canal also runs parallel to the northern boundary of the Specific Plan area and will remain as-is during development.

Drainage

The locations of the Specific Plan subwatersheds and detention basins at the conceptual level are shown in Figure 11-3. Storm water runoff for the project would be directed to McCoy Creek and Union Creek. A series of nine storm water detention basins would be constructed such that the post-development peak flows are 90 percent of the predevelopment peak flows for a 100-year storm event, pursuant to City of Fairfield standards.

Flood Zones

A portion of the project site is within the Federal Emergency Management Agency (FEMA) designated special flood hazard area, previously referred to as the 100-year flood zone (see Figure 11-4). Flooding in the vicinity of the Specific Plan area generally occurs along waterways, with infrequent localized flooding also occurring due to constrictions of storm drain systems and/or surface water ponding. In the Specific Plan area, culverts restrict the amount of flow that can move down Union Creek and elevated railroad and former railroad beds serve as dams, with floodwater apparently backing up behind them.

11.3 Dry Utilities

11.3.1 Solid Waste

The City of Fairfield coordinates solid waste management services but contracts with Solano Recycles/Solano Garbage Company (Solano Garbage Co.) for collection and hauling of solid waste. Solano Garbage Co. provides curbside recycling and garbage pickup for residences and commercial businesses within Fairfield under contract with the City.

The Potrero Hills Landfill services the project site and is located outside of Suisun City along SR-12. The Potrero Hills Landfill is located on a permitted disposal area of 190 acres. The landfill has a permitted maximum throughput of 4,330 tons per day and accepts nonhazardous municipal and special waste from several jurisdictions. The landfill accepts approximately 650 tons of waste each day from Fairfield, Suisun City, and the unincorporated areas of Solano County. In 2008,



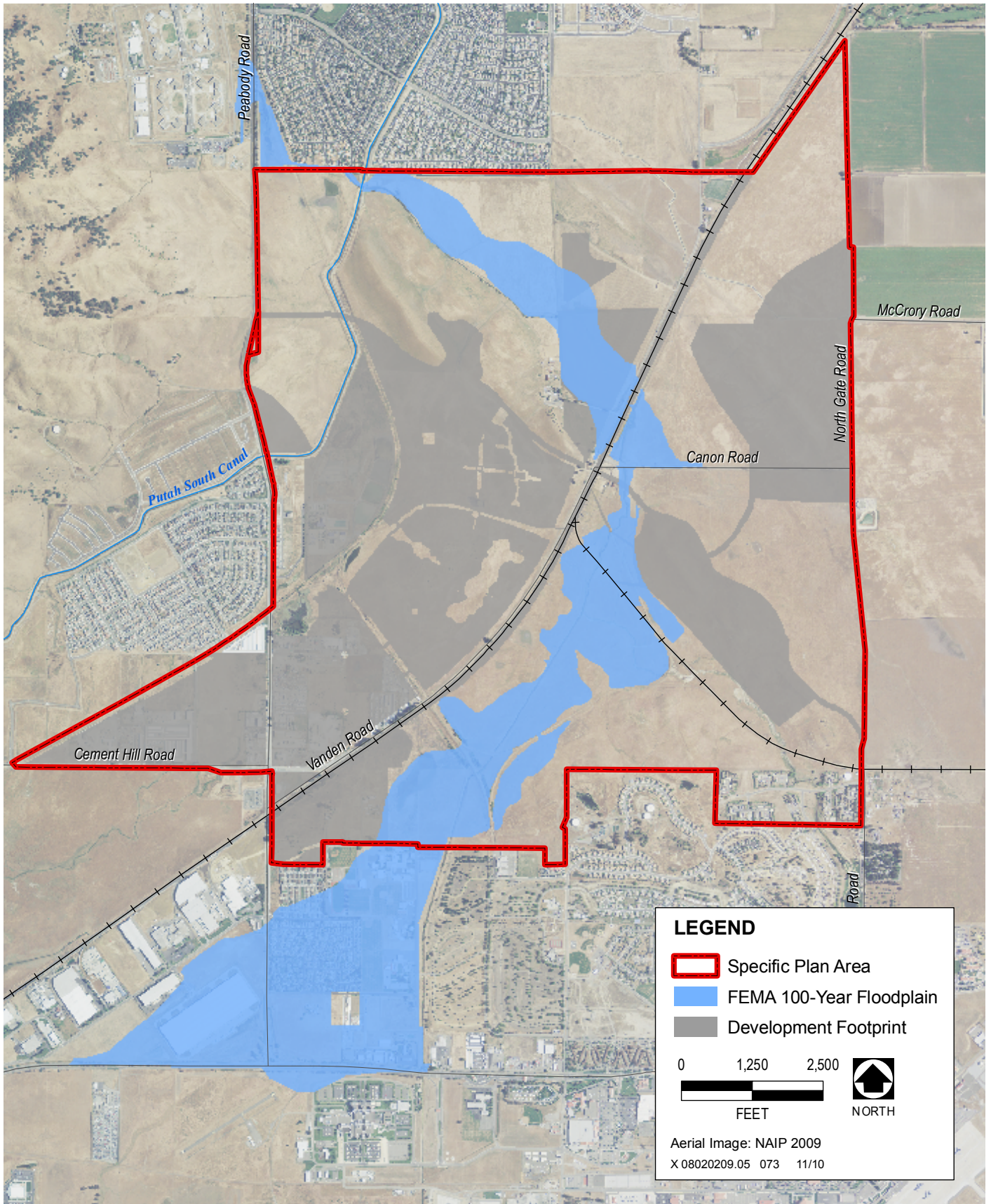


Figure 11-4: 100-Year Floodplain Map



the facility accepted 29,655 tons of waste from Fairfield alone. The existing Potrero Hills Landfill is expected to reach capacity in 2014.

Solano County is currently in the process of permitting an expansion of the Potrero Hills Landfill, which would be located directly adjacent to the existing facility and increase the allowable landfill heights. The expansion is expected to double the capacity of the existing facility to approximately 83 million cubic yards and would extend the life of the landfill between 35 and 43 years.

11.3.2 Electrical Service

Electricity for Solano County is provided by Pacific Gas and Electric (PG&E). PG&E delivers electricity to approximately 15 million people throughout the 70,000-square-mile service area in northern and central California. PG&E maintains 141,215 circuit miles of electrical distribution lines and 18,616 circuit miles of interconnected transmission lines. In 2007, PG&E delivered approximately 3,317 million kilowatts per hour (kWh) to customers in Solano County. Average electricity use in California was 7,032 kWh per capita in 2005, the lowest per capita use in the United States.

All public electrical energy for Solano County is generated outside the County and supplied via transmission lines. There is a 230-kV power line located parallel to the railroad line extending from Vacaville, northeast of the Specific Plan area, to a PG&E substation located just southwest of the intersection of Vanden and Peabody Roads. There is an overhead 60-kV line that services Travis AFB. From the Base, this power line extends to the north on North Gate Road, then west along Cannon Road, and then northeast into Vacaville adjacent to the 230-kV power line. There are also 21-kV overhead distribution lines along all of the main roadways, including Peabody Road, Cement Hill Road, Vanden Road, and Noonan Lane.

A PG&E substation is located adjacent to the Train Station, immediately west of Peabody Road and south of Cement Hill Road. This substation will continue operation throughout the buildout and operation of the Specific Plan. It is possible that PG&E will need to expand the existing substation or construct additional substation facility(ies) to meet demands of this Specific Plan. From substation(s), underground electrical service would be extended into the new FTSSP community through a series of underground trunk feeder lines. Generally, the feeder lines would follow the proposed roadway alignments out into the community to strategically positioned underground vaults, from which underground primary lines would extend throughout the community to transformer sites that would provide secondary power to the various residential, commercial, and industrial users.

As part of the project approval process, each project applicant will be required to coordinate with, and meet the requirements of PG&E

regarding the extension and locations of on-site infrastructure for each phase of the project, including underground lines.

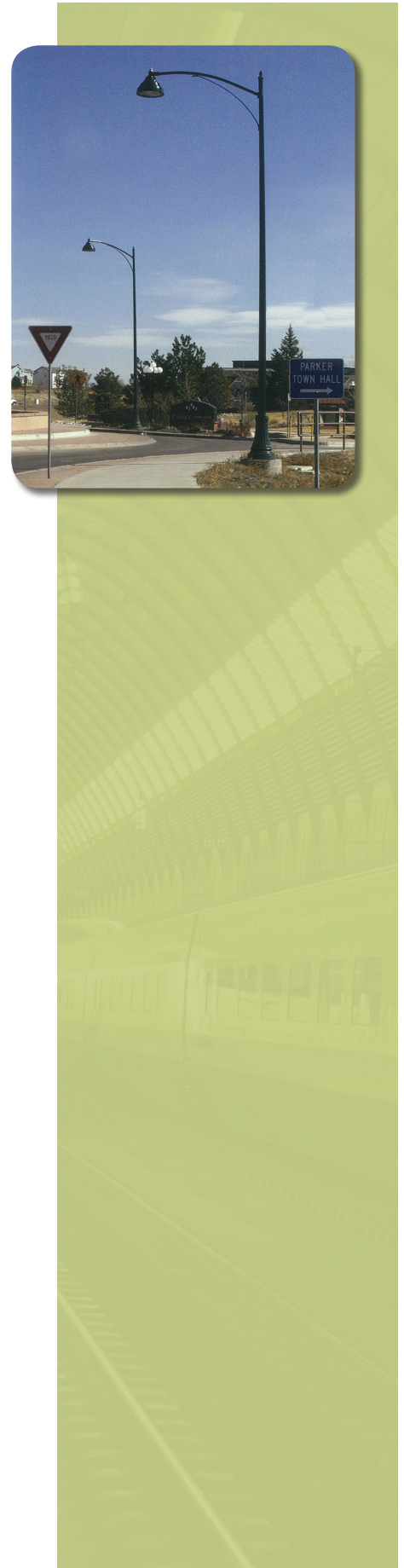
11.3.3 Natural Gas

Natural gas service for the project site is provided by PG&E. PG&E customers consumed a total of approximately 4,528 million therms of natural gas in 2007, of which approximately 231 million therms were delivered to PG&E customers in Solano County. PG&E's natural gas infrastructure system consists of 41,141 circuit miles of distribution pipelines and 6,438 circuit miles of transmission pipelines.

PG&E provides natural gas service to the City of Fairfield through both underground and aboveground transmission and distribution facilities. New distribution facilities are constructed within easements on private property. However, in some instances, new facilities are constructed within existing streets to increase capacity. Locations of distribution facilities generally depend upon how and when an area develops, as PG&E is highly involved with developers during the planning of new development projects. There are no natural gas transmission pipelines mapped within either the project site or the surrounding areas. However, there are distribution pipelines for serving individual properties located within the Specific Plan area. Kinder Morgan has a petroleum conveyance pipeline, located within or adjacent to the right-of-way of Vanden Road.

11.3.4 Communications

AT&T provides local telephone communication service for all of Solano County, including Fairfield and the project site. AT&T offers local phone service, long-distance phone service, and high-speed internet service. Major telephone transmission lines traverse the region. These lines usually follow rights-of-way that parallel major roadways and rail lines. Some of the utility line easements described above under "Electrical Service" are telephone transmission lines. In general, communications infrastructure would be constructed within existing rights-of-way and undergrounded. The on-site service lines would be sized to meet the demands of the Specific Plan area and public utility easements would be dedicated for all underground facilities. As part of the project approval process, each project applicant will be required to coordinate with and meet the requirements of AT&T and Comcast regarding the extension and locations of on-site infrastructure for each phase of the project, including underground lines.



11.4 Utilities Policies

Policy 11-1

Each development project shall provide utility services to meet the needs of its residents and businesses.

Policy 11-2

Each development project shall provide funding for the expansion of existing facilities and services to accommodate the increase in demand for such utilities which include, but are not limited to, the provision of water lines, storm drains, sanitary sewer lines, and other infrastructure needs.

Policy 11-3

Each development project shall provide planned improvements associated with new utility infrastructure.

Policy 11-4

All public improvements dedicated to City shall conform to the Fairfield Municipal Code, the City's Standard Specifications, and where applicable the standards of the FSSD and SID unless a deviation from those standards is approved by the City in writing. Where FTSSP standards vary from Standard Specifications, FTSSP shall prevail.

Policy 11-5

Landowners within the Specific Plan area shall dedicate all easements and fee title to rights-of-way and parcels necessary to accommodate the utilities described in the Specific Plan. All such dedications of easements and fee title by landowners shall be:

1. provided at no cost to the City;
2. free and clear of all liens and assessments or other encumbrances contrary to its intended public purpose; and
3. subsequent to clearance for development by US Fish and Wildlife Service, US Army Corps of Engineers, California Department of Fish and Game, California Department of Water Resources, and any other similar regional, state or federal agency.

Policy 11-6

To the extent feasible, the Lake Park, Great Park and other larger park areas shall be irrigated with nonpotable water.

Policy 11-7

Each development project shall include measures to promote water conservation as directed by the City.



Policy 11-8

Prior to or concurrently with the initial Master PUD Permit within the Specific Plan, the applicant shall submit a Master Water System Plan to the City for its review and approval. The Master Water System Plan shall be substantially in accordance with the Conceptual Water System shown in Figure 11-1 and shall include the entire Specific Plan area. The plan shall be acceptable to the City Engineer and shall include the following minimum elements:

1. water demand by land use and area,
2. pipe diameters, and
3. location and specifications for all reservoirs, booster pump(s), and hydropneumatic tank(s).

Policy 11-9

The City desires to use nonpotable water for lakes, water features, and landscape irrigation within public parks, to the extent feasible. As part of the Master PUD Permit applications for PA4, PA5, and PA6, the applicant(s) shall submit evidence that they have submitted a request to SID for annexation of industrial lands as well as all, public parks, and rights-of-way to be irrigated with nonpotable water to SID's service area. Plans and specifications for nonpotable water distribution systems, including treatment as required by the City and/or SID, shall be approved prior to approval of the final map.

Policy 11-10

Prior to or concurrently with the initial Master PUD Permit within the Specific Plan, the applicant shall submit a Master Sewer System Plan to the City for its review and approval. The Master Sewer System Plan shall be substantially in accordance with the Conceptual Sewer System shown in Figure 11-2 and shall include the entire Specific Plan area. The plan shall be acceptable to the City Engineer and shall include the following minimum elements:

1. wastewater generation factors by land use and area,
2. all flow information,
3. pipe diameters location and specifications for all lift stations, and
4. report on the status of the capacity of existing and proposed sewer lines by Fairfield Suisun Sewer District (FSSD) which will serve Specific Plan area. Report shall include: a) estimate of capacity of the existing 12-inch sewer line in Peabody Road which is available to serve new development in the Specific Plan area, b) status of construction of Walters Peabody Sewer Relief Line by FSSD, and c) determination as to the alignment and preliminary design of a sewer main from the terminus of the FSSD relief sewer to Vanden Road.





Policy 11-11

Until the FSSD completes construction of the Walters Peabody Sewer Relief Line, City shall limit the issuance of building permits to correspond to the capacity available in the existing 12-inch sewer line. Capacity available in the existing 12-inch line for new development shall be determined by FSSD. Such building permits shall be issued on a first-come-first-served basis.

Policy 11-12

Prior to approval of the initial final map within the Specific Plan area, a Master Drainage Plan shall be approved by the City. Each Master PUD Permit submittal shall be consistent with the approved Master Drainage Plan and shall include an Area Drainage Plan for storm water conveyance and detention within the appropriate subdrainage area(s) affecting the proposed development. The location of drainage detention facilities shall be substantially in accordance with the Conceptual Drainage System shown in Figure 11-3. The plan shall be acceptable to the City Engineer and shall include the following minimum elements:

1. location of each detention basin(s) within the Master PUD Permit boundary;
2. conceptual design, including sizing of each detention basin(s);
3. location, sizing, and method of conveyance of storm water to and from detention basin(s); and
4. method by which landowners within Master PUD Permit will pay for:
 - a. basin construction of all water quality and other features (i.e., Low Impact Development [LID]) required to meet requirements of the City's National Pollutant Discharge Elimination System storm water permit; and
 - b. on-going monitoring of water quality, as required by City's storm water permit.

Policy 11-13

The Lake Park shall be designed primarily as an aesthetic and recreational facility with a limited, secondary role in storm water detention/retention. The Master PUD Permit for PA4 shall include the following:

1. Lake Management Plan shall be prepared by a qualified expert and approved by City.

2. The Lake Park shall be designed with a maximum of 1 foot of free-board for storm water detention.

Policy 11-14

A Master Storm Drain Reuse Plan shall be submitted with the Master PUD Permit for PA4. The Master Storm Drain Reuse Plan shall be substantially in accordance with the Conceptual Storm Drain Reuse Facilities shown in Figure 11-3. The plan shall be acceptable to the City Engineer and shall include the following minimum elements:

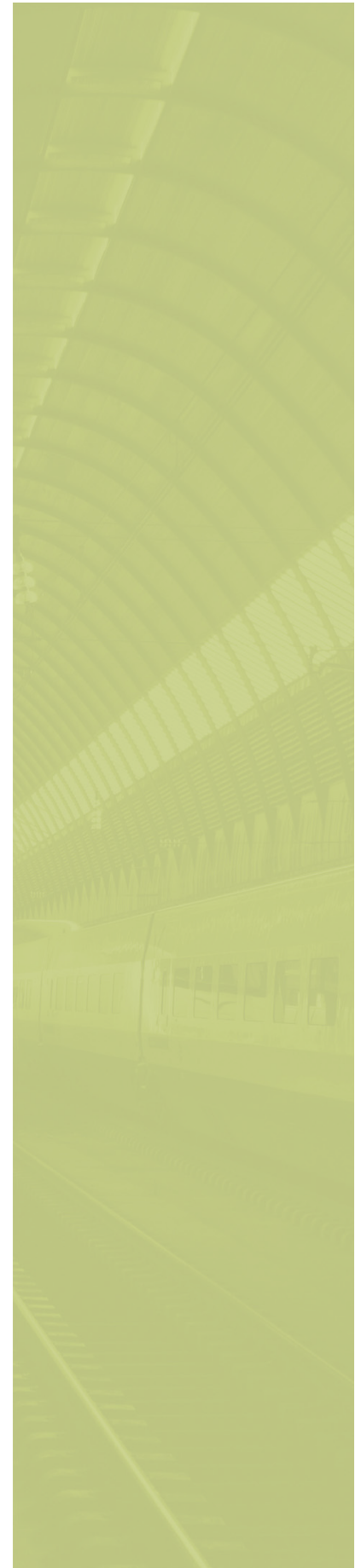
1. size and location of conveyance facilities,
2. location and specifications for pump stations, and
3. conceptual design for the reuse basin; the minimum size of the reuse basin shall be 30 acre-feet.

Policy 11-15

Sewer lines with a diameter less than 12 inches must comply with City's Standard Specifications. Sewer lines with a diameter 12 inches or larger must comply with the standards of the FSSD.

Policy 11-16

To minimize the amount of solid waste that must be disposed of by transformation and land disposal, the FTSSP shall promote (in order of priority) source reduction, recycling and composting, and environmentally safe transformation and land disposal, in accordance with California Integrated Waste Management Act (CIWMA) of 1989.



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12.0 LANDSCAPE PLAN



12.0 LANDSCAPE PLAN

July 26, 2011



12.1 Landscape Design Principles

The landscape plan is a major component of the overall Specific Plan area and will guide and maintain project design themes, while emphasizing community continuity. The landscape plan seeks to achieve an interconnected system of greenways, landscapes, parks and recreation venues, and open spaces that are viable and sustainable and will support native species, maintain natural ecological processes, sustain air and water resources, and contribute to the overall health and quality of life in the Specific Plan area.

The following goals have been established as benchmarks of performance. The landscape design should:

- Seek to utilize current best practices in sustainable site development and be adaptable to new technologies as they prove themselves reliable.
- Provide an interconnected hierarchy of green spaces at a variety of scales, including major parks, minor parks, wide corridors, narrow corridors, and private landscapes.
- Recognize the social and natural context of location by developing landscapes that embrace required drainage strategies and incorporate these into the public landscapes of Fairfield.
- Seek to enhance the Project's overall quality of life by providing recreational opportunities that promote a healthy lifestyle, are safe, and complement the well-being of the FTSSP residents.
- Improve the visual quality and amenity values of Fairfield.

12.2 Landscape Design Goals

The landscape design for the FTSSP is grounded in the following concepts, which are established as benchmarks of performance and guide the provision of landscapes throughout the project. The landscape design of the FTSSP should:



- Create a strong community identity (public and private) for the FTSSP and enhance the community-making principles described in the Specific Plan.
- Be designed in a manner that integrates Fairfield into the existing landscape context of its setting, while creating a unique environment that has a strong and distinctive sense of place.
- Incorporate infrastructure elements and enhance their functionality throughout the Project.
- Encourage pedestrian and bicycle access throughout the FTSSP and into the rest of the City of Fairfield.
- Provide open space, play areas, and places that are meaningful to the residents.
- Include a design vocabulary that embraces the history of agriculture and its surrounding undulating rolling grasslands.
- Be founded on the traditions of rural patterning for farming.
- Establish a landscape that maximizes the future opportunities for sustainable and productive landscapes.
- Be aligned with the city's maintenance resources.

12.2.1 Sustainable Landscape Goals

The landscape concept for the FTSSP is also based on the principles of sustainability and environmental stewardship. Consequently, the landscape design should:

- Use sustainable materials in the construction of landscapes including but not limited to, recycled materials, materials able to be recycled, and/or certified “green” products.
- Reuse soils from the site, if appropriate, as horticultural soils. If the on-site soils are deemed not suitable, natural and sustainable sourced ameliorants should be used.
- Follow the state model water-efficient landscape ordinance during the plant selection and irrigation design process.
- Evaluate site storm drainage conditions and, where consistent with current best practice, reduce runoff and promote reuse of storm water in the landscape.
- Select plant materials to provide a valuable landscape amenity that is both attractive and meets the sustainability goals of the Project. Plants should be chosen for their relative visual merits, consistency with the goals of the design guidelines, and their ability to thrive in intended locations. Plants should be selected based upon their reduced demand for water, fertilizers, pesticides, and maintenance as well as their potential to provide habitat value.
- Encourage paving materials that reduce runoff, reflect sunlight, and absorb less heat and are consistent with the adjacent architectural design guidelines.

- Design and construct site amenities including signage, seats and benches, litter receptacles, and other furniture elements to promote principles of sustainability.
- Promote recycling of waste and other materials throughout Fairfield by aligning with any existing recycling programs in the greater Fairfield area
- Reduce the long-term maintenance cost.

12.3 Landscape Themes

The concept for the landscape plan draws from the culturally rich landscapes of the region. Thematic elements may be drawn from the natural, agrarian, and urban landscapes in Fairfield and its surrounding environment. Each theme guides the landscape design and plant selection with the goal of integrating the FTSSP into the surrounding landscapes in a compatible manner while creating a coherent, unique, and sustainable place.

Three landscape themes constitute the landscape concept for the Project and are described in detail below: 1) Natural, 2) Agrarian, and 3) Cultural. The intent is that the Natural Landscape Theme may be used throughout the project to provide a consistent sense of connection with the broader landscape of the region. The Agrarian and Cultural Themes will be used to build a layered landscape design that is authentic, appropriate to the context of Fairfield, and innovative in order to best meet the demands of a growing and vibrant community.

12.3.1 Natural Landscape Theme

The Natural Landscape Theme is intended to provide guidance on the distribution and thematic development of native or naturalized vegetation across the FTSSP area. Generally, planting of native species in the park system, within the public right-of-ways and the landscape corridors will seek to provide design elements that are reflective of the surrounding area and are viable forms of vegetation. The following landscape features have been developed and should be used to develop design responses for broad-scale land uses:

Rolling Grasslands

The rolling grasslands that surround the Specific Plan area serve as a major inspiration for the landscape design of future development. Successful landscape design will incorporate drought-tolerant, tall grasses into the plant palette. To achieve this look, tall grasses should be planted in the parkways, landscape corridors, community gateways, medians, parks, neighborhood entries, parking lots, and the Linear Park Trail.





Wetlands

Wetlands add an important historical educational value to the community. The wetlands located adjacent to the Linear Park Trail should be sensitively enhanced to allow people to experience the natural flora and fauna that exist within the Fairfield community. Enhancements in this area should:

- Provide seating areas, shade structures, and informal trails to allow for interaction with the wetland area
- Add additional wetland-oriented planting where appropriate
- Integrate interpretive signage describing the function and benefits of wetlands

Open Space and Rolling Grasslands

The Fairfield community enjoys abundant surrounding open space, including a greenbelt/recreation open space, habitat conservation area, mitigation bank, vernal pool conservation area, and rolling grasslands. Consistent with the land use plan, large expanses of open space areas will remain free from development ensuring the land will remain an amenity for the citizens of Fairfield and surrounding communities. To physically protect these areas, agricultural fencing and other techniques should be utilized including the use of interpretive signage. Where appropriate, natural open space areas should be screened from any unsightly adjacent uses consistent with the guidelines in the Design Guidelines Chapter.

12.3.2 Agrarian Landscape Theme

The Agrarian Landscape Theme is intended to provide guidance on the distribution and thematic development of broader scale landscapes across the Project. The following broad-scale landscape types have been developed for the park system, within the public right-of-ways, the landscape corridors, and selected developed areas:

Rangeland

Much of the early agrarian history of the region is affiliated with grazing. Landscapes are broad scale and delineated by agricultural fences, meandering creeks, and section lines. Planting is generally limited to shelter planting and drifts of native oak species. To tie into the rangeland theme, the landscape in the FTSSP should:

- Consider a simple and strong ground plane pattern
- Plant tall grasses in the parkways, landscape corridors, medians, retention basins, parks, and industrial areas
- Encourage private homeowners to consider tall grasses as a no-mow lawn

Farming

Farming areas exist in this region. The landscapes of these areas are strongly articulated rows of crops and plantings extending from the valley floor to the gentle sloping foothills. Landscapes in FTSSP, particularly associated with office and light industrial uses, should consider farming as an inspirational theme. To achieve this look, landscapes should:

- Consider symmetrical and geometric ground plane patterns
- Support the ground plane patterns with equally strong tree plantings to achieve the farming articulation
- Frame prominent views of the site and important distant vistas

Orchards

Orchard areas have contributed significantly to the landscape of the Fairfield community. They are typically located near the outskirts of the town and have richer soils. Orchard landscapes are strongly articulated in rows with remnant orchards extending to the former outskirts of the towns. To achieve the orchard-inspired look, the landscapes in FTSSP should:

- Establish a strong grid pattern to recall existing orchards nearby
- Consider sparse ground planes
- Select appropriate trees to simulate the orchard concept and connect to the local environment
- Consider the orchard concept in the Great Park and smaller institutional areas

Landscapes in the FTSSP's medians and streetscapes should consider orchards with strong grids of trees, clearly defined edges, and sparse ground planes as design cues. In some locations along Vanden Road, orchards are residual landscapes that organically create clearings that could serve as the basis of programmatic spaces in parks such as the Great Park and smaller institutional areas.

12.3.3 Cultural Landscape Theme

The Cultural Landscape Theme is intended to provide guidance on the distribution and thematic development of urban landscapes in the FTSSP such as the right-of-ways, gateways, neighborhood identity markers, walls and fences, exterior lighting, and signage. The following landscape elements have been developed from settlement patterns in the region itself and may be used for both broad-scale land uses and individual parcel landscapes within the site:

Town Center

The goal of the town center neighborhood landscape area is to produce landscapes that are civic in design and reflect a more public provision of open space. The Town Center Landscape Theme should



be urban in intent and should be designed to accommodate higher levels of use by residents and visitors to the FTSSP. This neighborhood should encompass areas such as the mixed-use, retail, high-density residential, train station, and other landscapes central to the Specific Plan area.

- Consider the urban park as a basis for the landscape design
- Meet demands of an urban-based environment by accommodating a higher level of use
- Select street trees with canopies that can be pruned to showcase shop windows and signs
- Select street trees that provide shade

Village Core

The goal of the village core landscape area is to produce landscapes that are vibrant, reflect a diversity of uses, and are cores of neighborhood activity. The Village Core Neighborhood Landscape Overlay Theme, like the Town Core area, should have a suburban design intent. This area should encompass mixed-use, high-density residential neighborhoods and the Lake Park.

To achieve these goals, the Village Center landscape design should:

- Be founded on small-scale urban park design
- Connect to the centerpiece of the neighborhood, the Lake Park
- Create social and recreational spaces for neighborhood residents
- Draw from traditional town park design
- Be innovative to reduce water demand
- Meet the demands of a growing population while integrating sustainable landscape principles and goals

Parks

The goal of the park landscape in the FTSSP is to produce landscapes that provide a range of recreational opportunities of a scale complementary to the land use areas surrounding various types of parks. To achieve this goal, the landscape design should:

- Draw design inspiration from Northern California town park-making and the adjacent environment
- Create social and recreation spaces that best fit the land uses
- Integrate a primarily native and drought-tolerant plant palette
- Encourage innovative techniques to reduce water demand
- Meet contemporary needs for open space
- Integrate agrarian elements such as orchards, wind rows, furrows, and a simple groundplane treatment



Edge

The edge neighborhood is defined by its locations near and adjacent to the significant open space surrounding the project. The goal of the edge neighborhood landscape area is to provide a visual and functional transition between the densely populated areas of the FTSSP and the surrounding open space buffer zones. To achieve this goal, the edge landscape design should:

- Be more rural in design and encompass edge neighborhoods and lower density neighborhoods near the perimeter of the FTSSP.
- Incorporate traditional agricultural patterns with the intention of creating social spaces for neighborhood residents
- Use innovative techniques in order to reduce water demand
- Meet contemporary needs for open space while integrating sustainable landscape principles and goals

12.4 Edge Conditions

Traditionally, the edges of towns are diverse landscapes reflective of rural land uses and the urban areas that abut them. The FTSSP will have several edge conditions and each should be designed to provide an integrated landscape that is diverse yet consistent with the overall scale and land use patterns of the adjacent grasslands and agriculture land that surround the development. The Fairfield community includes six major edge conditions as described below. The landscapes of these edge conditions should be consistent with the landscape themes described in the Specific Plan and be of a scale that reflects adjacent land uses. See Figure 12-1 for location of edge conditions.

12.4.1 Greenbelt Recreation/Open Space Edge

The Greenbelt/Open Space Edge represents the northern boundary of the Specific Plan area and will form a valuable backdrop to large portions of the project. The Greenbelt/Open Space Edge offers a significant opportunity to integrate an open space recreation amenity in the community while providing a buffer for development. The design should be minimal, including a potential restoration of Union Creek and a pedestrian trail that will connect to the Great Park, and focus on preserving natural elements of the open space area.

12.4.2 Habitat Conservation Edge

The Habitat Conservation Edge, representing the northeastern boundary of the FTSSP, will provide visual relief within the project and will function as a gateway to the community.



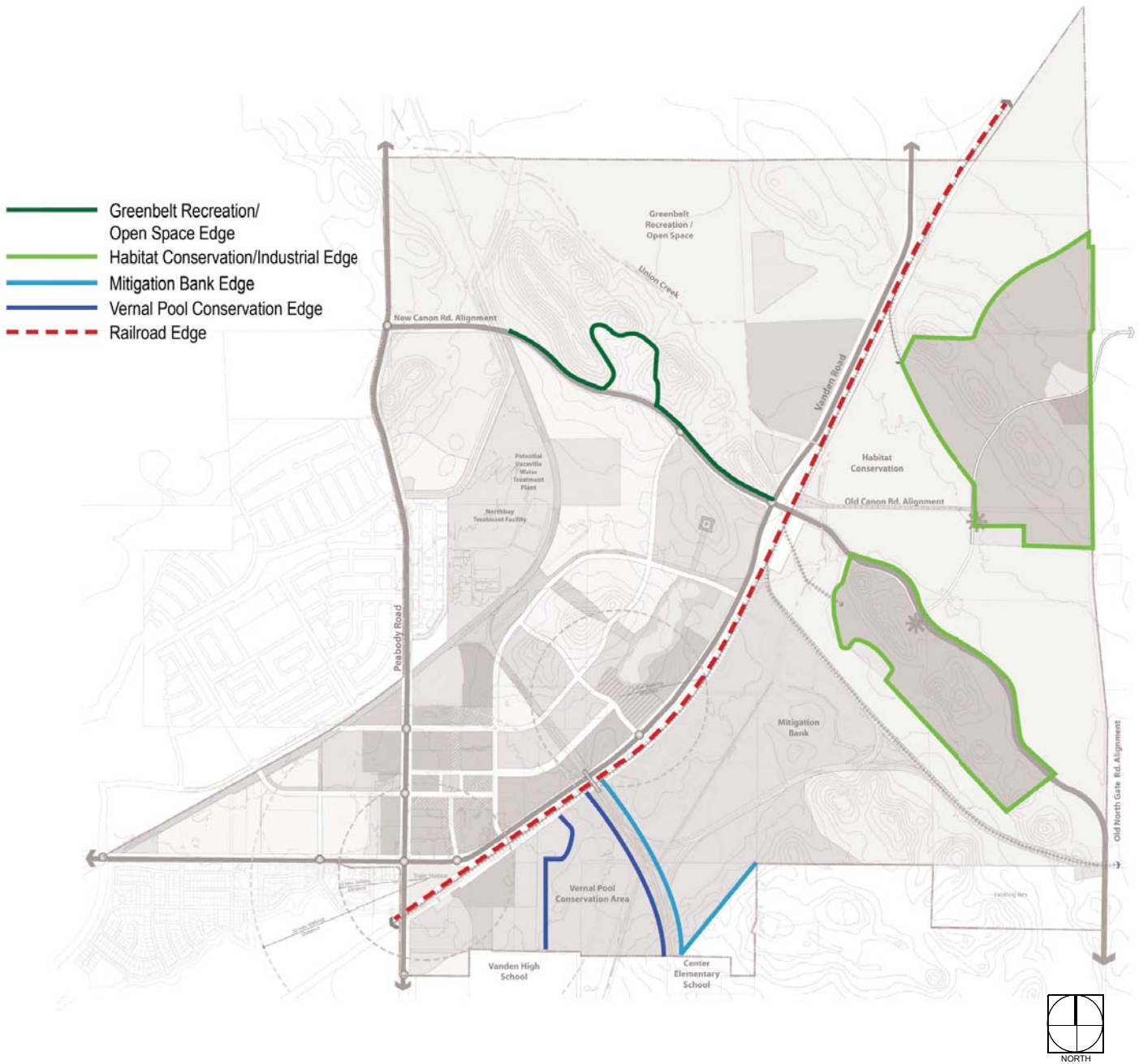


Figure 12-1: Edge Condition Diagram

Policy 12-1

Landscapes adjacent to the Habitat Conservation Edge shall be designed to protect and complement the area as follows:

- Install appropriate fences and walls to control access into the sensitive area along the railroad tracks, roads and industrial edge
- Incorporate interpretive signage to acknowledge the importance of the sensitive areas to the community along New Canon Road, Old North Gate Road and the Industrial Collector

12.4.3 Mitigation Area Edge

The Mitigation Area Edge represents a significant portion of the eastern boundary of the Specific Plan area and will provide vital habitat for the region's flora and fauna. This area will also provide a visual relief and a physical buffer between the new residential development and Travis AFB. Interpretive signs along the Linear Park Trail will provide information on the significance of this undeveloped area.

12.4.4 Vernal Pool Conservation Edge

The Vernal Pool Conservation Edge represents a significant portion of the southeastern boundary of the FTSSP and will conserve vital habitat for the region's flora and fauna. There will be no improvements in this area.

12.4.5 Railroad Edge

The Railroad Edge running roughly north to south forms a divide between a majority of the project's development and its greenbelt open space buffer. The Railroad Edge should integrate the project by linking landscapes from the town center to residential neighborhoods and to the Great Park along Vanden Road.

12.4.6 Industrial Edge

The FTSSP Industrial (Employment) Areas are located on the east side of Vanden Road and the Union Pacific Railroad and surrounded by the Habitat Conservation Area and North Gate Road. Landscape design along this edge condition should draw inspiration from the adjacent Habitat Conservation Area and the open farmland to the east.

Policy 12-2

The industrial edge landscape design shall:

- Incorporate agricultural patterns such as furrows, formal lines of trees, and a simple plant palette
- Promote tree planting to reflect the surrounding agrarian environment
- Protect surrounding land uses by storm water detention
- Screen unsightly areas and discourage trespassing with appropriate fencing



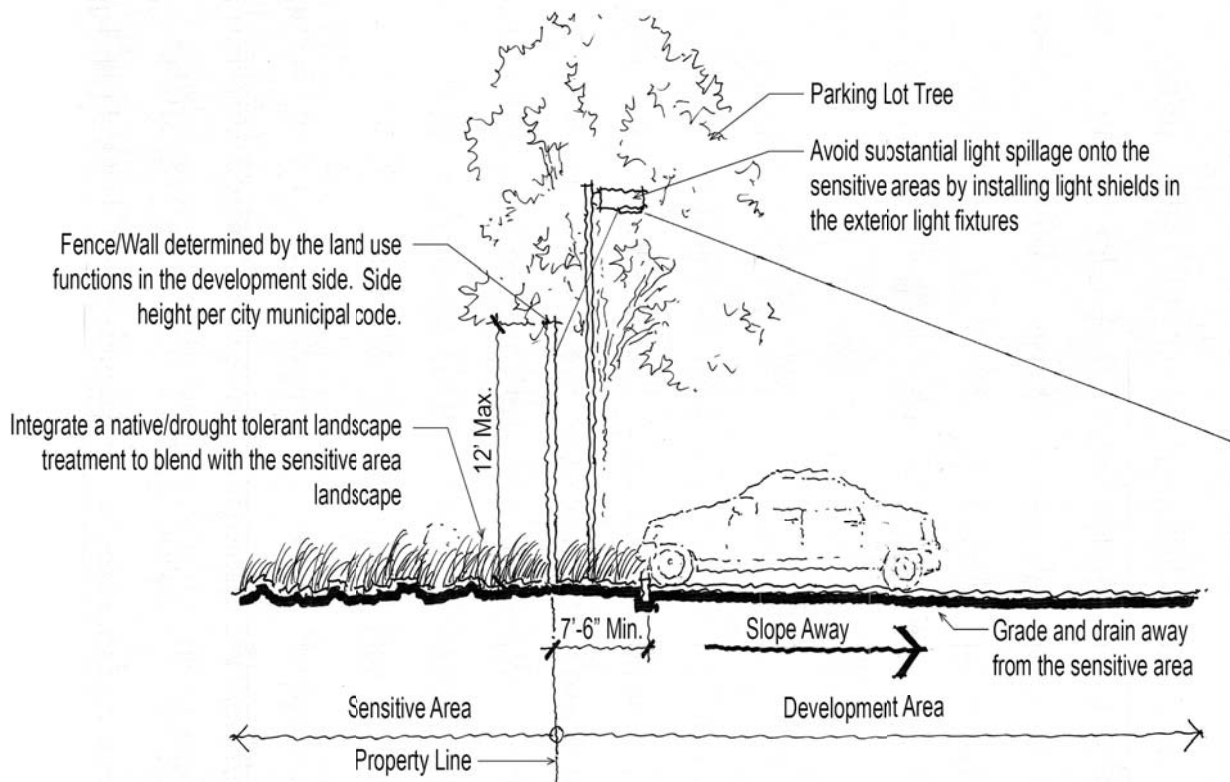


Figure 12-2: Wall Industrial Edge Condition

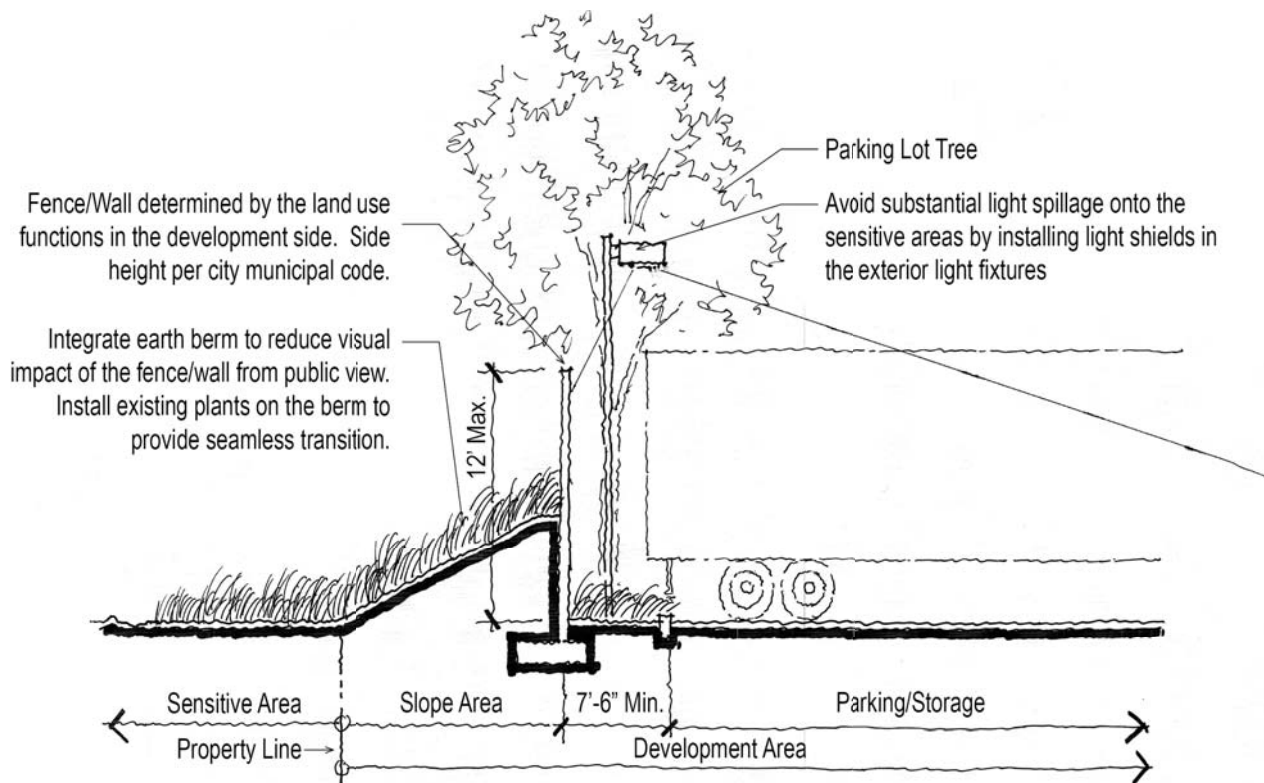


Figure 12-3: Bermed Industrial Edge Condition

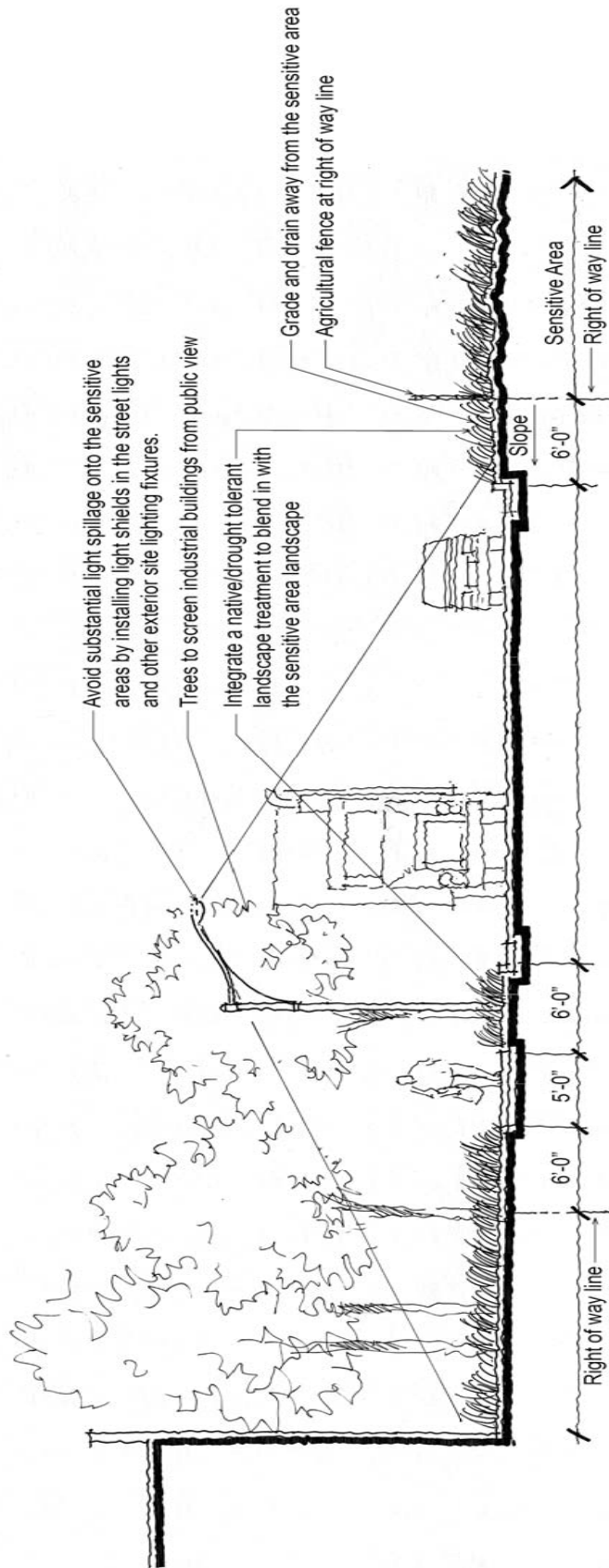


Figure 12-4: Single Loaded Street Industrial Edge Condition



- Install appropriate fences and walls to control access into the sensitive area along New Canon Road, North Gate Road and the Mitigation Area Edge
- Install earth berms adjacent to any solid walls to lessen the public visual impact of the walls along the Industrial Edge
- Integrate native/adaptive planting to enhance the development area and provide a seamless edge condition transition along the Habitat Conservation and Railroad Edges, New Canon Road and Old North Gate Road
- Incorporate interpretive signage on site to acknowledge the importance of the sensitive areas to the community

See Figures 12-2 through 12-4 for specific condition requirements.

12.5 Streetscapes

Streets provide a valuable resource for the Specific Plan community. They provide circulation and a venue for people to meet and interact with each other. Streets should be regarded as valuable open spaces with emphasis placed on provision of appropriate landscapes to promote social interaction, encourage activity, and reduce heat island impacts caused by large amounts of unshaded paved surfaces.

Street and road plantings will include a mixture of formal and informal plantings dependent upon their function, intensity of use, and scale within the Project. Larger and busier streets should be more formal in their design to reflect the denser community fabric anticipated adjacent to these streets.

Adjacent land uses will also impact street design, especially in areas like the Town Center and commercial areas, where careful placement of trees with open branching will provide valuable environmental amelioration and maintain views to shop windows and entrances. Similarly, residential areas may have more informal groupings of trees dependent upon driveway locations, park locations, and access to bike pathways.

The following policies have been established for the streetscape design of the FTSSP:

Policy 12-3

Street trees shall be selected from the planting matrices included in the Specific Plan (see Plant Palette in Section 12-17) and shall be chosen for their form, canopy, and ability to prosper in the locations proposed. Intersections and entries to individual development parcels may be planted with unique species, if desired, but shall be consistent with the design and context of the particular neighborhood within the FTSSP.

Policy 12-4

Street trees shall be planted on both sides of each street where possible at a spacing of 30 feet for commercial, 25 feet for residential

and 20 feet for industrial, unless an informal planting plan is approved by City. Unless required by the growth characteristics of selected species, all street tree planting shall typically be a minimum 3-inch caliper and 15-foot tall and shall be irrigated by low emission bubblers.

Policy 12-5

Both formal and informal planting patterns shall be used to promote a diverse streetscape with greater formality preferred in the Town Center. The mature scale and growth habits of street trees shall be considered in determining their placement in order to maximize their value to the Project and provide scale and articulation of building entries.

Policy 12-6

Landscape parkways and medians within the right-of-way for each street may be designed to incorporate passive storm water management, including low-flow storm water drainage channels, bioswales, bioremediation trenches, and other forms of sustainable storm water management, where such drainage will not undermine the structural integrity of the abutting street.

Policy 12-7

The establishment of high water-use landscapes within public rights-of-way, such as irrigated turf, shall be limited. Turf areas shall be located to enhance the overall town building principles of the Specific Plan.

Policy 12-8

Surfaces shall meet Americans with Disabilities Act (ADA) requirements for universal access and be designed according to the City's Standard Specifications for sidewalks and recreation pathways.

Policy 12-9

Parking lots are serviced by the street network and provide a nucleus of activity for commercial- and community-serving endeavors. Parking lots shall be designed to promote clear access from individual parking stalls to the adjacent uses. They shall be planted with a minimum of 1 tree per 8 stalls in order to reduce reflective heat and promote a clear path from the vehicle to the appropriate land use. Where possible, parking lots shall also include bioswales and other forms of storm water management and treatment consistent with the sustainability goals described in the Specific Plan.

12.5.1 Vanden Road

Vanden Road is proposed as a four-lane road with a median and is one of two roadways connecting the city of Vacaville to the city of Fairfield, through the Specific Plan area. The tree planting scheme reflects the region's orchards while the understory should consist of tall grassland communities to achieve the "rural look" and honor the grasslands that surround this project. Refer to Section 10.2.1 for street section.



12.5.2 Cement Hill Road

Cement Hill is the easterly continuation of the partially improved Manuel Campos Pkwy which will connect to Vanden Road at Peabody Road within the FTSSP Area. Cement Hill Road should include a double row of formal canopy street trees on both sides of the road coupled with a double row of formal canopy trees in the median. To complement the rural look of the surrounding grassland community, a simple understory treatment of the tall grasses shall be installed in the wide parkways, landscape setbacks and medians. Refer to Section 10.2.1 for street section.

12.5.3 Peabody Road

The landscape treatment on Peabody Road through PA1, PA2 and PA3 shall have a more formal planting pattern given the proposed urban nature of the abutting development. A double row of street trees shall be planted on either side of the road to provide shade for pedestrians. The portion of Peabody Road from the northern end of PA9 to the intersection of the New Canon Road will have an asymmetrical street section with a 10-foot sidewalk on the east/development side of the road. The streetscape for this portion should reinforce a rural setting, with drifts of primary street trees carefully integrated into the parkway, sidewalks and landscape setbacks to provide shade and encourage pedestrian circulation. A simple understory treatment of tall grassland communities in the parkways and landscape setback should provide a seamless transition to the adjacent open space. The portion of Peabody Road north of New Canon Road should also reflect this rural look; however, there will be a 10-foot wide multi-use trail in this segment to the Vacaville city limits.

12.5.4 New Canon Road

With its new alignment, New Canon Road connects the Travis AFB north gate with the FTSSP and the City of Vacaville. The streetscape should include drifts of primary street trees carefully integrated into the street design with an understory of native and low water vegetation. The trees should be selected and placed to reduce the scale of large structures, provide shade, and promote pedestrian circulation. Refer to Section 10.2.1 for street section.

12.5.5 Industrial Collector

The Industrial Collector is the primary circulation route through an industrial portion of the project. The landscape should reflect a “rural” design concept utilizing a native plant palette. Refer to Section 10.2.2 for street section.

12.5.6 Connector Streets

Neighborhood connector streets are the backbone of the Specific Plan area. The landscape design for connector streets should include formal street trees planted in equal on-center spacing to provide a



sense of order to the streetscape. This formal spacing should be interrupted when the street touches any open space of a park. The park landscape design takes precedence over the streetscape to reinforce the importance of park and open space to the community. Refer to Section 10.2.3 for street section.

12.5.7 Boulevard

The Boulevard that links the Town Center and the Village Center within the FTSSP will create iconic roadways through the Project that will include central landscape medians and potentially include neighborhood park amenities. The boulevard should be designed to embrace the landscape themes described in the Specific Plan. Placement of larger trees in the landscape median and parkways should be carefully integrated into the Boulevard design to reduce the scale of large structures, provide shade, and promote pedestrian circulation. Refer to Section 10.2.4 for street section.

12.5.8 Main Street

The Main Street will be primarily a commercial street within the FTSSP and should be designed to reflect its role as the major civic, retail, and entertainment corridor of the project. Placement of larger trees in the tree wells should be carefully integrated into the Main Street design to reduce the scale of large structures, provide shade, promote pedestrian circulation, and maintain views to shop windows and entrances. Refer to Section 10.2.5 for street section.

12.5.9 Icon Streets

The Icon Streets represent a network of residential scale streets that link parks, open spaces, schools, and other community elements. Double rows of street trees in the parkways and in the landscape corridor provide a strong sense of order, provide shade, and bring scale to the streetscape. Street parking can occur on both sides of the street. Refer to Section 10.2.6 for street section.

12.5.10 Residential Streets

The Residential Streets within the FTSSP provide the day-to-day access to homes within the Project. Street trees in the landscape parkways will be integrated into the street design to reduce the scale of the homes and to promote easy pedestrian circulation along sidewalks. Refer to Section 10.2.7 for street section.

12.5.11 One-Way Street

The One-Way Streets may be allowed in unique locations, primarily in the low traffic volume, residential areas. They have the narrowest right-of way but still allow for street parking on the residential side of the street. Street trees should be selected to provide shade, provide a sense of scale, and add order to the streetscape. Refer to Section 10.2.8 for street section.





12.5.12 Alleys

The Alleys within the FTSSP provide the day-to-day vehicular access to homes within portions of the residential neighborhoods. Alleys should be designed to minimize nonpermeable pavement and screen appurtenances and refuse container storage. Smaller species of trees should be planted in the Alleys when possible. Refer to Section 10.2.9 for street section.

12.6 Community Elements

This section describes common elements that contribute to the overall town building principles for the FTSSP.

12.6.1 Parks and Open Space

The FTSSP park system is intended to provide residents and visitors a meaningful public space experience, be educational, and promote active participation and healthy lifestyles.

The FTSSP will provide a connected system of public landscapes, including parks and open spaces that emphasize the landscape setting and history of the Specific Plan area. By providing a comprehensive framework and consistent design, the parks, open spaces, and streetscapes should link the land use components of the Project, creating strong visual, functional, and circulatory connections.

The parks and open spaces of FTSSP are intended to:

- Meet or exceed the general plan standards of 3.5 acres per 1,000 people
- Function as focal points within the Project for residents and visitors by offering safe opportunities for people to gather, recreate, exercise, rest, and play
- Create linkages between various neighborhoods and land uses within the FTSSP through an integrated system of convenient and safe pedestrian and bicycle pathways
- Create landscaped gateways and buffers at the Project's perimeter that enhance the site and ensure compatibility with surrounding land uses and development

Additionally, the open spaces and parks of the FTSSP should be further enhanced through the careful articulation of the following elements:

- A hierarchy of interconnected streets
- An integrated system of sidewalks, and bicycle and pedestrian pathways
- A clear and easily navigable program of directional, safety, and informational signage
- Landscape buffers and screening at the Project perimeter

- Park and street furnishings

The Fairfield Parks and Open Space Framework may include, but is not limited to the following elements:

- Pocket Parks
- Neighborhood Park
- Lake Park
- Great Park (Community Park)
- Linear Park Trail

See Table 12-1 and Figure 12-5 for the Open Space elements within the Specific Plan area. For a detailed description of public and private park/open space and recreation refer to Chapter 9.0, Public Services.

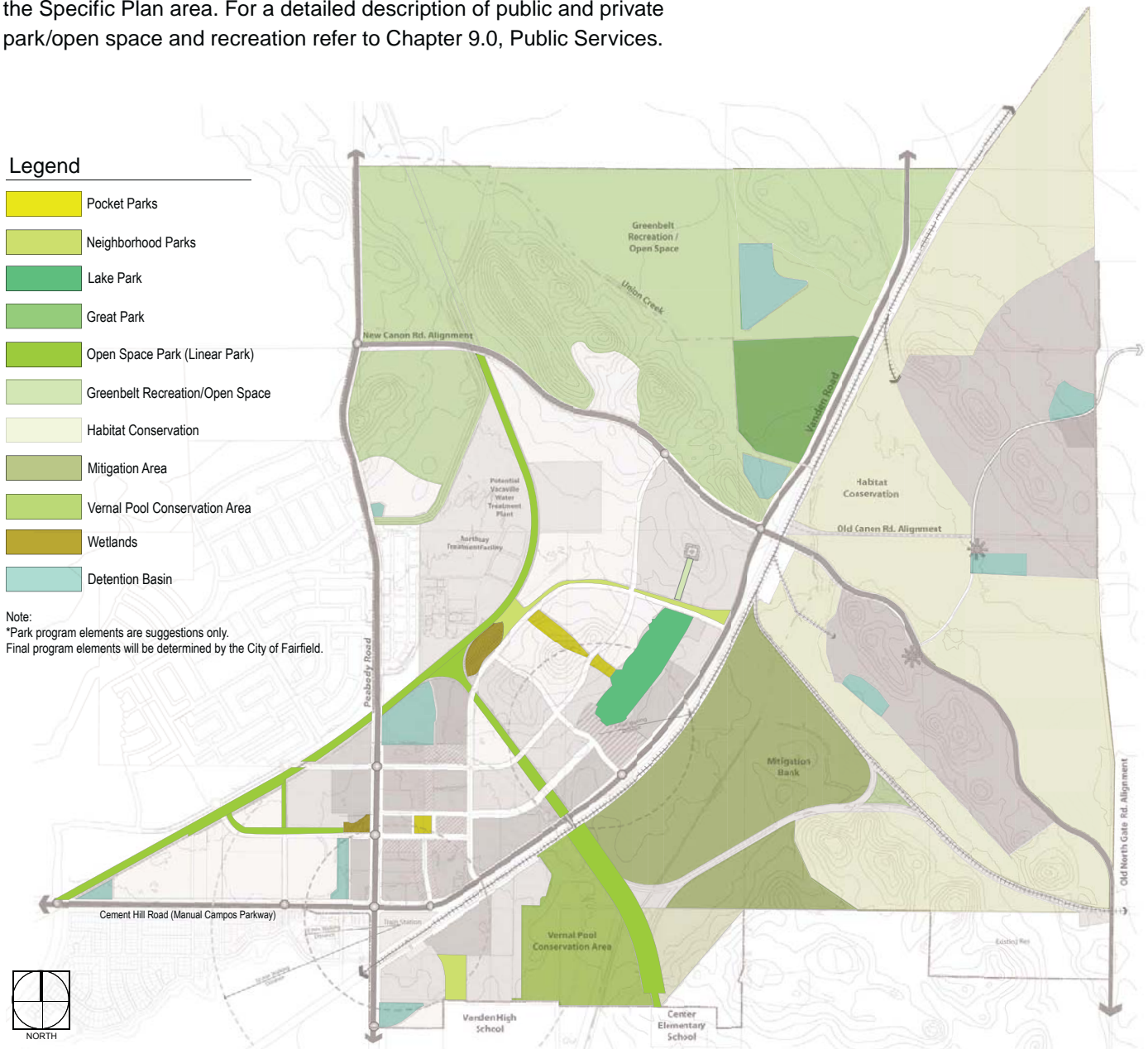


Figure 12-5: Open Space Diagram

Program Elements	Pocket Parks .33 Ac. To 3.0 Ac (P1-P8)	Neighborhood Park 5.0 Ac. To 7.0 Ac (P9-P11)	Lake Park 22.0 Ac (P12)	Great Park 50.0 Ac (P13)	Open Space (Linear Park) 40.9 +/- Ac (P14)	Greenbelt Recreation Open Space (P15)	Habitat Conservation (P16)	Mitigation Band (P17)	Vernal Pool Conservation Area (P18)
Age Specific Play Areas (2-5 & 5-12)	*	*		*					
Amphitheater				*					
Batting Cages				*					
BBQ Stations	*	*		*					
Climbing Walls				*					
Court Games		*		*					
Exercise Stations		*	*	*	*				
Field Games		*		*					
Fishing Points			*	*					
Informal Seating	*	*	*	*	*	*	*	*	*
Lake			*	*					
Nature Education Center				*					
Open Meadow			*	*					
Open Turf Areas	*	*	*	*					
Parking	On-Street	On-Street	On-Site	On-Site	On-Street				
Picnic Areas	*	*		*					
Restroom				*					
Seating Plazas			*						
Security Lighting	*	*	*	*	*				
Shade Structure/Pavilions	*	*	*	*					
Skateboard Park				*					

Figure 12-1 Open Space Program Elements

12.7 Gateways

This section describes community gateways and neighborhood entry hierarchy principles for the FTSSP.

12.7.1 Peabody Road and Vanden Road

The Train Station Community Gateway at the cross streets of Peabody Road and Vanden Road is intended to be a hub of North East Fairfield activity due to its proximity to the Union Pacific railroad station. The plaza at this location should provide a distinct sense of arrival and serve as a gateway to the new development. The plaza should also provide an ease of pedestrian circulation and iconic architecture at the corners while maintaining the small town/village character.

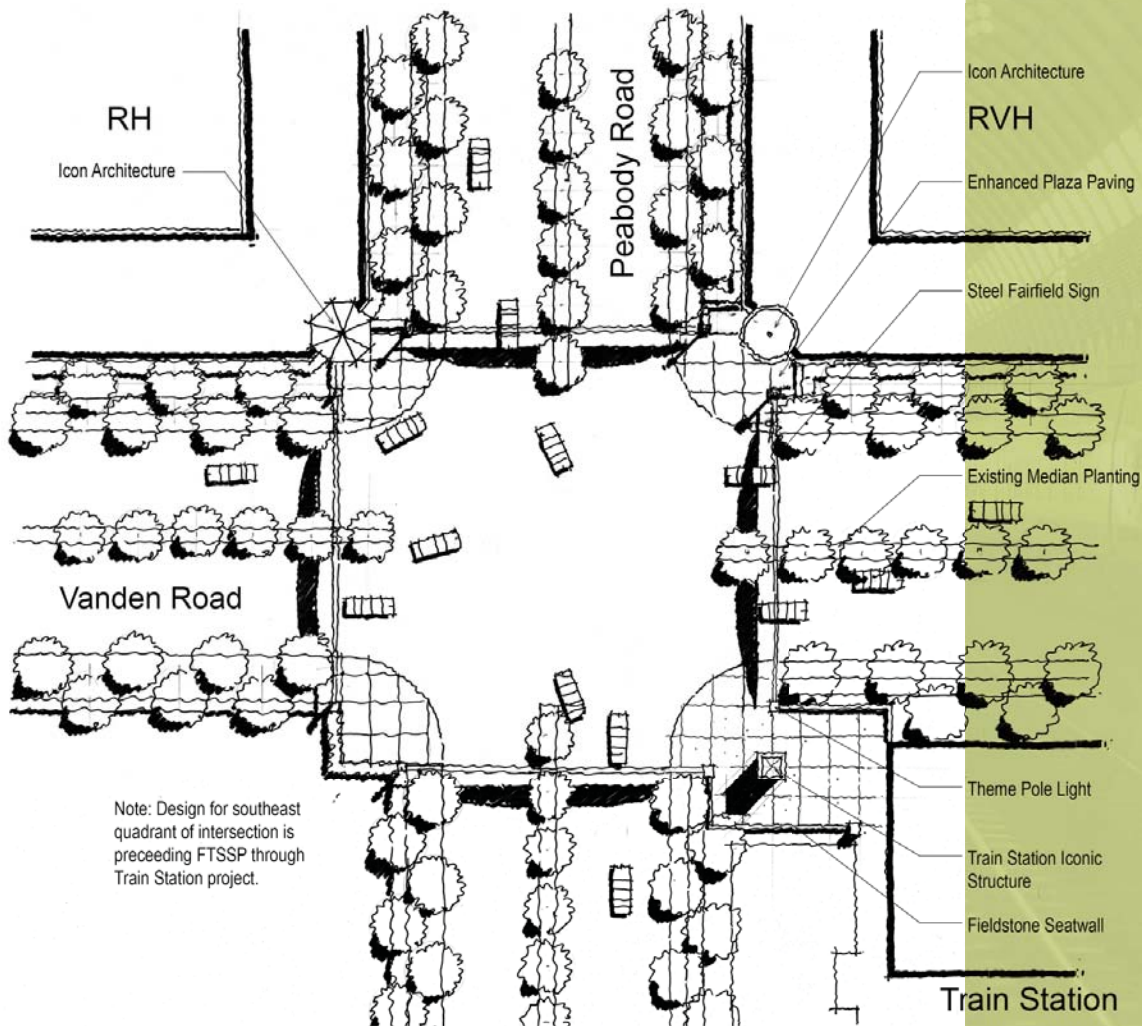
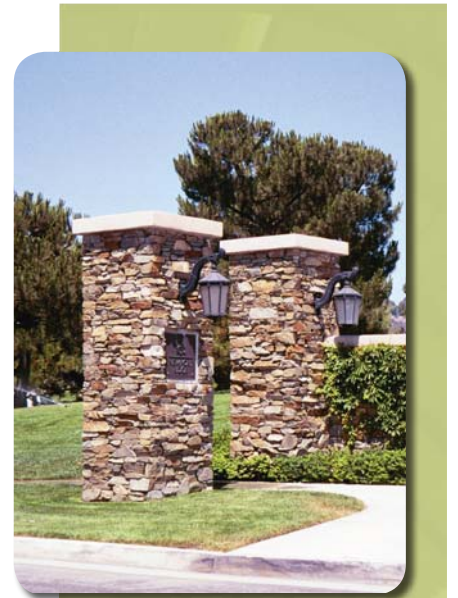


Figure 12-6: Gateway at Peabody Rd. and Vanden Road

12.7.2 Peabody Road and New Canon Road

The Hillside Community Gateway at the cross streets of Peabody Road and New Canon Road is intended to mark the significant intersection to welcome people into the city of Fairfield. The gateway should integrate local historical materials and structures to announce arrival. Plant material shall be indigenous trees, shrubs, and grasses to blend into the rolling grasslands.

12.7.3 Vanden Road at the Great Park

The Vanden Road Community Gateway at the Great Park is intended to be a welcome sign into the city of Fairfield. The design of this Gateway shall be determined through the approval of the Great Park design. The landscape should be integrated into the Great Park theme and may include local wall materials, fences, accent lighting, signage, appropriate paving material, and indigenous landscape planting.

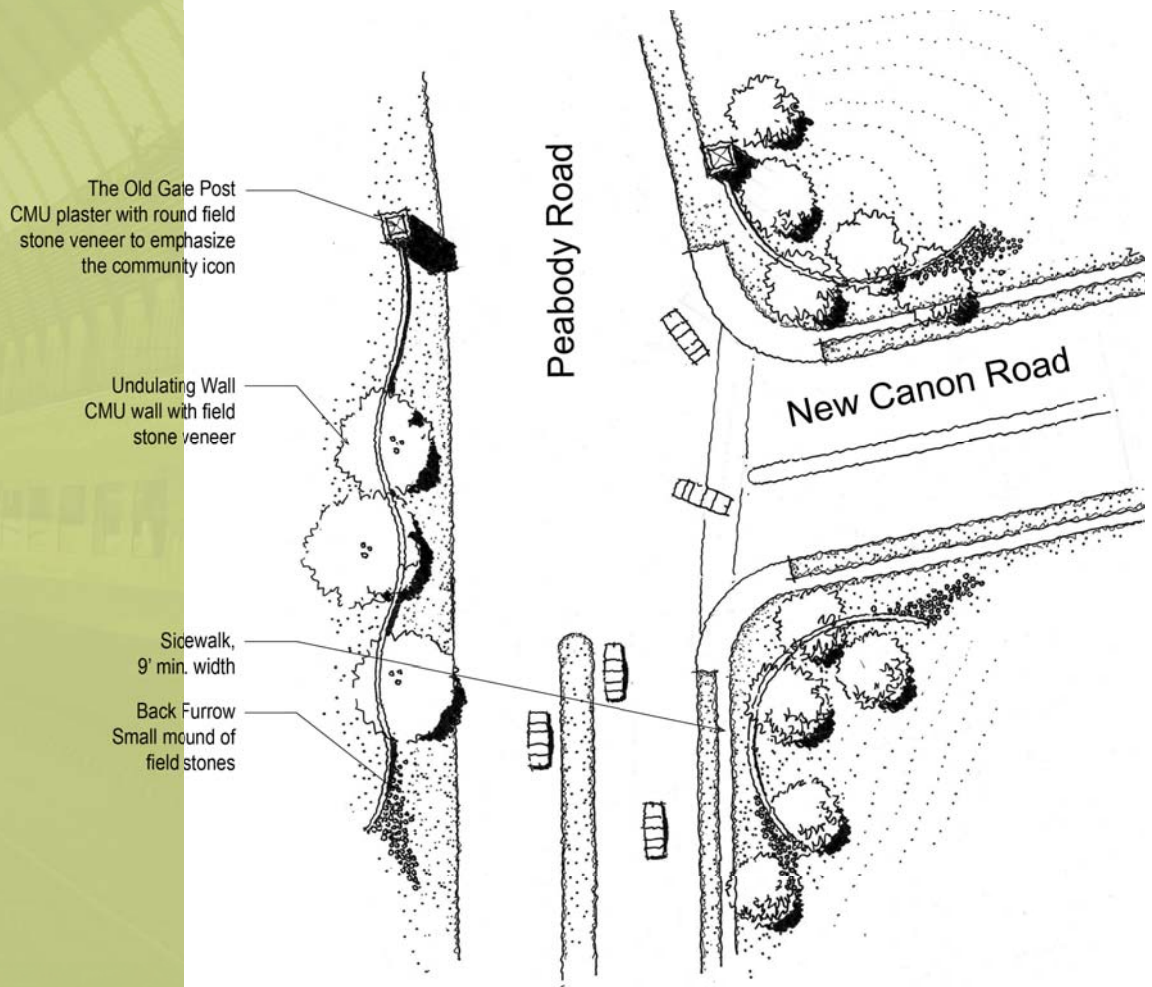


Figure 12-7: Gateway at Peabody Rd. and New Canon Rd.

12.7.4 Major Neighborhood Entry

Major Neighborhood Entries in Specific Plan area will serve as corner markers that provide a sense of arrival and provide a signature element within each neighborhood, and set the landscape theme for each neighborhood (see Figure 12-8).

12.7.5 Minor Neighborhood Entry

Minor Neighborhood Entries in the Specific Plan area are tertiary markers that further reinforce the respective neighborhood theme. These entry markers are smaller in scale than the major entry elements but they integrate the same construction material vocabulary (see Figure 12-9).

Figure 12-10 depicts the locations of community gateways, major neighborhood entries, and minor neighborhood entries with the Specific Plan area.

12.7.6 Vehicular Bridge

Vehicular bridges can be iconic elements in the community. Their function is to separate and provide smooth flow without interruption. All bridges built into this community should adopt a consistent theme to complement the community gateway markers and reinforce the rural character surrounding the City of Fairfield. Walls, pilasters, railings, paving, and graphic finishes should have a similar treatment to achieve a consistent theme. Not only should the bridges aesthetically relate to one another, but they should also visually tie into the greater community element design.

12.7.7 Pedestrian/Bicycle Trail Overcrossing and Undercrossing

Pedestrian/bicycle trail overcrossings and undercrossings are proposed at select locations. Their functions are to separate the pedestrian and vehicular circulation systems for safety and ease of movement throughout the community.

Figure 12-11 depicts the locations of vehicular bridges, pedestrian overcrossings and undercrossings.



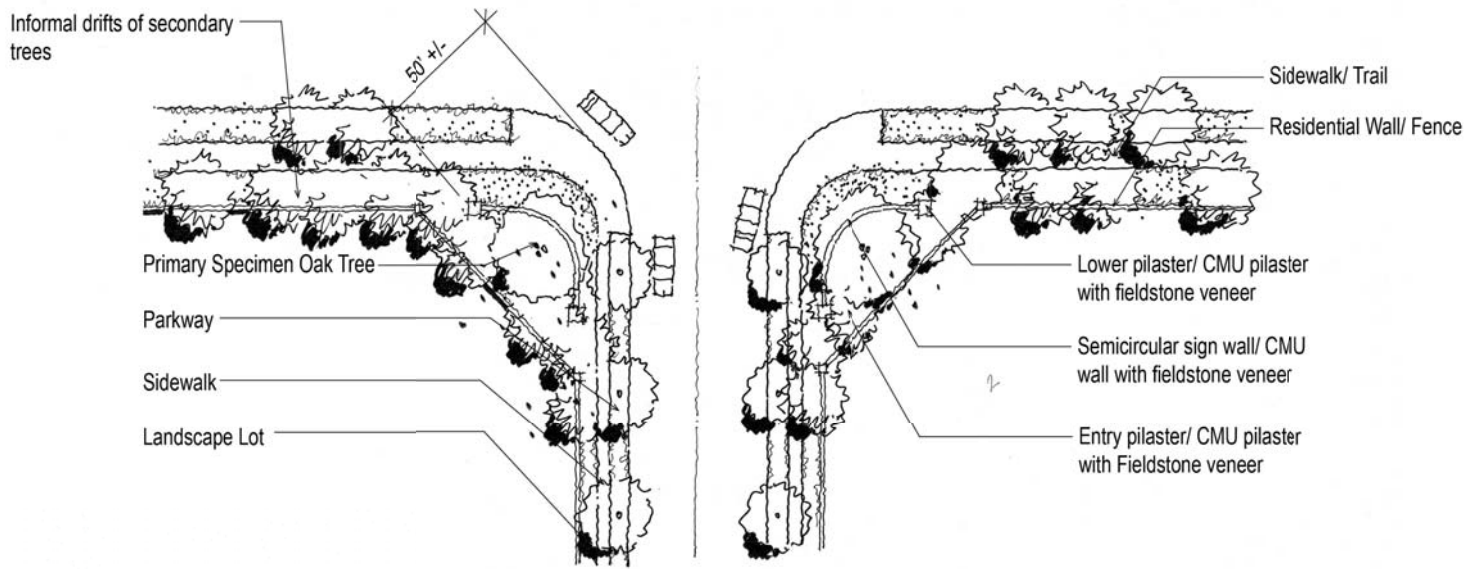


Figure 12-8: Major Neighborhood Entry

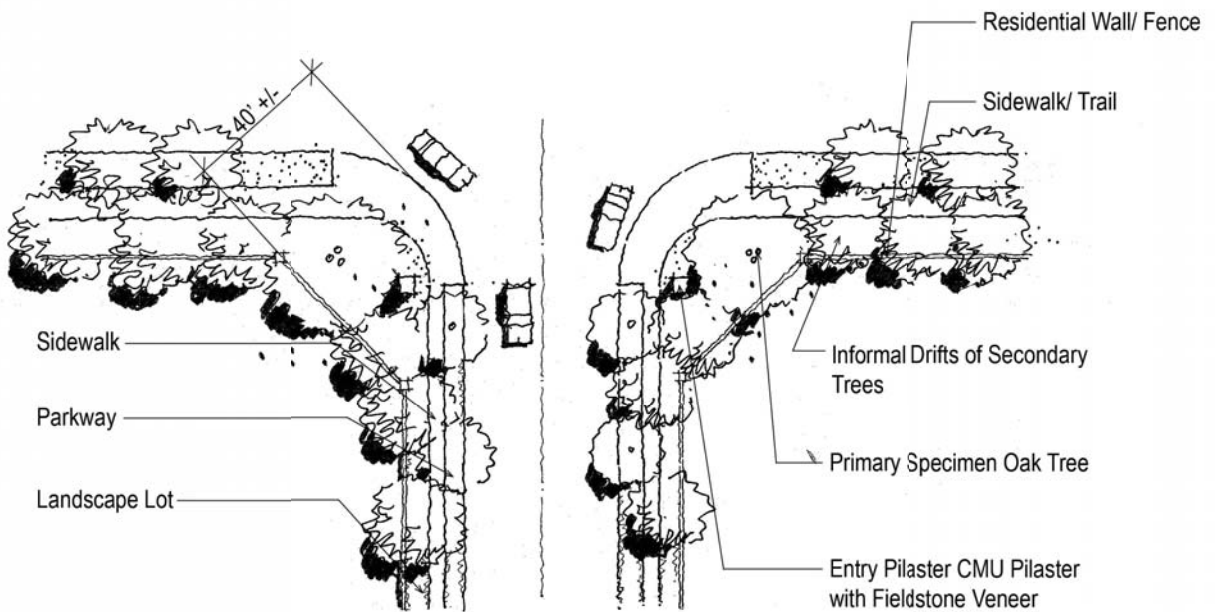


Figure 12-9: Minor Neighborhood Entry



Figure 12-10: Community Gateway and Neighborhood Entry Plan

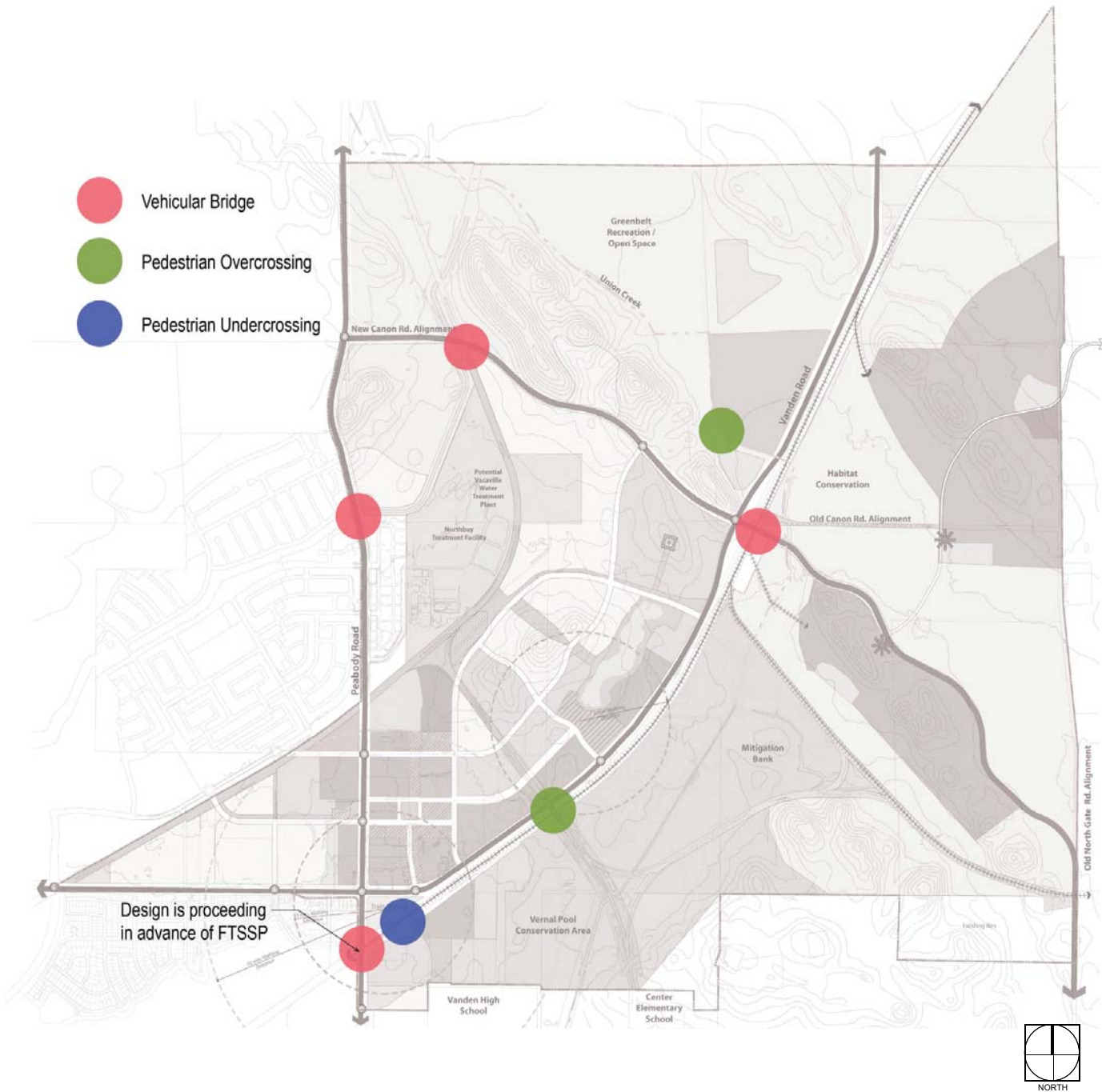


Figure 12-11: Vehicular Bridge, Pedestrian Overcrossing and Undercrossing Location Plan

12.8 Site Furnishings

Site furniture should be provided throughout the public realm of the FTSSP to provide opportunities for the residents and visitors to come together or take a rest. Opportunistic seating should be promoted, through the location of appropriately sized landscape elements such as rocks, play structures, walls, and other landscape elements if more formal seating is not readily available.

Other site furniture should include signage, bicycle parking, litter bins, recycling bins, bollards, and other traffic control devices. These should be selected, where necessary and desirable, to complement the adjacent development and the community building principles described in this plan.

All street furniture should be placed in safe, well-lit locations, minimizing risks to personal safety and vandalism of furniture.

Seating is provided in two forms: freestanding furnishing such as benches and chairs at high activity areas and view nodes; and informal seating provided by low walls and landscape features created to define use areas, emphasize vistas, and shape the pedestrian environment.

Other potential street furnishings include:

- Decorative, oversized planters
- Flag poles
- Trash receptacles and ash urns
- Bicycle racks
- Drinking fountains

Bicycle racks are located at easily accessible, well-lit areas of a transit center, as a way to promote alternative transportation.

Policy 12-10

Seating should be:

- Skate resistant and not allow sleeping.
- Shaded by tree canopy, shade structures, or extensions of adjacent architecture if possible.
- Provided in high activity areas, placing seating in areas easily accessible to pedestrians.
- Incorporate picnic benches, game tables, or other forms of interactive seating in designated areas along the Lake Park.
- Safe distance from vehicular traffic.



12.9 Bus and Transit Shelters



Bus and transit shelters will not only provide shelter while waiting for transport but will also provide a valuable community building resource. Generally, bus and transit shelters provide opportunities for community notice boards and modest advertising opportunities for local businesses along with a valuable tool in defining the look and feel of the Project as a whole.

Detailed design of bus and transit shelters should be architecturally unique to FTSSP as a whole, yet may be modified to reflect the adjoining land uses and neighborhoods.

Policy 12-11

Bus and transit shelters may be provided at transit stops and designated intersections along planned transit routes. When provided, these shelters will generally be located adjacent to major neighborhood gathering places such as neighborhood commercial areas, parks, schools, and major activity-generating land uses.

Policy 12-12

The design of the bus and transit shelters shall embrace sustainable principles through potential use of solar energy to provide lighting in evening hours as well as provide efficient shading and shielding of passengers from inclement weather.

Policy 12-13

Bus and transit shelters shall be located to give clear lines of sight for passengers waiting for transport from both standing and seated positions within the shelter. ADA principles shall be used to promote easy access to and from buses and to promote universal access to this valuable resource.

12.10 Residential Landscape Design Guidelines

The landscape within the residential neighborhoods of the FTSSP provides a valuable resource for the Project as a whole due to the City's requirement for homebuilders to provide front yard landscaping and irrigation. This creates an opportunity to incorporate an aesthetic theme that complements the overall agrarian and cultural landscape themes.

Landscape themes for residential landscapes at the FTSSP should be based on examples prevalent in the region, complementing the architectural style of each type of residential development. These residential landscapes should also provide a contemporary and more water-sensitive interpretation of typical domestic landscapes.

Regardless of landscape theme, plantings should be designed and selected to minimize the use of irrigation beyond an initial plant establishment period of two years. Plant materials tolerant of growing conditions in the region and those that typically require less irrigation

than traditional ornamental landscape species should be used. Typical water use budgets should be required for each lot, home style, and multi-family development.

Hedges, fences, and walls often mark the development perimeters and assist in defining the public street edge. Behind this edge is typically a mix of perennials, succulents, native shrubs, and ornamental trees. Within the Specific Plan area, residential landscapes should seek to reflect this tradition yet seek to be more contemporary in their installation in an attempt to further implement water conservation measures in the landscape.

Street Side Landscapes

The street side landscape area includes the area from the front of the dwelling unit, side yard, and walls to the street. The design for the street side landscape should relate to the landscape theme and architectural style of the residential development. The street side landscape for each residence should be specifically designed for the actual configuration of the lot and should consider housing type, adjacent housing and other land uses, solar orientation, views, access, and visibility in defining plant and tree locations.

Policy 12-14

Planting in the residential neighborhoods of the FTSSP shall utilize a combination of the following plantings depending on housing type and development:

- Screen planting
- Formal hedge planting
- Informal hedge planting
- Enhanced planting
- Foundation planting
- Turf
- Street tree

Screen Planting

Screen planting shall consist of a diverse combination of large and medium shrubs and vines. The goal of screen planting should be to reduce the scale of large blank walls and façades, and to provide separation between adjacent land uses.

Formal Hedge Planting

Formal hedge planting should consist of a limited combination of medium or small shrubs capable of being trimmed to shape. The goal of formal hedge planting should be to take the place of fences in defining boundaries or for screening of adjacent uses where lateral conditions are restricted.



Informal Hedge Planting

Informal hedge planting should consist of a diverse combination of medium and small shrubs and may potentially be trimmed to shape. The goal of informal hedge planting should be to provide boundary definition and low-scale screening that is integrated into field planting.

Enhanced Planting

Enhanced planting should consist of a wide array of species including small trees, large, medium, and small shrubs and ground covers to provide visual and functional interest for the outdoor space. The goal of enhanced planting is to provide a diverse planted landscape that covers large areas, reduces water demand, and provides an attractive and patterned landscape to the yard.

Foundation Planting

Foundation planting shall consist of small and medium shrubs and ground covers. The goal of foundation planting is to provide low-level screening of the foundation and ground plane of each housing type and provide screening of unsightly elements such as air conditioning units.

Turf

Turf shall consist of natural turf plantings or artificial turf, which meets the landscape sustainability goals. Natural turf species should be drought-tolerant, low-water demand species wherever possible and consistent with the intended role of the turf area.

Enhanced Planting

Enhanced planting should consist of a wide variety of plant species planted in patterns to provide visual and functional interest to the front yard and to enhance the entry experience and views from the home. Enhanced planting follows the city-required front yard landscaping and irrigation code and allows homeowners to customize their landscape, including adding elements such as pots, small water-conserving features, arbors, trellises, and the like.

The juxtaposition of the above planting types should be laid out for each lot in a manner that reduces water demand, as described in the Specific Plan, and meets the functional needs of each lot.

The following policies apply to all types of residential development:

Policy 12-15

Provide street side landscaping with the combination of screen planting, hedges, foundation planting, enhanced planting, turf and street trees.



Policy 12-16

All landscaping shall be designed and installed in accordance with the City's water efficient landscape ordinance in effect at the time of landscape plan approval.

Policy 12-17

At least 50 percent of the front yard landscape shall be enhanced planting.

The following policies apply to Single Family Detached only:

Policy 12-18

Front and street side yard landscaping shall be installed by homebuilders prior to occupancy of any residential housing unit for residential lots less than 6,000 square feet.

Policy 12-19

Residential development entries shall be emphasized through the use of enhanced planting, including a mix of native/adaptive planting.

The following policies apply to Townhomes/Condominiums, Apartments, Green Courts and Auto Courts:

Policy 12-20

Soften large vertical walls with screen planting.

Policy 12-21

Refer to the Fairfield Municipal code regarding minimum tree density per square foot of the landscape area.

Policy 12-22

In either natural turf plantings or artificial turf, soil permeability and storm water management concerns shall be addressed.

The following policies apply to Townhomes/Condominiums, Apartments, and Green Courts:

Policy 12-23

Link common areas and open spaces with clearly defined pedestrian circulation and reinforce with a themed landscape treatment.

Policy 12-24

Screen any utility structures, trash enclosures and/or parking lots.

The following policies apply to Townhomes/Condominiums, Apartments and Auto Courts

Policy 12-25

Use the landscape to clearly define and reinforce vehicular and pedestrian circulation and to avoid conflict with one another.



Policy 12-26

Reinforce project monumentation with hedges and enhanced planting.

The following policy applies to Single Family Detached, Townhomes/Condominiums and Auto Courts:

Policy 12-27

Refer to Fairfield Municipal code regarding quantity of street trees per street frontage.

The following policy applies to Townhomes/Condominiums and Apartments:

Policy 12-28

Clearly define the main entrance experience with a themed landscape treatment.

The following policy applies to Townhomes/Condominiums and Green Courts:

Policy 12-29

Integrate canopy trees along pedestrian circulation to provide shade and promote the walking experience.

The following policy applies to Townhomes/Condominiums and Auto Courts:

Policy 12-30

Canopy trees shall be planted along the vehicular circulation to provide shade and mitigate the potential of “heat island effect” from any large expanses of paving

12.11 Non-Residential Landscape Design Guidelines

The landscapes within nonresidential use areas of the FTSSP should provide a valuable resource for the Project as a whole and should offer the ability for each owner/occupant to customize their landscape area to maximize use, provide locations for employee and public gathering, and be a significant visual amenity.

Landscape themes for nonresidential landscapes at the FTSSP should be based on examples prevalent in the region, and should complement the adjoining architectural style. These landscapes should also provide a more water-sensitive interpretation than typical nonresidential landscapes of the area.

Regardless of the landscape theme, plantings should be designed and selected to minimize the use of irrigation beyond a plant establishment period of 2 years and should focus on the use of plant materials tolerant of growing conditions in the region and that typically require

less irrigation than traditional ornamental landscape species. Typical water use budgets should be required.

Regionally, the landscapes associated with nonresidential uses should be eclectic with a mix of mature landscapes incorporating perennials, succulents, shrubs, shade, and ornamental trees. Periodically, hedges, fences, and walls should be used to define perimeters. At the FTSSP, the nonresidential landscapes should seek to improve upon this tradition, be more contemporary in their installation, and provide a more Project-oriented landscape that promotes a sense of gathering and comfort in both public and private landscape spaces. Wherever possible, water conservation techniques should be integrated into the landscape design, including rain gardens, storm water management features, and low water demand landscapes.

This section illustrates key elements and design strategies for nonresidential uses in the FTSSP. The guidelines recognize that each owner or tenant may desire to customize their landscape surroundings and, as such, describe a minimum level of landscape.

12.11.1 Town Center

Landscapes in the Town Center should be designed to accommodate a diverse range of activities and events from strolling and sunbathing to parades, community festivals, farmers markets, and other special events that help build a sense of community. Consequently, the Town Center should include a range of landscape spaces that offer both formal and informal gathering locations in both hard and soft form. Wherever possible, sidewalks should offer a clear line of sight to commercial premises, be protected from the elements by either shade trees or awnings, and incorporate a consistent design for street furniture, signage, and landscape elements. Gathering spaces and communal areas should include seating opportunities, both formal and opportunistic, such as low walls and landscape berms. Consideration of special event signage should be included in the form of changeable event signs on lamp posts and other vertical elements in the landscape.

Landscapes within the Town Center should seek to maximize display windows for retail establishments wherever possible. Formal dining and seating opportunities should be encouraged adjacent to dining establishments and these areas should be landscaped with shade structures and separators such as modest walls, fences, and changes in grade.

12.11.2 Lake District

Landscapes in the Lake District should be designed to accommodate a diverse range of activities and events from jogging and sunbathing to concerts, holiday picnics, community festivals, and other special events that help build a sense of community. Therefore, the Lake District should be designed to include a variety of landscape spaces





that encourage formal and informal gatherings in both soft and hardscape. Wherever possible, trails should offer clear lines of sight to the lake, include tree shaded sections, and incorporate a consistent design for site furniture, signage, and landscape elements. Gathering spaces and communal areas should include seating opportunities, both formal and opportunistic, such as low walls, boulders, and landscape berms. Consideration of special event signage should be included in the form of changeable event signs on lamp posts and other vertical elements in the landscape.

Landscape shall be used to separate the Lake Park from the rear yards and patios of the directly adjacent homes. A landscape transition zone shall be created that inhibits the public from crossing out of the park onto private land through intense groundcover and shrub landscape. Development pads shall be elevated at least three feet above the park land to further insure privacy.

12.11.3 Commercial

Landscapes in the commercial areas within the FTSSP should be designed to accommodate a range of activities and events that support the adjacent enterprise and develop a design that is consistent with the adjoining architectural style. The commercial areas should include a range of spaces that offer both formal and informal gathering locations including formal and opportunistic seating opportunities.

12.11.4 Institutional

Landscapes in the institutional areas within the FTSSP, which include schools, community centers, and other public facilities, should be designed to accommodate the anticipated users of each facility. They should be scaled and organized in a clear plan reinforced by plant materials with the goal of articulating clear access and connections to the greater Fairfield community.

12.11.5 Industrial

Landscape in an industrial parcel is of extreme importance to promote the establishment of compatible land uses; lessen the visual bulk from a public vantage point of large manufacturing/distribution structures; maintain a consistent streetscape; screen items such as off-street parking, outdoor storage and large expanses of undifferentiated wall surfaces, and reuse/utility facilities; provide shade; and increase the overall aesthetic appeal of the area. Landscape should enhance the appearance of desirable visual elements as well as screen undesirable views and enhance the outdoor environments for users of new development as it occurs in the Specific Plan area.

12.11.6 Landscape Criteria (Nonresidential)

This section describes minimum standard landscape requirements for building setbacks in non-residential areas of the FTSSP.

Policy 12-31

Planting shall be used to direct and protect pedestrians walking through large parking areas.

Policy 12-32

Native and adaptive (low water demand) shrubs and groundcovers shall be installed in the common and street side landscape areas.

Policy 12-33

A simple and bold landscape treatment shall be implemented to minimize maintenance costs.

Policy 12-34

A mixed palette of succulents and perennials shall be introduced in the common areas, entry courts, plazas and project signage for an accent treatment.

Policy 12-35

Provide screen planting and/or opaque walls and fences in industrial parcels to screen exterior storage, machinery and equipment.

Policy 12-36

Turf in the uncovered landscape area shall be minimized, including areas from the planting beds to the edge of the public sidewalk and common walkways/building access.

Policy 12-37

Small gathering spaces shall be integrated in the appropriate common areas by providing a diverse range of landscape features including courtyards and play/seating areas consistent with the intended population of the facility.

Policy 12-38

Adequate lighting and signage shall be incorporated to enhance the facility's ability to function and manage to reduce energy demand.

Policy 12-39

Trees shall be planted to provide shade and mitigate the potential of "heat island effect" from any large expanses of paving.

12.12 Parking

This section describes minimum standard landscaping requirements for parking lots in non-residential areas of the FTSSP.





Policy 12-40

Circulation paths for both pedestrians and vehicles shall be clearly defined. Curb cuts and entry drives shall be reduced to the minimum number required to provide a functional parking lot. City may require reciprocal parking and access easements for parking lots on adjoining commercial and retail properties. These internal connections avoid traffic circulation onto public streets and improve utilization of parking lots.

Policy 12-41

Parking lots shall be screened from public streets with hedges and ornamental trees. Where landscape screening is limited, consideration of structured screens such as walls, fences, and planted structures should be considered subject to design review.

12.13 Plazas/Courtyards/Outdoor Spaces

This section describes minimum standard landscaping requirements for outdoor spaces in non-residential areas of the FTSSP

Policy 12-42

Plazas, courtyards, and other open, outdoor gathering spaces shall be provided adjacent to buildings, wherever possible, to enhance the quality of environment and the design of each building.

Policy 12-43

Plazas, courtyards, and other open spaces shall be located along south-facing walls where possible to maximize solar gain.

Policy 12-44

Seating opportunities shall be integrated in the plazas, courtyards, and other open spaces.

Policy 12-45

Ornamental and perennial planting shall be integrated at entries to each space to enhance the sense of community

12.14 Service Docks/Loading Bays

This section describes minimum standard landscaping requirements for service and loading bays in non-residential areas of the FTSSP.

Policy 12-46

All service docks and loading bays shall be screened from adjacent streets, open spaces, and public pathways by walls designed as part of the architectural expression of each facility. Additional screening should be provided by shade trees and tall shrub and ornamental tree planting.

12.15 Utility Screening

This section describes minimum standard landscape requirements for screening of utilities in the FTSSP. Utilities should be incorporated into building design and landscape areas to minimize the potential visual and noise impacts.

Policy 12-47

Utilities above ground such as transformers, backflow devices, switchboxes, related vaults or cabinets, and related facilities shall be designed and screened to minimize functional and visual impacts on the neighborhood and along pedestrian ways. While adequate room for servicing must be provided; these facilities shall not draw unnecessary attention or be visually noticeable or distracting.

Policy 12-48

Air conditioning equipment/units, to the greatest extent possible, shall be located at the rear or side of buildings/units away from public views. HVAC units shall be screened by landscaping and/or enclosed with screening materials that are either consistent or compatible with materials used on the main building/unit.

12.16 Trash and Recycling

Trash and recycling facilities must be provided. As a variety of building types are required for each neighborhood, several solutions for waste management are required, as discussed below.

12.16.1 Curbside Cart

Policy 12-49

Individual trash containers shall be used by small-lot detached homes and townhomes with individual garages.

Policy 12-50

Appropriately sized concrete pads shall be placed adjacent to driveways along alleys in keeping with applicable waste management collection standards (see Figure 12-12). Approximately 10 linear feet of space per dwelling unit shall be provided for this purpose. This allows for a 2-foot-wide trash, a 2-foot-wide recycling and a 2-foot-wide yard waste cart with 3 feet in between and space on either side (sizes approximate).

12.16.2 Trash Enclosures

Policy 12-51

Buildings that do not include driveways for individual units or that have very closely spaced driveways, such as garden court attached buildings or tuck-under building types that have group parking areas, must provide common trash enclosures. Enclosures must be a



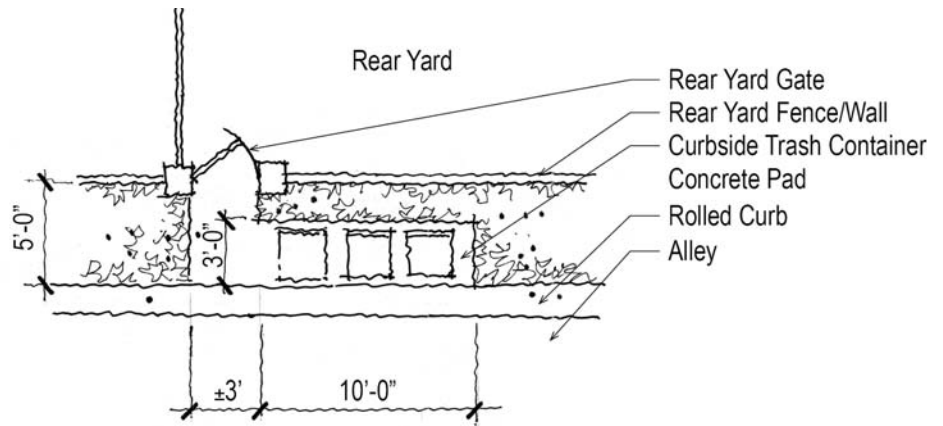


Figure 12-12: Curbside Cart

minimum of 250 feet from the nearest point of each unit and at least 15 feet from front entrances.

Policy 12-52

Common trash enclosures must be screened from view and sited to minimize conflicts with open spaces or other highly visible locations. Common trash enclosure areas shall be constructed of durable materials such as concrete block and be designed to be in keeping with the character of nearby buildings.

12.17 Plant Palette

The following plant palette provides an initial list of appropriate plant species for use by the Project. The palette is intended to serve as a guide for the selection of plant materials and may be expanded as new cultivars are developed by the nursery industry as additional species are found to be viable in the Fairfield area. For additional plant species information, refer to the City of Fairfield Street Tree List and document entitled “Drought Tolerant Plants for Fairfield.”



TREES	
SCIENTIFIC NAME	COMMON NAME
<i>Acer macrophyllum</i>	Big-leaf Maple
<i>Acer negundo</i>	Box Elder
<i>Acer palmatum</i>	Japanese Maple
<i>Acer rubrum 'Red Sunset'</i>	Red Sunset Maple
<i>Aesculus californica</i>	California Buckeye
<i>Ailanthus altissima</i>	Tree of Heaven
<i>Albizia julibrissin</i>	Silk Tree/Mimosa
<i>Alnus cordata</i>	Italian Alder
<i>Alnus rhombifolia</i>	White Alder
<i>Arbutus menziesii</i>	Madrone
<i>Betula pedula 'Dalecaslica'</i>	Cutleaf Weeping Birch
<i>Broussonetia papyrifera</i>	Paper Mulberry
<i>Callistemon citrinus</i>	Lemon Bottle Brush

<i>Calocedrus decurrens</i>	Incense Cedar
<i>Carpinus betulus</i> 'Fastigiata'	Upright European Hornbeam
<i>Carya illinoensis</i>	Pecan
<i>Catalpa speciosa</i>	Western Catalpa
<i>Cedrus deodara</i>	Deodar Cedar
<i>Celtis australis</i>	European Hackberry
<i>Celtis sinensis</i>	Chinese Hackberry
<i>Cercis canadensis</i>	Eastern Rosebud
<i>Cercis occidentalis</i>	Western Redbud
<i>Chilopsis linearis</i>	Desert Willow
<i>Cinnamomum camphor</i>	Camphor Tree
<i>Cornus nuttallii</i>	Pacific Dogwood
<i>Cupressus macrocarpa</i>	Monterey Cypress
<i>Eucalyptus</i> ssp.	Eucalyptus
<i>Fraxinus dipetala</i>	California Ash
<i>Fraxinus</i> h. 'Moraine'	Moraine Ash
<i>Fraxinus latifolia</i>	Oregon Ash
<i>Fraxinus oxycarpa</i> 'Raywood'	Raywood Ash
<i>Fraxinus velutina</i> 'Modesto'	Modesto Ash
<i>Fraxinus uhdei</i>	Evergreen Ash
<i>Fremontodendron californicum</i>	Flannel Bush
<i>Gleditsia tricanthos</i> 'Rubylace'	Red Honey Locust
<i>Juniperus californica</i>	California juniper
<i>Koelreuteria paniculata</i>	Golden Rain Tree
<i>Lagerstroemia indica</i>	Crape Myrtle
<i>Liquidambar styraciflua</i>	Sweet Gum
<i>Lithocarpus densiflorus</i>	Tanoak
<i>Lyonothamnus floribundus</i>	Catalina Ironwood
<i>Magnolia grandiflora</i>	Magnolia
<i>Maytenus boaria</i>	Chile Mayten Tree
<i>Melia azedarach</i>	Chinaberry
<i>Morus alba</i>	Mulberry
<i>Olea</i> ssp.	Olive
SCIENTIFIC NAME	COMMON NAME
<i>Pinus brutia</i>	Calabrian Pine
<i>Pinus eldarica</i>	Mondell Pine
<i>Pinus halepensis</i>	Aleppo Pine
<i>Pinus ponderosa</i>	Western Yellow Pine
<i>Pinus sabiniana</i>	Foothill Pine
<i>Platanus racemosa</i>	Sycamore
<i>Populus nigra</i> 'Italica'	Lombardi Cottonwood
<i>Pistachia chinensis</i>	Chinese Pistache
<i>Prosopis glandulosa</i> var. <i>torreyana</i>	Mesquite
<i>Prunus blireiana</i>	Flowering Plum
<i>Pyrus kawakamii</i>	Evergreen Pear
<i>Quercus ilex</i>	Holly Oak
<i>Quercus</i> ssp.	Oak
<i>Rhus lancea</i>	African Sumac





<i>Robinia x ambigua</i>	Locust Tree
<i>Salix brewerii</i>	Brewer's Willow
<i>Salix gooddingii</i>	Goodding's Willow
<i>Salix laevigata</i>	Red Willow
<i>Salix lasiolepis</i>	Arroyo Willow
<i>Sequoia sempervirens</i>	Redwood
<i>Schinus molle</i>	California Pepper
<i>Sophora japonica</i>	Chinese Scholar Tree
<i>Torreya californica</i>	California Nutmeg
<i>Tristania conferta</i>	Brisbane Box
<i>Ulmus parvifolia</i>	Chinese Elm
<i>Zelkova serrata</i>	Japanese Zelkova

SHRUBS, GRASSES AND GROUNDCOVER

SCIENTIFIC NAME	COMMON NAME
<i>Abelia grandiflora</i> sp.	Sherwood Abelia
<i>Abronia pogonantha</i>	Desert Sand Verbena
<i>Achillea millefolium</i>	Yarrow
<i>Agrotis pallens</i>	Diego Bent Grass
<i>Allium campanulatum</i>	Dusky Onion
<i>Aquilegia saximontana</i>	Rocky Mountain Columbine
<i>Arctostaphylos glandulosa</i>	Eastwood Manzanita
<i>Arctostaphylos</i> spp.	Manzanita
<i>Artemisia douglasiana</i>	Sagebrush
<i>Asclepias californica</i>	Milkweed
<i>Aster chilensis</i>	California Aster
<i>Atriplex lentiformis</i>	Brewer's Salt Bush
<i>Baccharis pilularis</i>	Dwarf Coyote Bush
<i>Baccharis salicifolia</i>	Mulefat
<i>Berberis aquifolium</i>	Oregon Grape
<i>Berberis aquifolium</i> var. <i>dictyota</i>	Jepson's Oregon Grape
<i>Buxus microphylla</i>	Boxwood
<i>Calochortus venustus</i>	Butterfly Mariposa Lily
<i>Carex glauca</i>	Blue Sedge
<i>Carex</i> ssp.	Sedge
SCIENTIFIC NAME	COMMON NAME
<i>Carpenteria californica</i>	Bush Anemone
<i>Ceanothus gloriosus</i>	Point Reyes Ceanothus
<i>Ceanothus</i> ssp.	Ceanothus
<i>Ceanothus thyrsiflorus</i>	Blue Blossom
<i>Cercocarpus montanus</i>	Mountain Mahogany
<i>Cistus 'Sunset'</i>	Magenta Rockrose
<i>Clematis ligusticifolia</i>	Clematis
<i>Cornus sericea</i>	Creek Dogwood
<i>Cornus sericea</i> ssp. <i>occidentalis</i>	Western Dogwood
<i>Dendromecon rigida</i>	Bush Poppy
<i>Deschampsia cespitosa</i>	Tufted Hair Grass
<i>Dudleya</i> ssp.	Dudleya
<i>Elymus glaucus</i>	Blue Wildrye
<i>Eriodictyon californicum</i>	Yerba Santa

<i>Erigonum fasciculatum</i>	California Buckwheat
<i>Erigonum nudum</i>	Nude Buckwheat
<i>Eschschozia californica</i>	California Poppy
<i>Euonymus ssp.</i>	Euonymus
<i>Festuca ssp.</i>	Fescue
<i>Forsythia ssp.</i>	Forsythia
<i>Fragaria ssp.</i>	Strawberry
<i>Geranium incanum</i>	Carpet Geranium
<i>Helianthemum scoparium</i>	Common Sun Rose
<i>Hemerocallis sp.</i>	Daylily
<i>Heteromeles arbutifolia</i>	Toyon
<i>Hordeum brachyantherum</i>	Meadow Barley
<i>Iris douglasiana</i>	Douglas' Iris
<i>Juncus ssp.</i>	Rush
<i>Lavandula ssp.</i>	Lavender
<i>Leymus glaucus</i>	Creeping Wildrye
<i>Ligustrum japonicum</i>	Wax-leaf Privet
<i>Lilium pardalinum</i>	Leopard Lily
<i>Limnanthes douglasii</i>	Meadow Foam
<i>Lonicera ssp.</i>	Honeysuckle
<i>Lupinus bicolor</i>	Miniature Lupine
<i>Malacothamnus fasciculatus</i>	Chaparral Mallow
<i>Medicago sativa</i>	Alfalfa
<i>Melica californica</i>	California Melic Grass
<i>Mimulus ssp.</i>	Monkey Flower
<i>Muhlenbergia ssp.</i>	Deer Grass
<i>Myrica californica</i>	California Myrtle
<i>Nasella pulchura</i>	Purple Needle Grass
<i>Phormium sp.</i>	New Zealand Flax
<i>Pittosporum sp.</i>	Pittosporum
<i>Polystichum munitum</i>	Western Sword Fern
<i>Rhamnus californica</i>	Coffeeberry
<i>Rhamnus crocea</i>	Reberry Buckthorn
<i>Rhamnus ilicifolia</i>	Evergreen Buckthorn
SCIENTIFIC NAME	COMMON NAME
<i>Rhus ovata</i>	Sugar Sumac
<i>Ribes aureum</i>	Golden Currant
<i>Ribes malvaceum</i>	Chaparral Currant
<i>Ribes sanguineum</i>	Red Flowering Currant
<i>Ribes ssp.</i>	Currant
<i>Romneya coulteri</i>	Matajila Poppy
<i>Rosa californica</i>	California Rose
<i>Rosa californica 'Elsie'</i>	California Rose
<i>Rosmarinus officinalis</i>	Rosemary
<i>Rubus spectabilis</i>	Salmonberry
<i>Salicornia ssp.</i>	Pickleweed
<i>Salvia spathacea</i>	Hummingbird Sage
<i>Salvia ssp.</i>	Sage
<i>Scirpus ssp.</i>	Tule



<i>Sisyrinchium ssp.</i>	Blue-eyed Grass
<i>Sporobolus airoides</i>	Alkali Sacaton
<i>Suaeda ssp.</i>	Suaeda
<i>Thymus ssp.</i>	Thyme
<i>Triphysaria versicolor</i>	Clover
<i>Triphysaria ssp.</i>	Clover
<i>Vitus californica</i>	California Grape

12.18 Lighting

The purpose of the Lighting Design Guidelines is to establish the design character for lighting within the Specific Plan area to ensure that lighting design is appropriate to the surrounding context, is visually attractive, and contributes to the Project’s image, character, and standards of excellence. The overall Project Design Goals discussed in Section 12.1 create a broad framework for these Lighting Design Guidelines.

The Lighting Design Guidelines are intended to create Project identity, provide for a safe and secure experience, improve access throughout the Project, and enhance the overall sense of community and place. Lighting will be integral to the establishment of an overall sense of well being and safety throughout the Project.

12.18.1 Lighting Design Principles

The following Lighting Design Principles are intended to provide guidance for the provision of lighting throughout the Specific Plan area. The lighting design principles recognize that regulations, materials, and manufacturing technologies will change and advance significantly throughout the life of the Project; therefore, they provide performance criteria and allow a degree of flexibility. The lighting design should meet the following criteria:

- Focus on “uniformity” and “visibility” rather than wattages, out-puts and foot candles;
- Provide a safe and secure experience for residents of and visitors to the Project;
- Create a strong sense of community identity for both residential and non-residential land uses and enhance the community principles outlined in this Specific Plan;
- Provide a cohesive hierarchy of lighting at a scale appropriate for the intended use including, but not limited to, arterial roads, collector roads, local streets, alleys, parking facilities, sports and recreation facilities and pedestrian areas as well as a range of decorative, accent and landscape illumination uses;
- Varying standards for light fixtures with prior City approval will be used throughout the Project to complement the sense of neighborhood identity described in this Specific Plan;



- Select light fixtures that are consistent with and complement the architecture, hardscape and landscape of each use with particular emphasis placed on high-use areas and those bounded by greater building mass or density;
- Incorporate lighting fixture materials, color and design styles that reflect the cultural history of the region when appropriate;
- Encourage and promote safe night-time pedestrian, bicycle, and vehicular movement throughout the Project with appropriate lighting design;
- Seek to enhance the Project's overall quality of life by providing appropriate lighting levels for recreational uses and natural spaces;
- Minimize light spillage onto adjacent properties and environmentally sensitive areas;
- Include appropriate shielding and/or diffusing to conceal the light source and minimize glare;
- Provide light sources that operate from dusk-to-dawn should be adequately shielded, and
- When necessary, photometric studies shall be completed by a certified lighting engineer to meet City standards.

12.18.2 Sustainable Lighting Design Policies

The lighting design should seek consistency with the Sustainability Program regarding lighting and be adaptable to new technologies as they prove themselves reliable. The following policies have been established to support, where environmentally responsible and feasible, the provision of a sustainable lighting program for the Project:

Policy 12-53

Sustainable materials shall be considered for use in the construction of lighting including, but not limited to, recycled materials and/or certified "green" products.

Policy 12-54

Light fixtures shall be designed and constructed of solid, durable, long-life materials.

Policy 12-55

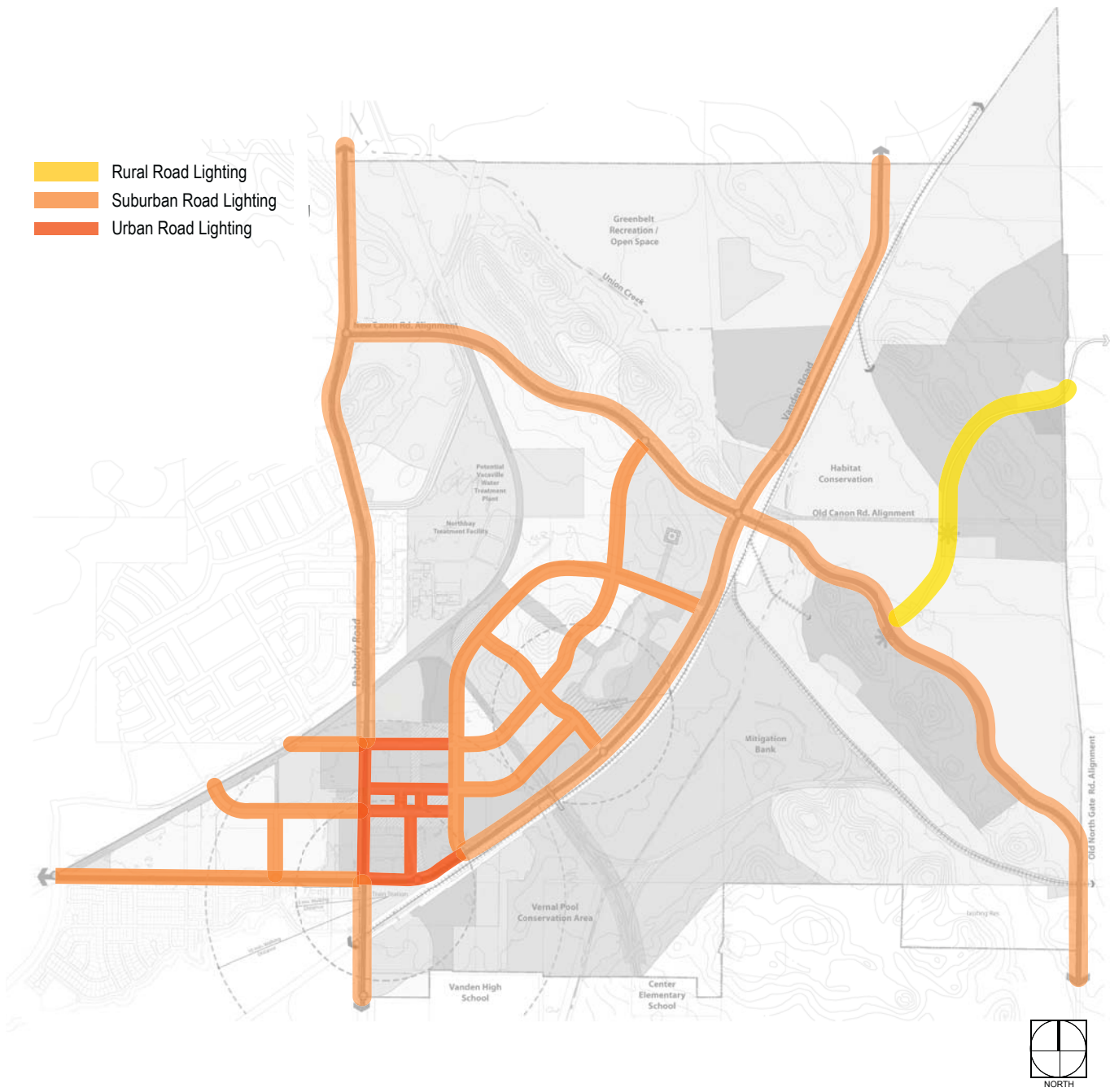
Light fixtures selected for use shall be energy efficient while meeting the minimum light output requirements.

Policy 12-56

Spacing of street lights shall minimize dark spots near access drives, mail-box collection, bus-stops, transit areas, alleyways and crosswalks.

Policy 12-57





1. Provide lighting along all internal streets wherever a pedestrian path sidewalk is present. Lighting design should reflect one of the three lighting themes depending on surrounding context.

Figure 12-13: Lighting Theme Diagram

Street lighting types shall be selected by City which provide the best balance between cost (including initial cost, maintenance cost and life-cycle cost), efficiency in energy use, and effectiveness in lighting.

Policy 12-58

Outdoor security lighting in the industrial areas shall be fully designed to cut off visibility of light sources from surrounding open spaces, public streets and residential areas.

12.18.3 Lighting Design Themes

In accordance with the design goals and principles described in this Specific Plan, FTSSP seeks to establish thematic lighting elements drawn from the agricultural history, including ranching and farming of the area, and urban landscapes similar to those found in nearby urban areas. These themes shall guide lighting design and material selection in order to integrate FTSSP’s public spaces and individual developments into the surrounding landscape in a compatible manner while establishing a coherent and unique place. To that end, 3 roadway lighting themes have been established for the Specific Plan area lighting program to reflect its context and character, as shown in the Figure 12-13.

Policy 12-59

All lighting within the Specific Plan area shall be designed based on one of these themes: Rural Road Lighting, Suburban Road Lighting, or Urban Road Lighting.

Policy 12-60

Lighting should be provided along all streets whenever there is a pedestrian path/sidewalk present and be based on the three following themes depending on location within the Project.

Rural Road Lighting

This lighting most directly reflects the existing rural/agricultural vernacular of the Project’s setting in the County. The lighting along the designated rural/agricultural road settings will maintain the same low-level lighting that can be found on comparable County roads. Lighting design for these roads will emphasize safety and focus lighting at intersections, significant grade changes or sharp curves rather than utilizing specific pole spacing intervals. This less intense lighting scheme will promote the rural ambiance of the county road.

Suburban Road Lighting

This lighting most directly reflects the existing suburban vernacular of the surrounding cities. The suburban road lighting will have lower-level street lighting with an emphasis on safety at intersections. Additional suburban street lighting may be required along specific roads and will be assessed on a street by street basis. The suburban road lighting may have additional ambient lighting from adjacent buildings, walkways



and landscape areas to give greater emphasis on vehicle/pedestrian interaction, as applicable. This increased lighting will provide the safety needed for diverse transportation needs while complying with the city standards.

Urban Road Lighting

This lighting most directly reflects the existing urban vernacular of the Town Center and other commercial/mixed-use areas of the surrounding cities. The lighting layout for the urban roads within the Project will be a more regimented design with light poles at specified intervals to achieve the uniform visibility needed for these more populated areas. Urban road lighting will have additional ambient lighting from adjacent buildings, signs, walkways and landscape areas to greater emphasize vehicle/pedestrian interaction. This increased lighting will provide the safety needed for diverse transportation needs while complying with the City standards.

12.19 Signage

The Purpose of the Signage Design Guidelines is to establish the design requirements and constraints for all primary signage within the Specific Plan area and to ensure that its design is appropriate for its surrounding context, is visually attractive and contributes to the Project's image, character and standards of excellence. The overall Project Design Goals (Section 12.1) and Landscape Themes (Section 12.3) create a broad framework for the character of the Specific Plan area's signage. Signage design is a unifying element that distinguishes the Project and creates a strong and memorable identity both in its overall scale and throughout its districts and neighborhoods.

Successfully integrated community gateway signage and monumentation will immediately inform the visitor that they have entered the Specific Plan area. A seamless program of directional signage will help to orient residents and visitors and identify primary destinations and attractions. Thematic elements and amenities will lend a festive atmosphere to the Project's streetscapes and communicate the Region's historic themes. Commercial signage will add color and life to the Project through the use of materials, graphics and forms that promote tenant identity, character and image.

12.19.1 Signage Design Character

The following principles ensure that Community signage provides efficient, clear and attractive communication to residents of and visitors to the Project. Signage throughout FTSSP should:

- Reinforce a sense of cohesive identity through the project signage that ties the Project together and contributes to residents' sense of civic pride.
- Respect the City and County's historical context, culture and rural/agricultural character.



- Convey relevant information through a range of necessary sign types and community “branded” amenities and express the regional character and a sense of timelessness through the use of material, form, scale, color, typography and lighting.
- Integrate and complement the design character of the surrounding architecture, landscape and overall streetscape.

Four character themes have been established for the Specific Plan area signage program to reflect the Project’s context and character.

Policy 12-61

All signage within the Specific Plan area shall be designed based on one of these themes: Historical/Regional, Rustic Contemporary, Contemporary, or Eclectic. One or more of these themes shall be applied within the Planning Areas as specified below.

Policy 12-62

A master signage program shall be prepared and submitted by the builder during the Master PUD Permit phase for approval by the City.

Historical/Regional

These signs most directly reflect the existing rural/agricultural vernacular of the Specific Plan area’s setting in the County through use of material and form. This theme shall be applied in Planning Areas 4, 5, 7, 8 and 9. The context for this signage theme is either open space or suburban, with the preferred materials including fieldstone, brick, masonry, local quarried stones, treated wood and corten steel.

Rustic Contemporary

Signage in this category maintains its contextual character through its use of material while reinterpreting many of the design elements in a more modern style. This theme shall be applied in Planning Areas 1, 3, 4, 5, 7, 8, 9 and 10. This theme incorporates elements from the Historical/Regional and Contemporary themes and creates a blended style that ties the Project together. Preferred materials include fieldstone, brick, masonry, local quarried stones, treated wood, steel, plastic and glass.

Contemporary

Signage in this category incorporates modern materials, shapes and other design elements while still respecting its surrounding context. This theme shall be applied in Planning Areas 2 and 6 given the urban and industrial context. Preferred materials include masonry, concrete, treated wood, steel, plastic and glass.

Eclectic

This category of signage allows for creatively designed signs that make a positive visual impact on their immediate surroundings. These signs





provide strong graphic character through their more unusual use of form, color, texture, material, scale and proportion. The Eclectic theme shall be applied in Planning Areas 1, 2, 3 and 6, where the land uses include urban, suburban and industrial. Preferred materials include fieldstone, brick, masonry, local quarried stones, treated wood, steel, plastic and glass.

12.20 Fences and Walls

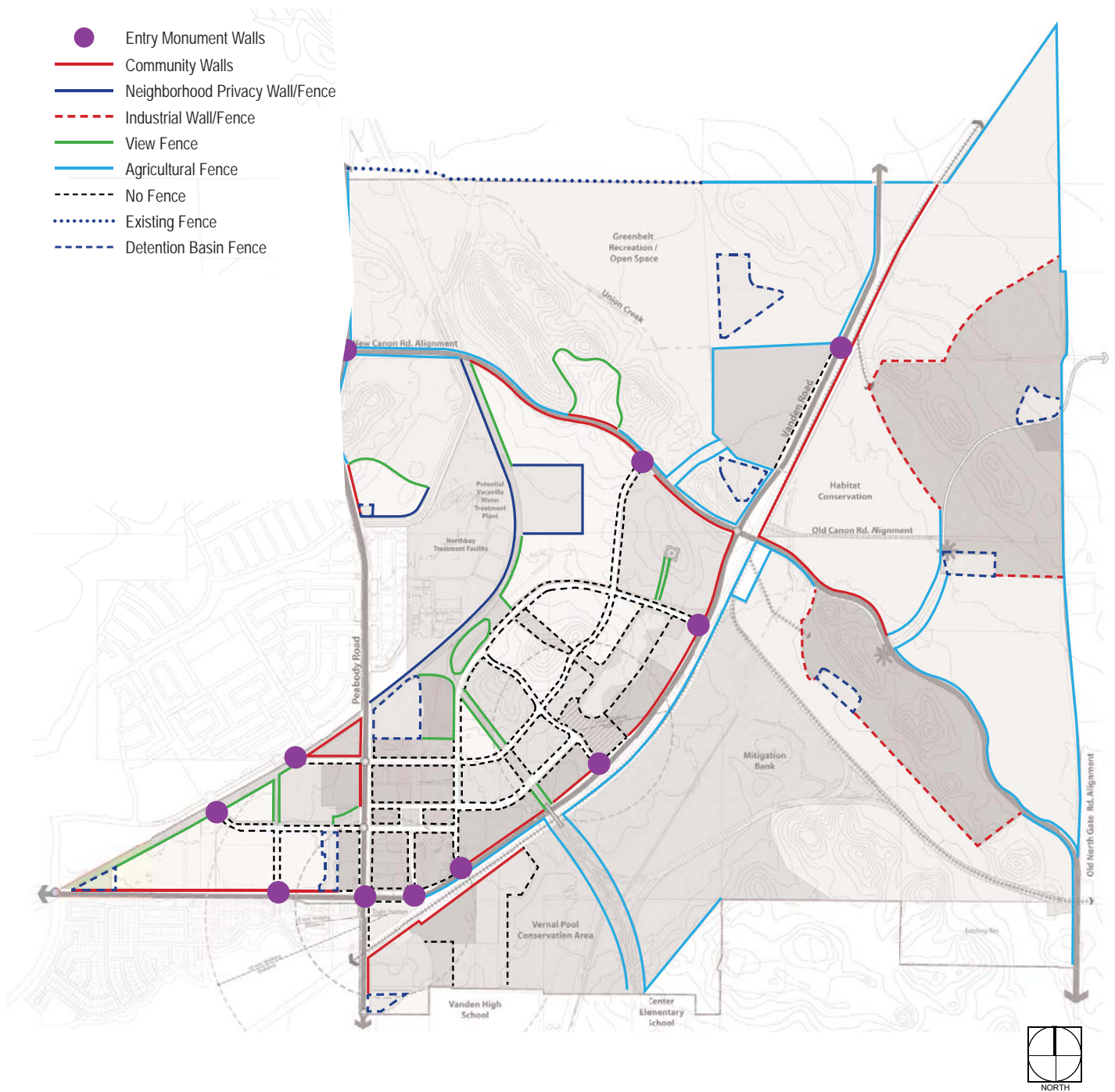
The purpose of the Fence and Wall Guidelines is to ensure that the design of Specific Plan area fences and walls is appropriate to the surrounding context and that they are visually attractive and contribute to the Project's image, character and standards of excellence. The Design Goals discussed in Section 12.1 and Landscape Themes (Section 12.3) create a broad framework for these design guidelines.

The Fence and Wall Design Guidelines are intended to reinforce the principles described in this Specific Plan to create a Project with limited barriers while encouraging small town relationships and dynamics. The Fence and Wall Design Guidelines provide a framework for the judicious use of walls and fences when needed.

12.20.1 Fence and Wall Design Principles

The Fence and Wall Design Principles are intended to guide provision of fences and walls throughout the Specific Plan area. The design principles recognize that materials and manufacturing technologies will change and advance significantly throughout the life of the Project; therefore certain flexibilities are allowed as follows:

- Create a strong sense of community identity for the Project and enhance community building principles for both residential and non-residential land uses;
- Provide a safe and secure environment for residents of and visitors to the Project while allowing access to Project recreational opportunities, amenities and natural spaces;
- Provide a cohesive hierarchy of fences and walls at a scale appropriate for the intended use including, but not limited to, sound attenuation, soil retaining, public safety, privacy, screening and public/private landscape zones;
- Use materials and design vocabularies that embrace the cultural history of the region;
- Encourage and promote pedestrian and bicycle access throughout the Project;
- Seek to enhance the Project's overall quality of life by directing access to recreational opportunities and natural spaces;



1. City may require fencing around all storm water detention basins; the type of fencing will vary depending on the level of landscaping within a basin, nature of abutting land uses and visibility from public streets.

Figure 12-14: Walls and Fences Location Plan



- Provide safety for the bio-resources as well as the residents of and visitors to the Project; and
- Allow and enhance the visual quality and site amenity values of the Project with sensitive design.

12.20.2 Sustainable Fence and Wall Design Goals

The following goals have been established to support, where environmentally responsible and feasible, the provision of a sustainable fence and wall program for the Specific Plan area:

- Seek to utilize then current sustainable construction techniques and materials and, where environmentally responsible and feasible, be adaptable to new technologies as they prove themselves reliable;
- Encourage the use of sustainable materials in the construction of fences and walls including, but not limited to, recycled materials and/or certified “green” products;
- Utilize durable life-long materials in the design and construction of fences and walls that resist vandalism and are easy to maintain;
- Promote principles of sustainability in the design and construction of fences and walls; and
- Recognize and support required drainage strategies during the design and installation of fences and walls.

12.20.3 Fence and Wall Type Criteria

The following wide range of fence and wall types are allowed by the Specific Plan area. Conceptual design standards for all of these are provided as follows, with the specific designs to be determined by City through approval of Master PUD Permits. See Figure 12-14 for locations of walls and fences within the Specific Plan area.

- Entry Monument Walls
- Community Walls
- Neighborhood Privacy Fences/Walls
- Industrial Wall/Fence
- View Fence
- Agricultural Fence

Entry Monument Walls

Policy 12-63

Entry monument walls shall be constructed from concrete masonry units. The various levels of community and neighborhood entries shall be marked by a variation in themed pilasters and wall graphics. These walls shall have the highest level of design and most detail. Specific design of entry monument walls shall be determined by City through

approval of Master PUD Permits. Entry monument walls shall be constructed by developers and privately maintained by HOA.

Policy 12-64

Any walls along connector and arterial streets adjacent to development are required to be masonry walls.

Community Walls

Policy 12-65

Themed community walls shall be constructed from concrete masonry units with concrete caps. Pilasters shall be provided at corners, change of grades and at intervals not more than 150 feet apart. Community walls shall have the secondary level of design and detail. Specific design of community walls shall be determined by City through approval of Master PUD Permits. Community monument walls shall be constructed by developers and privately maintained by HOA.

Neighborhood Privacy Fences/Walls

Policy 12-66

Neighborhood privacy fences/walls are located between residential lots. The level of design and detail shall be secondary to that of the entry monument and community walls. Walls and pilasters shall be constructed from concrete masonry units and include concrete caps. Privacy fences shall be constructed from either douglas fir, redwood or cedar. All wood fences shall be weather-protected with themed color exterior paint. Enhanced wood fences are required on corner lots within builder tracts. Specific design of neighborhood privacy fences/walls shall be determined by City through approval of Master PUD Permits.

Industrial Wall/Fence

Policy 12-67

Industrial walls used to screen outdoor storage or loading areas from view from public streets shall be constructed from concrete masonry units. Pilaster shall be provided at corners, change of grades and at intervals not more than 150 feet apart. Concrete caps shall be installed on the pilaster and walls. Industrial fencing along side and rear property lines not abutting a public street may be constructed from steel chain link fence material, or similar fencing of equivalent quality. The maximum height of the industrial walls and fences shall be 12 feet per city standard. Industrial fences in the front yards shall be constructed from steel square tubing material with a maximum height per City standards. All walls and fences shall be privately maintained. Further design refinements to be determined through approval of the Master PUD Permit by City.

View Fence





Policy 12-68

View fences shall be constructed from steel square tubing material. Picket spacing shall be specified per city code. Support posts shall be spaced per structural engineer’s recommendation. View fence shall have a maximum height of 6 feet from the finish grade. All view fences shall be privately maintained. Further design refinements to be determined in the Master PUD Permit phase of work and approval.

Agricultural Fence

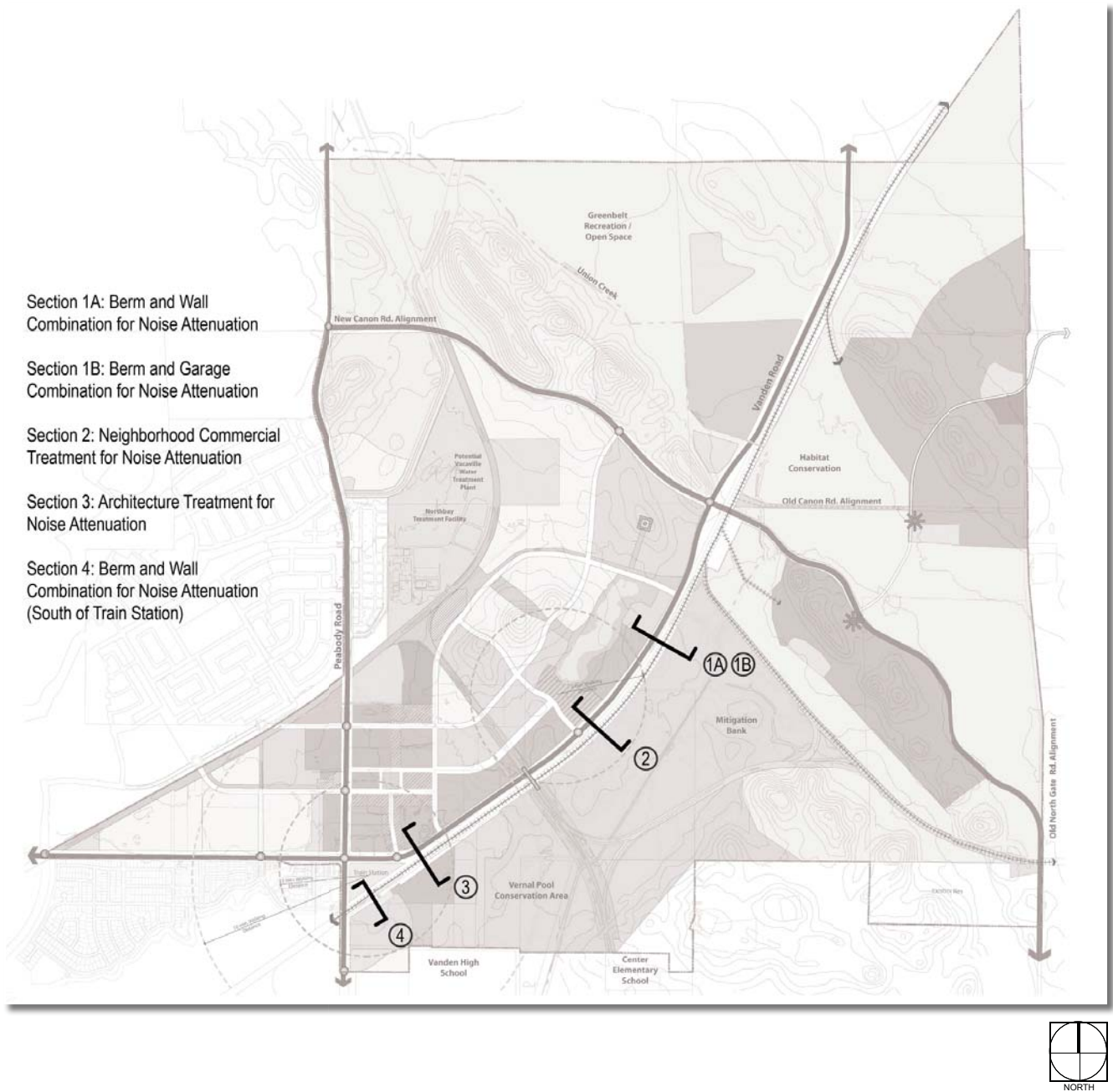


Figure 12-15: Noise Attenuation Sections Key Map

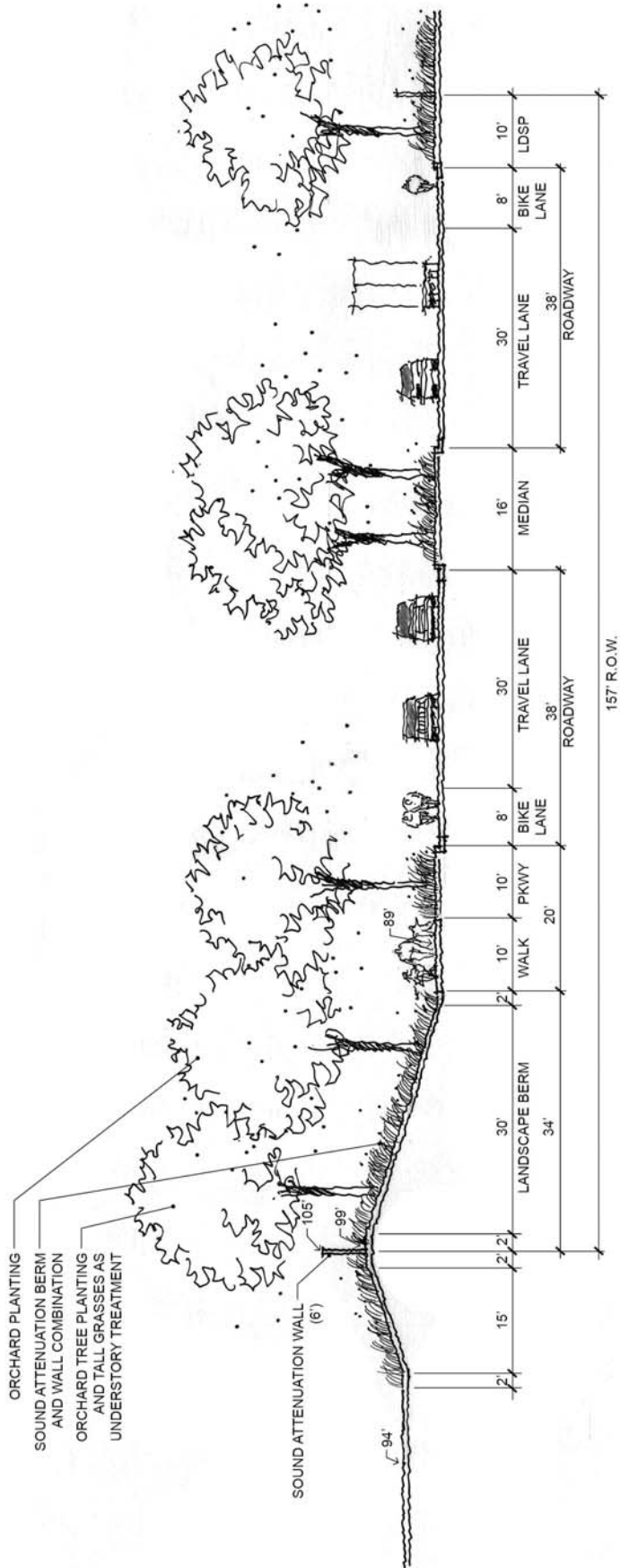


Figure 12-16: Section 1A - Berm and Wall Combination for Noise Attenuation

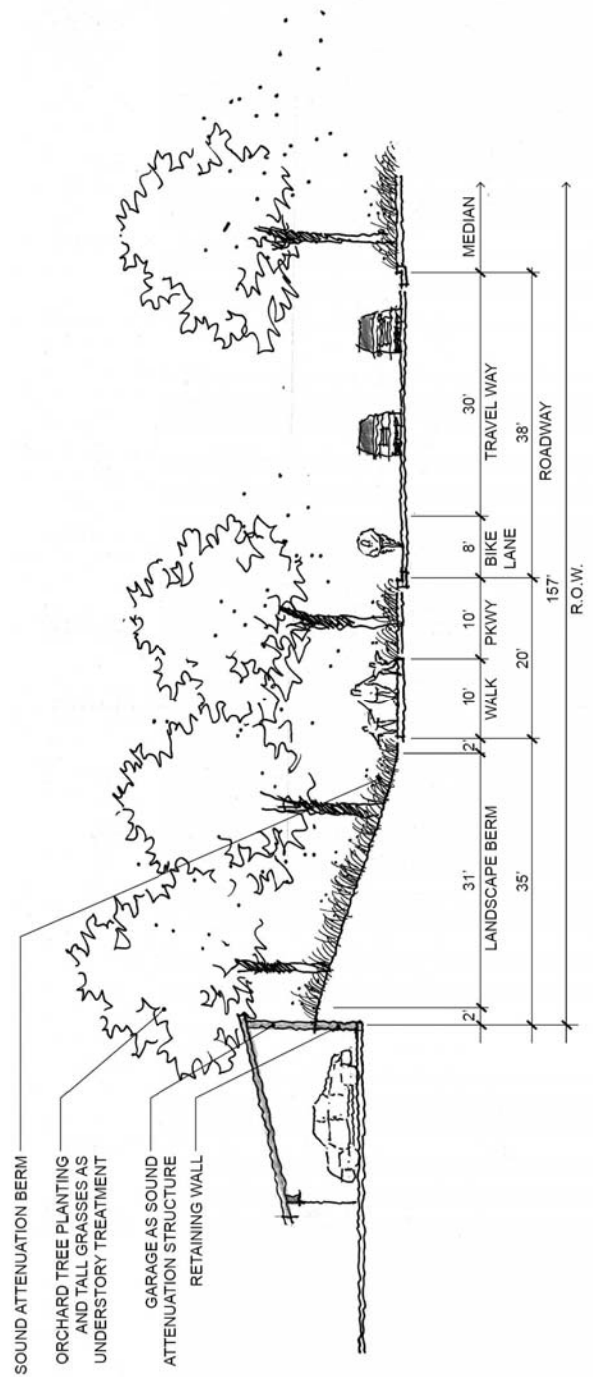


Figure 12-17: Section 1-B - Berm and Garage Combination for Noise Attenuation

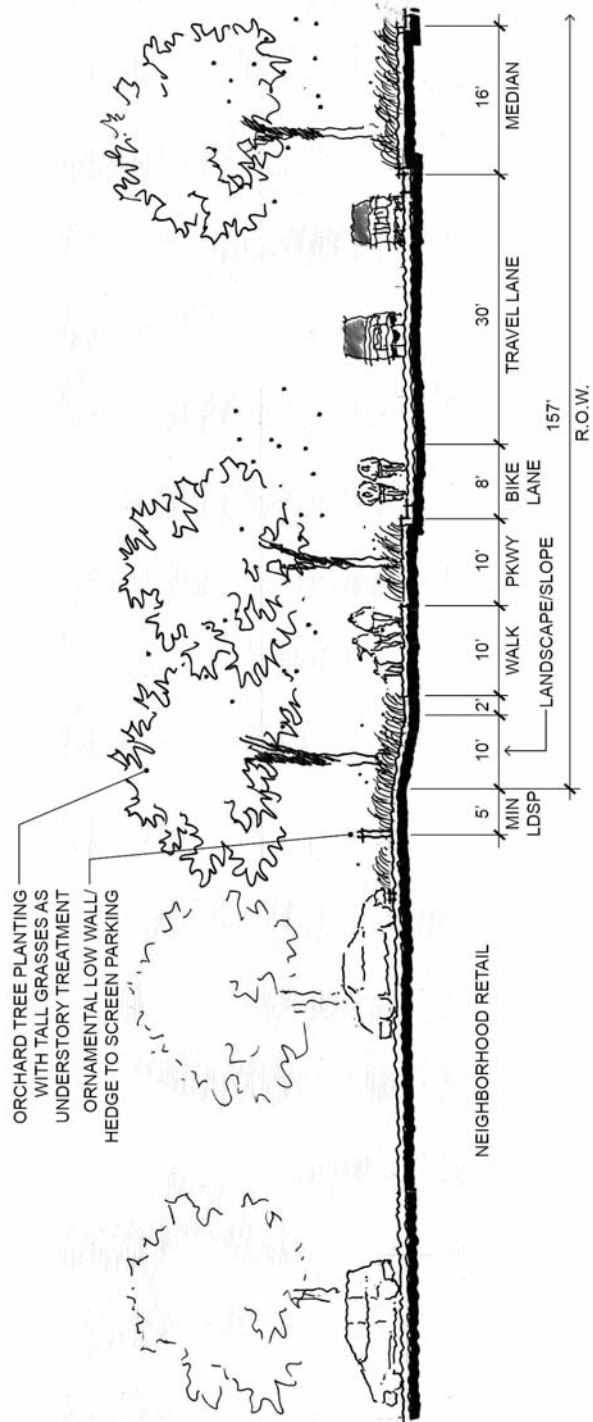


Figure 12-18: Section 2 - Neighborhood Commercial Treatment for Noise Attenuation

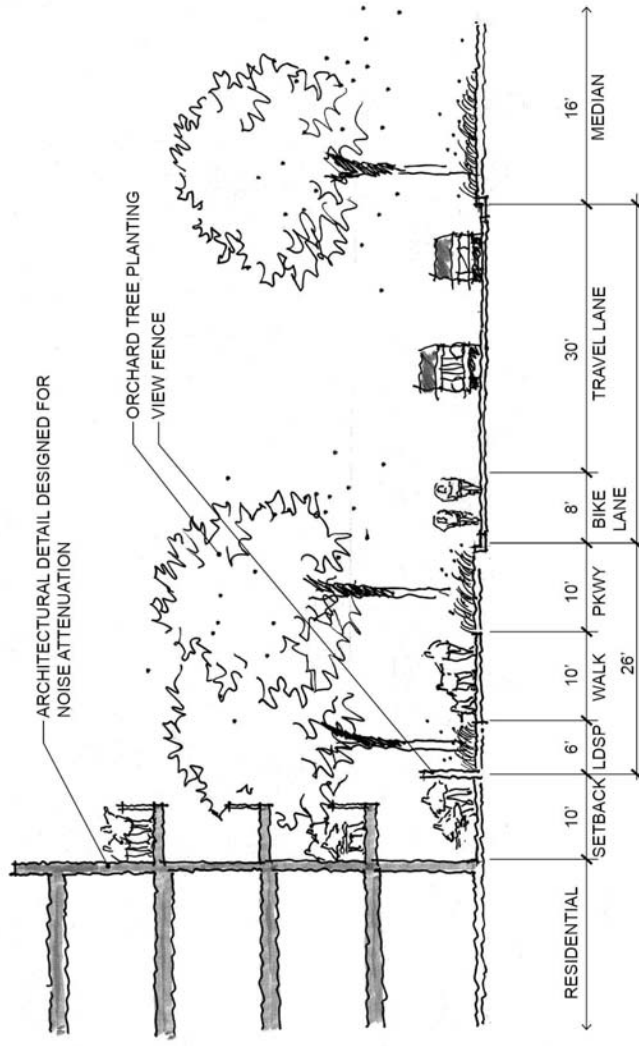


Figure 12-19: Section 3 - Architectural Treatment for Noise Attenuation

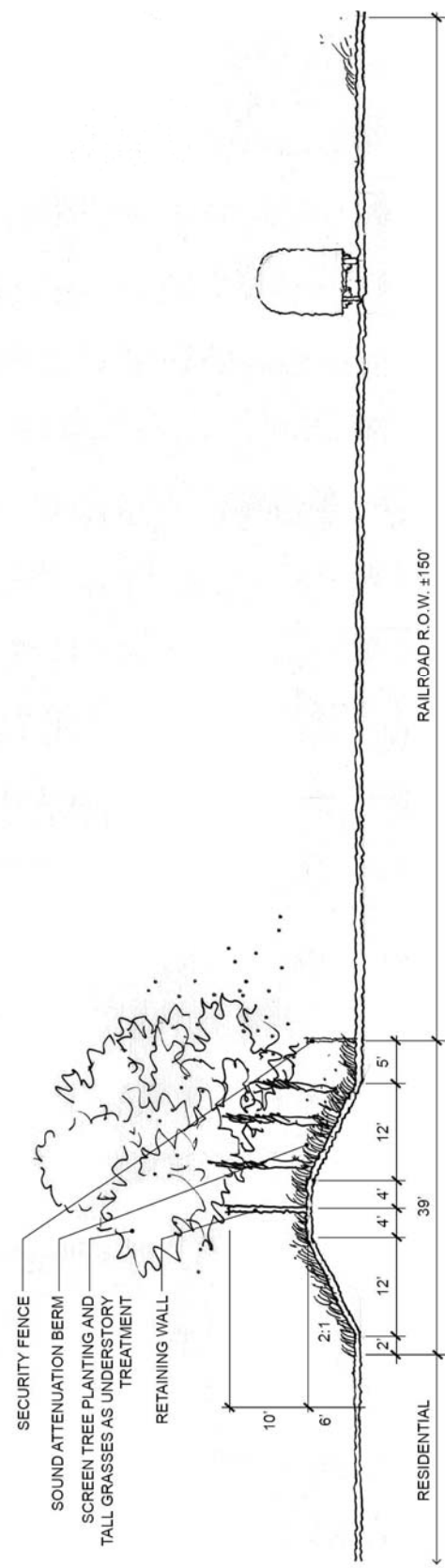


Figure 12-20: Section 4 - Berm and Wall Combination for Noise Attenuation (South of Train Station)



Policy 12-69

Materials and agricultural fences shall be installed per city standards and privately maintained.

12.21 Landscape Design Policies

Wall and Fencing Policies

Policy 12-70

A Master Wall and Fence Plan shall be submitted with each Master PUD Permit for the City's review and approval. The Master Wall and Fence Plan shall identify:

- Location of all walls and fences which:
 - abut schools, parks and open space,
 - abut public streets,
 - where required for noise mitigation, or
 - other situations where walls and fences are proposed for screening or security
- Height and materials of all walls and fences
- Wall and fencing within a planning area shall have a high level of design consistency

Policy 12-71

The Master Wall and Fence Plan approved with the initial Master PUD Permit within the Specific Plan area shall be used as a reference for City's subsequent Master Wall and Fence Plan approval.

Policy 12-72

There shall be an overall consistency of design, color and materials for walls and fences throughout the Specific Plan area, in order to provide a strong sense of community identify. However, City may allow some variation in fencing and walls among the planning areas.

Lighting Policies

Policy 12-73

A Master Exterior Lighting Plan shall be submitted with each Master PUD Permit for the City's review and approval. The Master Exterior Lighting Plan shall include:

- Design of light fixtures within the public rights-of-way for all street classifications, all public parks within the Master PUD Permit and any private recreational or common areas. Design shall include materials, colors, heights, and design styles.
- Lighting shall be designed to minimize light spillage onto adjacent properties, consistent with the need for public safety

Policy 12-74

The Master Exterior Lighting Plan approved with the initial Master PUD Permit within the Specific Plan area shall be used as a reference for City's subsequent Master Exterior Lighting Plan approvals.

Policy 12-75

There shall be an overall consistency of lighting design throughout the Specific Plan area, in order to provide a strong sense of community identify. City may allow some variation in lighting design among the planning areas. However, there shall be a consistency of light fixture design along streets or trail corridors that extend into multiple planning areas. For example, the lighting along Jepson Parkway (Vanden Road and Manuel Campos Parkway) shall have a consistent and uniform design.

Signage Policies

Policy 12-76

A master sign program shall be submitted for both the: (a) non-residential portion of the Town Center District, and (b) non-residential portion of the Lake District. Each master sign program shall be submitted concurrently with the initial Design Review application for each District.

Gateway Policies

Policy 12-77

Three gateway features shall be constructed as part of the Specific Plan. The locations of these gateways are: (a) Peabody and Vanden Road intersection, (b) Peabody and New Canon Road intersection, and (c) Vanden Road at the Great Park.

Policy 12-78

The design of each of the gateways shall be substantially in accord with the designs shown in Section 12.7. While each gateway will differ, there shall be a continuity and consistency in design among the three gateways that identifies FTSSP as a distinct community.

Policy 12-79

The design for the gateway at the Peabody/Vanden intersection shall be approved concurrently with the improvement plans for Peabody Road and Vanden Road, as part of the Train Station project and Jepson Parkway project respectively.

Policy 12-80

The design for the gateway at the Peabody and New Canon Road intersection and the gateway for Vanden Road at the Great Park shall be submitted with the Master PUD Permit for PA5.

Policy 12-81





The designs for major and minor neighborhood entries shall be substantially in accord with the designs shown in Section 12.7

Streetscape Policies

Policy 12-82

A Master Streetscape Plan shall be submitted with each Master PUD Permit. The Master Streetscape Plan shall identify:

- Types of trees and plants within public rights-of-way and landscape corridors
- Type of irrigation system
- Spacing and arrangement of plantings
- Size of plants
- Design and materials for site furniture and placement criteria

The Master Streetscape Plan shall be substantially in accord with the standards and recommendations of Chapter 12 of this Specific Plan.

Policy 12-83

The Master Streetscape Plan approved with the initial Master PUD Permit within the Specific Plan area shall be used as a reference for City's subsequent Master Wall and Fence Plan approval.

Policy 12-84

There shall be an overall consistency of landscape design throughout the Specific Plan area, to order to provide a strong sense of community identify. City may allow some variation in landscape design among the planning areas. However, there shall be a strong consistency of landscape design along streets or trail corridors that extend into multiple planning areas. For example, the landscape plan along Jepson Parkway (Vanden Road and Manuel Campos Parkway) shall have a consistent and uniform design.



13.0 DESIGN GUIDELINES



13.0 DESIGN GUIDELINES

July 26, 2011



13.1 Purpose and Intent

The purpose and intent of the Design Guidelines is to define and guide the architectural character of development for the Fairfield Train Station Community and to ensure that new neighborhoods and districts within the project area achieve the vision and standards of excellence expected by the City of Fairfield and its residents.

The Guiding Principles discussed in Chapter 1.0 of this document provide direction for the Design Guidelines which are intended to find a balance between establishing a minimum acceptable threshold for quality and character while allowing individual project design professionals considerable freedom of expression and creativity. Also the guidelines must not be so restrictive as to preclude development due to economic feasibility, especially given the probable stressed economic backdrop during early development phases.

It is the intent of the guidelines to clearly define mandatory design requirements while also identifying recommended components and concepts that would further enhance the visual quality of the community, but are not mandatory. A well written set of guidelines allows the project developer and design team clarity of direction so time and resources will not be wasted in unclear and vague direction.

13.2 Design Integrity

Great communities have a “timeless sense of place” created by comfortable human scale, visual charm, nurturing landscape and well proportioned spaces formed by appropriately positioned and articulated architecture.

The charm and inherent beauty of a great community is the result of good urban design, meticulous crafting of open space, and care taken in architectural execution. The concept of “neighborhood crafting” within the Fairfield Train Station Community is based on the desire to achieve much of the intimacy, charm and timelessness best represented during



the "Golden Era of Town Building" which occurred between the Civil War and World War II. During this period neighbors new each other, kids were safe on the street, parks and open spaces were close, streets were shaded with towering trees and lined with porches and a variety of home designs. There was also a vibrant small down town with Main Streets, squares and stately civic buildings within walking distance of most residents.

The design character and architectural vocabularies of this era are no longer replicable due to the natural evolution of our society and its building systems. However the Fairfield Train Station Community should be respectful of and take it's lead from the historical context of Solano County by crafting the building form, massing and architecture so that the spirit and of these earlier times are expressed in a walkable and visually interesting pedestrian scale and streetscape. The design of building elevations will be required to consider the composition of the adjacent street scene. Public and quasi-public outdoor spaces will be utilized as important design elements that complement the architectural style and vary the interaction of the buildings with the pedestrian pathways and streets.

Sufficient landscape space, themes, palettes and quantity will be required to ensure a balance of the natural and man-made within the community. Landscape design will be based on land use/density context, open space function and sustainability including sensitivity to responsible water use.

In summary, neighborhood and building design within the community should be more than an execution of minimal industry conventions. It should create streetscapes that encourage walking and outdoor activity, lead to a retention and growth of property values envied by others, and create post card quality places that are the pride of the city; carefully achieved within the market and economic context of the region.

13.3 Design Review Process

A Design Review Process will be implemented by the City of Fairfield to ensure individual development projects achieve the intent of the specific plan guiding principles and the design guidelines that follow. The city design review includes, but is not limited to, the following elements:

- Community design elements
- Neighborhood crafting
- Individual site plans
- Building architecture
- Civil engineering, drainage and retaining walls
- Landscape design

Prior to submission to the City for design review, within some portions of the community a preliminary design review at a master developer level may be required. This depends on location of parcels within the community.

The implementation Section 14.0 of the Specific Plan further describes the design review process in greater detail.

13.4 Residential Guidelines

13.4.1 Residential Design Principles

The purpose of this section is to provide specific planning and design guidance for the residential neighborhoods of Fairfield Train Station Community. The following design principles shall be used to guide the development:

- Organize residential neighborhoods around cores and parks
 - Design district cores as center of activity and people gathering
 - Utilize Parks as neighborhood organizing elements
- Establish a range of neighborhood types
- Enhance connectivity and encourage a multi-modal network
- Create great streets
- Maximize view corridors and orientation
- Provide a range of recreation activities

Subdivisions and homes will be crafted to create more of a “small town” and less of a mass produced feel. In addition, the residential neighborhoods will be part of an integrated sustainability program to conserve resources such as energy and water and minimize greenhouse gases.

Organize Residential Neighborhoods around Cores and Parks

The Fairfield Train Station Community will be organized into Districts defined by key elements such as the Train Station, Town Square, or the Lake Park. Districts will include multiple neighborhoods of varied types organized around a structured urban design pattern and on overall transition from higher density neighborhoods closer to the Town Center and lower density neighborhoods radiating outward.

Design District Cores as Centers of Activity and People Gathering

The Town Center and Lake Districts are organized around District Cores which serve as anchors for the residential neighborhoods that surround them. The District Cores will be planned and programmed to establish District character and be key identifiable elements for the Districts. The District Cores will provide central gathering places for



the residents to interact and connect and will take on a different focus and scale and provide a variety of functions and programs for the residents of its neighborhoods and the Project as a whole. Components of a District Core may include one or more elements including neighborhood level retail and services, a village club, public and civic facilities or parks and recreation spaces. The establishment of District Cores allows for more walkable, compact districts with social connectedness and the opportunity for a more urbane lifestyle.

Parks as Neighborhood Organizing Elements

Parks are important “place making” elements that establish community, district and neighborhood identity. A hierarchy of parks including The Great Park, the Lake Park, neighborhood parks, Linear Park Trail, pockets parks and school parks together with the Greenbelt open space will enhance opportunities for different levels of social and recreational functions. They will be strategically placed such that most homes will be within walking distance of one or more of these placemaking elements. The park and open space system will be interconnected with the Town Center and the District Cores to allow residents to walk or bicycle rather than drive to these amenities, further reducing vehicle trips within the Project.

Enhance Connectivity and a Multi-Modal Network

A well connected community encourages residents to use multiple modes of transportation in the course of their daily activities thereby reducing the number and length of vehicle trips, minimizing greenhouse gas impacts and conserving energy. Alternative modes of transportation are of particular importance to those who do not drive; especially the elderly, disabled and the very young. Higher levels of connectivity will be accomplished by linking project amenities like the Town Center, Lake Park, and the Great Park to residential neighborhoods within the Project through a network of sidewalks, off-street multi-use pathways, pedestrian pass-throughs and transit routes. Trail connections are also proposed to connect key offsite pedestrian destinations like Center Elementary school to the Project.

Create Great Streets

Attractive, safe and walkable streets should be promoted throughout the Project. Street pattern and character may vary to reflect the surrounding land uses and development intensity. In general, urban streets in the Town Center should be animated by active architecture with a diversity of activities, urban streetscapes, squares and plazas. Streets in the residential neighborhoods should have sidewalks separated from the curbs with street trees in the landscape parkway. Street trees will help establish the unique character of the street and minimize the heat island effect. Other elements such as front porches, courtyards, second story balconies, thoughtful garage placement,



refinements in architectural style and authenticity, architectural massing and roof design also will contribute to creating great streets.

Maximize View Corridors and Orientation

Views enhance the quality of everyday life and therefore are an important component of the economic and social value of the Site. They can establish a direct relationship between the built environment and nature defining a sense of orientation and identity for the Project. Unique, picturesque elements of the Site include the Lake and the Lake Park, the Greenbelt and the other smaller parks. Whenever possible, these scenic views should be considered as underlying criteria for orienting the layout of the components of the Project. Furthermore, when possible, buildings should be placed mindful of passive and active solar orientation.

Provide a Range of Community Amenities

Providing a wide range of amenities is important to promote social and recreational opportunities for the residents of the FTSSP Community. In addition to primary facilities such as parks, schools, and open space elements; other amenities such as a village club, recreation centers, specialty parks like the Lake Park and the Great Park, passive people gathering spaces like the Town Square and Village Square and active open space elements will be strategically distributed to further reinforce a finer grain network of amenities. These amenities will be important places for residents to connect with one another and will enhance the social infrastructure of the FTSSP Community.

Establish a Range of Neighborhood Types

To distinguish itself from conventional subdivision tract developments, the FTSSP Community employs traditional town planning principles which acknowledge historic towns in the region consisted of multiple neighborhood types. In general; these neighborhood types radiate from a formal urban character in the center to a more relaxed natural character at the towns rural edges. Fairfield Train Station Community has identified four (4) neighborhood types: Urban, Lake, Park, and Classic Romantic. Each neighborhood type is described below and locations are depicted in Figure 13-1, Neighborhood Type Location Map. Criteria for each neighborhood type is shown in Table 13-1.



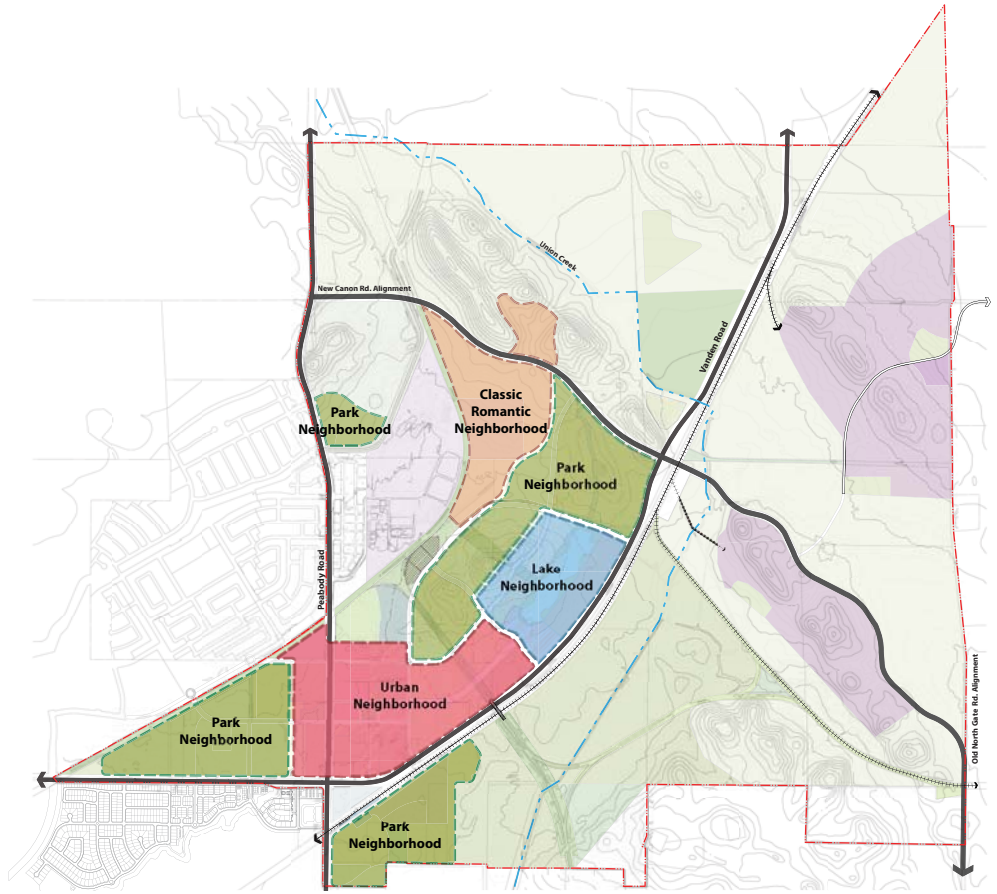


Figure 13-1: Neighborhood Type Location Map



Figure 13-2: Urban Neighborhood Conceptual Plan

Urban Neighborhoods

Urban Neighborhoods are located within or around the Town Center and consist of higher density and mixed-use housing types. Predominant housing types include apartments, condominiums, townhouses and other attached products such as duplex and triplex units. Residents are within easy walking distance of services, shopping, community activities and recreational facilities. A conceptual plan is shown in Figure 13-2, Urban Neighborhood Conceptual Plan.

Primary Character Elements:

- The highest density neighborhoods are typically located nearest to the Town Center allowing the greatest number of residents to have close proximity to community facilities and the Train Station
- The street and sidewalk systems are well connected in a traditional grid-like pattern
- Buildings are typically taller with a higher density
- The landscape character is more formal and urban reflecting the urban environment and proximity to the Town Center
- Open space is primarily organized around plazas and/or squares which are often framed by buildings
- The street framework within Urban neighborhoods is a tight, grid pattern with shorter block lengths to promote connectivity
- The street scene within this neighborhood tends towards tighter, more uniform building setbacks. Tree grates are used along key streets, such as Main street and E Street.
- Specific criteria for Thematic Streets in the Urban Neighborhoods are described in Chapter 5.0, Town Center District.





Figure 13-3: Lake Neighborhood Conceptual Plan

Lake Neighborhoods

Lake Neighborhoods are located around the lake and consist of a variety of housing densities. Housing types include apartments, condominiums, townhouses and other attached products such as duplex and triplex units as well as detached single-family homes on smaller lots. An attempt will be made to maximize the views of the units to the lake as well as the layout of the neighborhoods will enhance the connectivity to the lake park. Residents in Lake Neighborhoods will be within easy walking distance of local services, community activities and recreational facilities located in the District Core. A conceptual plan is shown in Figure 13-3, Lake Neighborhood Conceptual Plan.

Primary Character Elements:

- Concentration of densities around the cores, allowing a great number of residents to be within walking distance of community facilities, services and amenities.
- The residential layouts are designed to maximize views of the lake.
- The streets and sidewalks are well connected in bent-grid pattern.
- Building setbacks are tight along the streets.
- The landscape character is more traditional with street trees in the parkways and curb separated pedestrian walkways.
- The Linear Park Trail is designed to maximize the connectivity to the Lake Park.
- Specific criteria for Thematic Streets in the Lake Neighborhoods are described in Chapter 6.0, Lake District.





Figure 13-4: Park Neighborhood Conceptual Plan

Park Neighborhoods

The Park Neighborhoods constitute a large portion of the project and are a transition between the Urban Neighborhoods and the Classic Romantic Neighborhoods. They provide a supportive setting for traditional family living. A hierarchy of parks and recreational facilities, linked by a well connected street network and off street pedestrian Linear Park Trail, allow residents to easily and safely traverse between their homes and the parks, recreation facilities and open space. Park Neighborhoods include a mix of medium-high, medium and low density single family detached homes. A conceptual plan is shown in Figure 13-4, Park Neighborhood Conceptual Plan.

Primary Character Elements:

- A mix of medium-high, medium and low density housing types
- The streets and sidewalks are a connected network generally in a bent-grid pattern
- Careful neighborhood crafting, especially around neighborhood parks, icon streets and special landscape elements to achieve strong place-making
- The building setbacks on streets for the low density housing types are variable to enhance diversity except for homes fronting on special place-making elements such as neighborhood parks and icon boulevards where uniform building setbacks may be appropriate
- The architectural styles are diverse, providing the sense of a town built over time by a wide array of builders
- Landscape character is traditional with street trees in the parkways and curb separated pedestrian walkways.



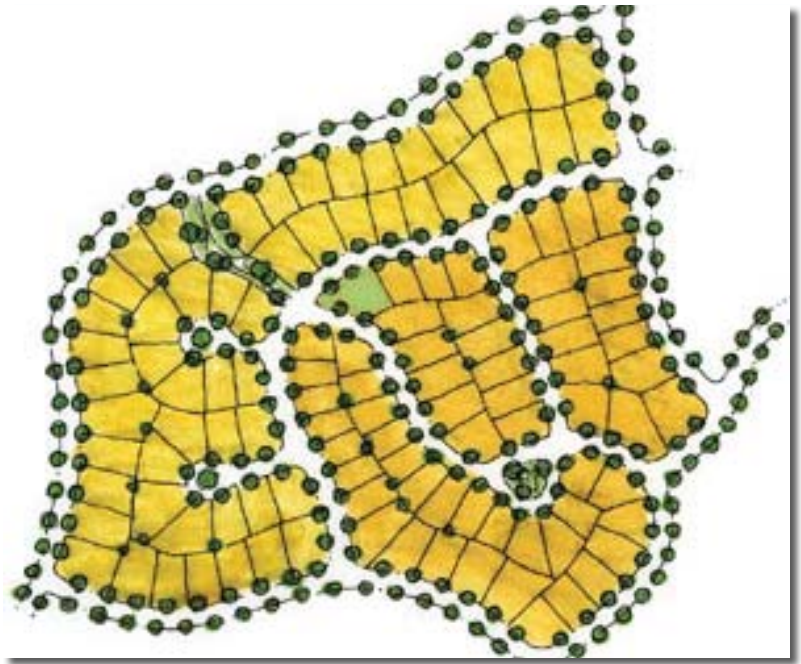


Figure 13-5: Classic Romantic Neighborhood Conceptual Plan



Classic Romantic Neighborhoods

The Classic Romantic Neighborhoods are characterized by a relaxed yet connected curvilinear street system organized around central open space features. They include medium to low density single family detached homes with strategically located neighborhood parks. A conceptual plan is shown in Figure 13-5, Classic Romantic Neighborhood Conceptual Plan.

Primary Character Elements:

- A mix of medium to low density housing types
- The street pattern is interconnected and curvilinear and may include variable building setbacks to create views and interest in the street scene
- The architectural style is diverse with an emphasis on styles appropriate to this neighborhood type
- The streetscape character is more informal with curb-separated pedestrian walkways

	Urban Neighborhood	Park Neighborhoods	Lake Neighborhood	Classic Romantic Neighborhood
Typical Block Length	400 Feet	600 Feet	600 Feet	800 Feet
Cul de Sacs	Not Permitted	Permitted	Discouraged	Permitted
Street Network Character	Grid Like	Curvilinear	Bent Grid	Curvilinear
Architectural Massing/ Character	Continuous Building Street Wall	Elevations Dominated By Porches and Stoops	Elevations Dominated By Porches and Stoops	Informal Massing
Pocket Park Average Size	6,000 sf (60' min width)	33,000 sf (0.75 Ac)	N/A	4,360 sf (0.1 Ac)
Typ. Pocket Park Frequency	1 per 160 Du	1 per 120 Du	N/A	1 per 60 Du

Table 13-1: Neighborhood Type Criteria

- Street patterns are typically curvilinear and informal in character with larger block lengths

Residential Neighborhood Crafting

Neighborhood crafting describes the level of design and planning details required to enhance the “small town” feel for the residential neighborhoods in the Fairfield Train Station Community. The neighborhoods that result are crafted to meet the aspirations of the residents related to their living environment. This shift in character, compared to typical subdivision neighborhoods, will distinguish the Fairfield Train Station Community and provide “added value” to builders and homeowners alike.

The creation of a more “small town” and less “mass produced” feel will be achieved through a mixture of smaller neighborhoods with a greater diversity of style, smoother transition between housing types and more thoughtful neighborhood landscape. The result will be a community that is more walkable, more attractive, feels safer, encourages interaction and that matures with elegance and visual richness.

Neighborhood Crafting Objectives

- Respond to Consumer Values and Preferences:
- Create Small Town character
- Design for an appearance and character of older, traditional neighborhoods
- Plan for diversity in housing character with a less production feel
- Create spaces and opportunities for neighbors to socialize
- Promote opportunities for community involvement
- Site smaller, accessible amenities versus larger amenities that are further away
- Create healthy living environments

Neighborhood Crafting Criteria

The following design elements allow neighborhoods to be crafted consistent with the Neighborhood Crafting objectives.

Home Builder Parcels

Integration of a variety of home builder parcel sizes allows a community to move away from the typical monotonous subdivisions of exclusively large single builder tracts. At Fairfield Train Station Community, builder parcel sizes may include a large grain, medium grain and fine grain mix. Neighborhood crafting principles as described in this section of the Specific Plan will apply to builder parcels of all sizes.



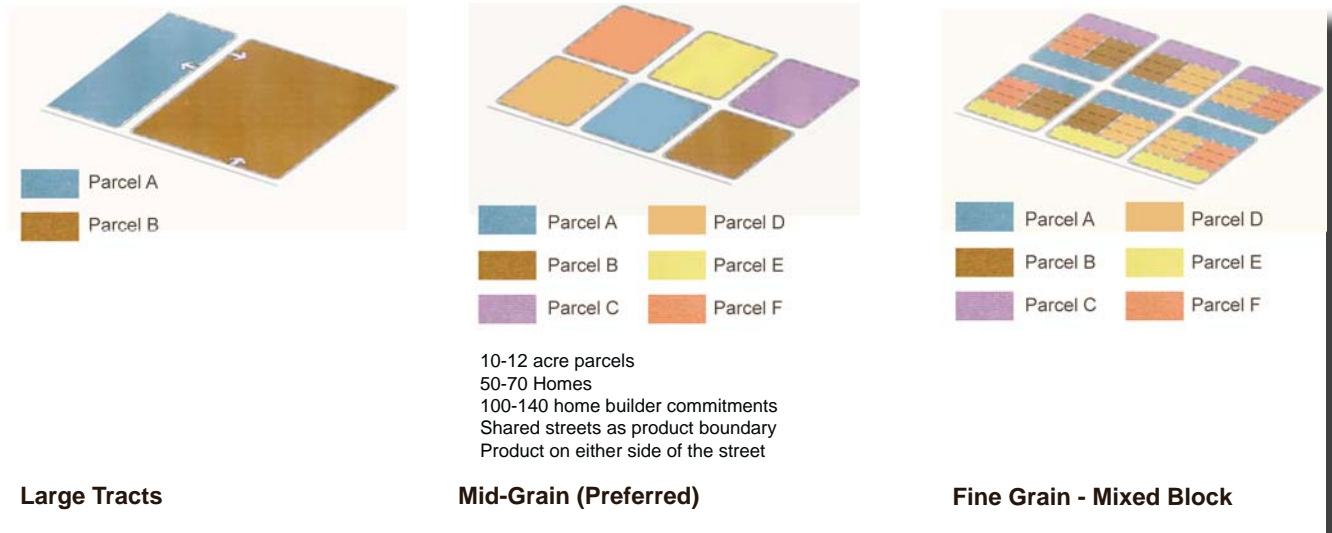


Figure 13-6: Prototypical Builder Parcel Sizes

Builder Parcels Criteria

Policy 13-1

Maximum single project size shall not exceed:

- 70 dwelling units for Single Family Detached products
- 150 dwelling units for Single Family Attached, Townhome and Condominium product
- 300 dwelling units for Multi-Family Apartments

Within a builder product line in exceeds the maximum builder parcel size, the builder may develop separate parcels that may be catty-corner to each other. The parcels may have one model home complex per product line.

Sensitive Edges

Neighborhood edges along collector roads and open space are often dealt with by backing lots up to the roads and lining them with tall privacy walls and fences. A more open and friendly community feel is achieved, if instead, subdivisions are designed with side-on conditions, front-on orientations without direct curb cuts, exposed parks and other devises that at minimize continuous wall length.

The character of elevations exposed to public view is a vital element to the overall integrity of Fairfield Train Station Community and require design sensitivity to create elevations, silhouettes and massing that reflect the quality of the Project.

Legend

-  Arterial Street Sensitive Edge
-  Connector Street Sensitive Edge
-  Open Space Sensitive Edge

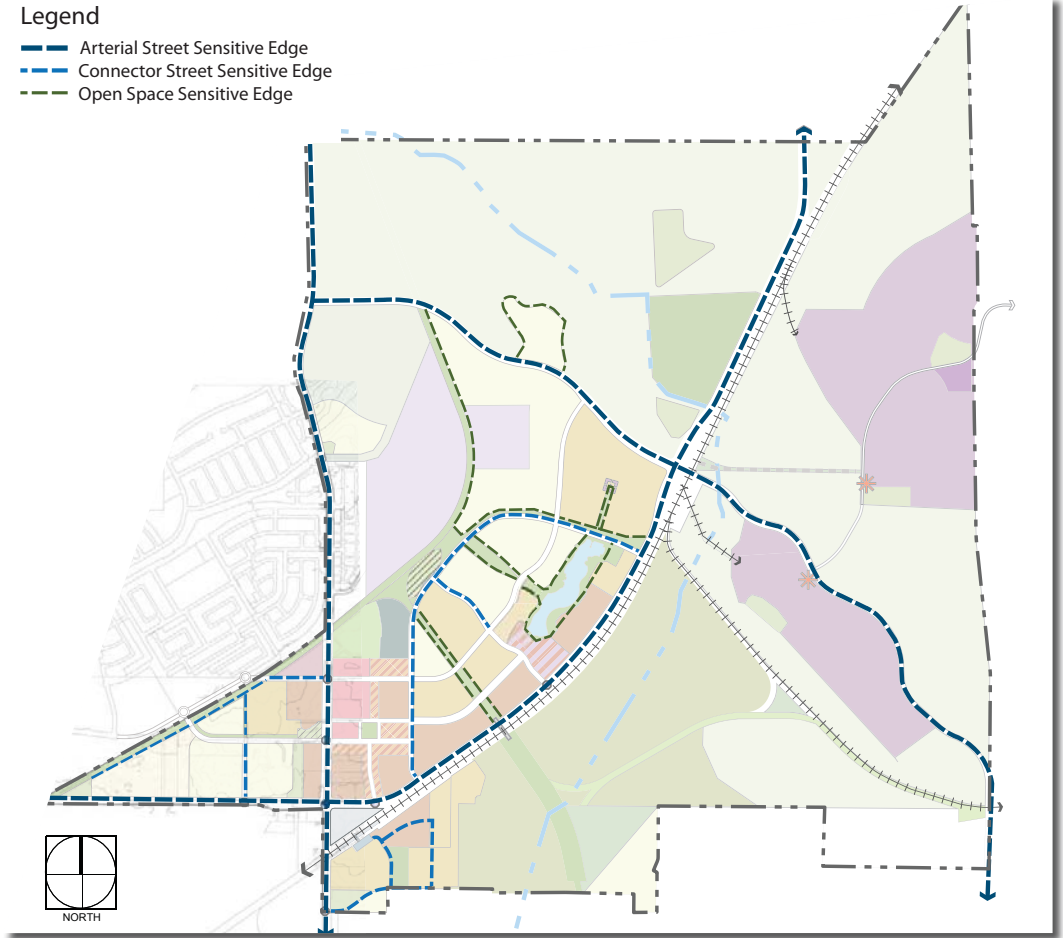
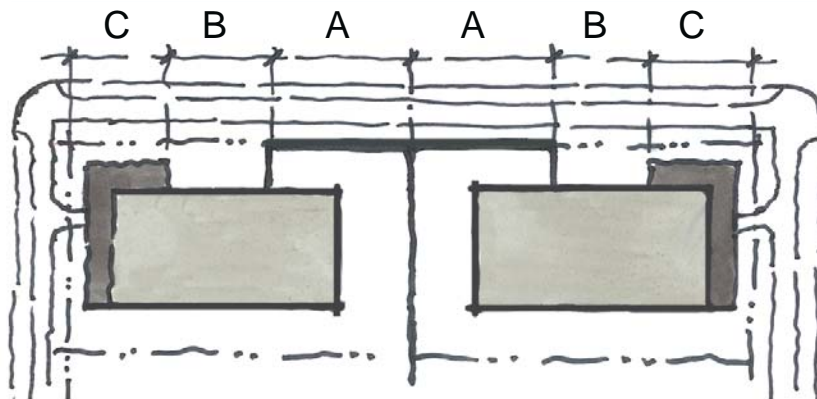


Figure 13-7: Sensitive Edges Diagram



Legend

- Condition A - 6' solid wall allowed. No greater than 40% of the length of the lot
- Condition B - 3' solid wall with view fence allowed. Maximum combined height of 6'
- Condition C - Maximum 3' wall allowed

Figure 13-8: Sensitive Edge Wall Diagram



Sensitive Edge Criteria

Policy 13-2

On all sensitive edges as identified on exhibit 13-7, architecture directly adjacent shall utilize at least two of the following enhancements:

- A balance of hip and gable roof forms
- 2 feet minimum offset massing (on individual plans or between plans)
- 18" minimum roof plane breaks (on individual plans or between plans)
- Special window treatment or fully trimmed windows
- Single-story elements on two-story homes
- Detail elements similar to the front elevation

Policy 13-3

Roof forms shall vary between plans with single side-to-side gable ends being limited.

Policy 13-4

On connector streets designated as a sensitive edge, a maximum of 300 feet may be continuous wall without breaks. Breaks may include open cul-de-sacs, single loaded streets, residential frontage (alley loaded), parks or open space.

Policy 13-5

Along arterials designated as sensitive edge, rear and side elevations visible from arterial travel ways shall have architectural articulation including full window trim, accent materials and color blocking, if appropriate.

Policy 13-6

For sensitive edges along public accessible open space, such as greenbelts, public parks and the Linear Park Trail, a maximum of 150 feet of continuous wall is allowed without breaks as defined above. The more articulated facade with entry shall orient to the open space.

Policy 13-7

For sensitive edges along public accessible open space, cul-de-sacs and single loaded streets are preferred. If rear loaded condition is required, than a maximum of 150 feet may be continuous wall without breaks. Breaks may include open cul-de-sacs, single loaded streets, residential frontage (alley loaded), parks or open space.

Thematic Streets

Thematic Streets play a part in creating civic space and pulling a neighborhood together. The goal is to orient living activity towards the street, emphasizing architectural detail and including interactive architecture including porches, courtyards, entries, windows and second-story balconies. The Thematic Street sections outlined below shall be used to determine building setbacks, sidewalk, parkway and building frontage. This requirement is in place to eliminate curb cuts unless necessary.



Figure 13-9: Thematic Streets Diagram

Thematic Street Criteria

Policy 13-8

Along Thematic Streets, no less than 80% frontage is required. For all other streets, 60% building frontage is required.

Policy 13-9

Alley-loaded product is required along Thematic Streets; conventional front-loaded lots of 70 feet or wider are permissible along all Thematic Streets, where permitted by land use, except on 'A Street' where direct driveway access is strongly discouraged.

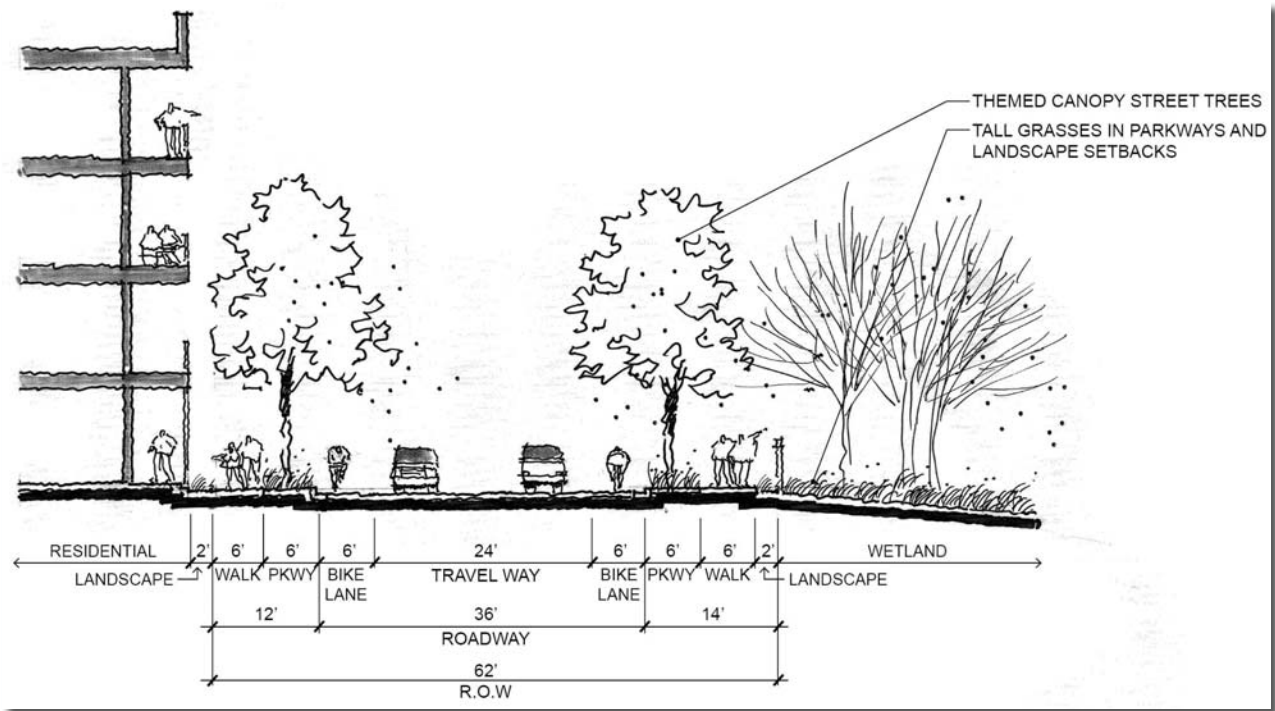


Figure 13-10: D Street West

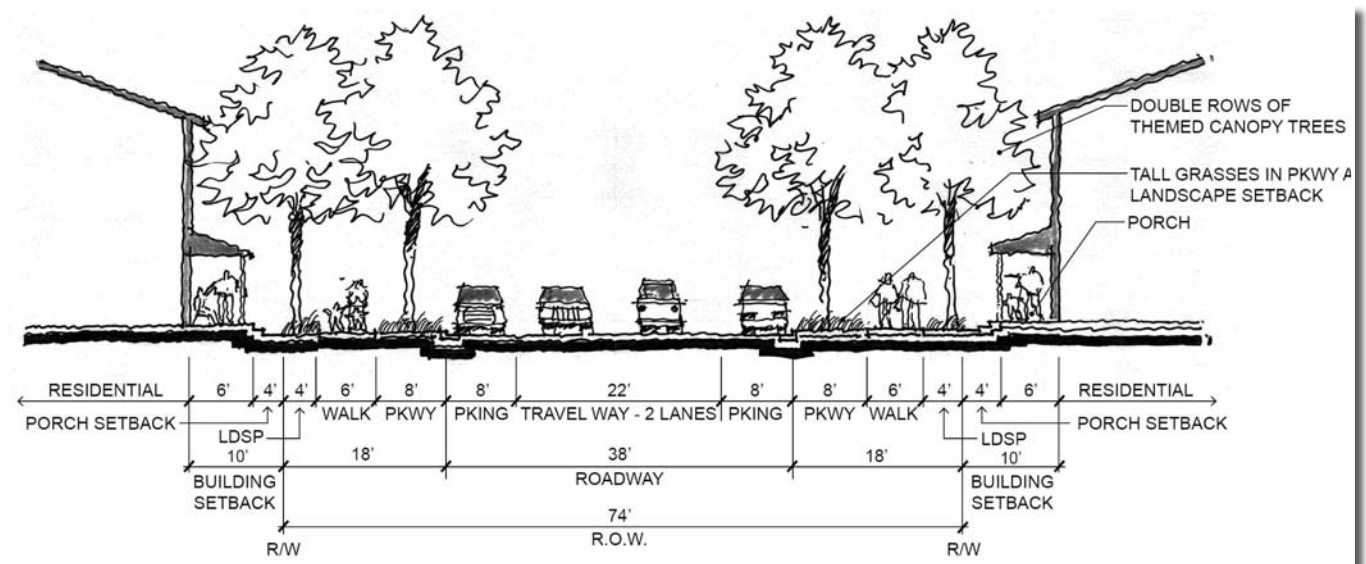


Figure 13-11: A Street

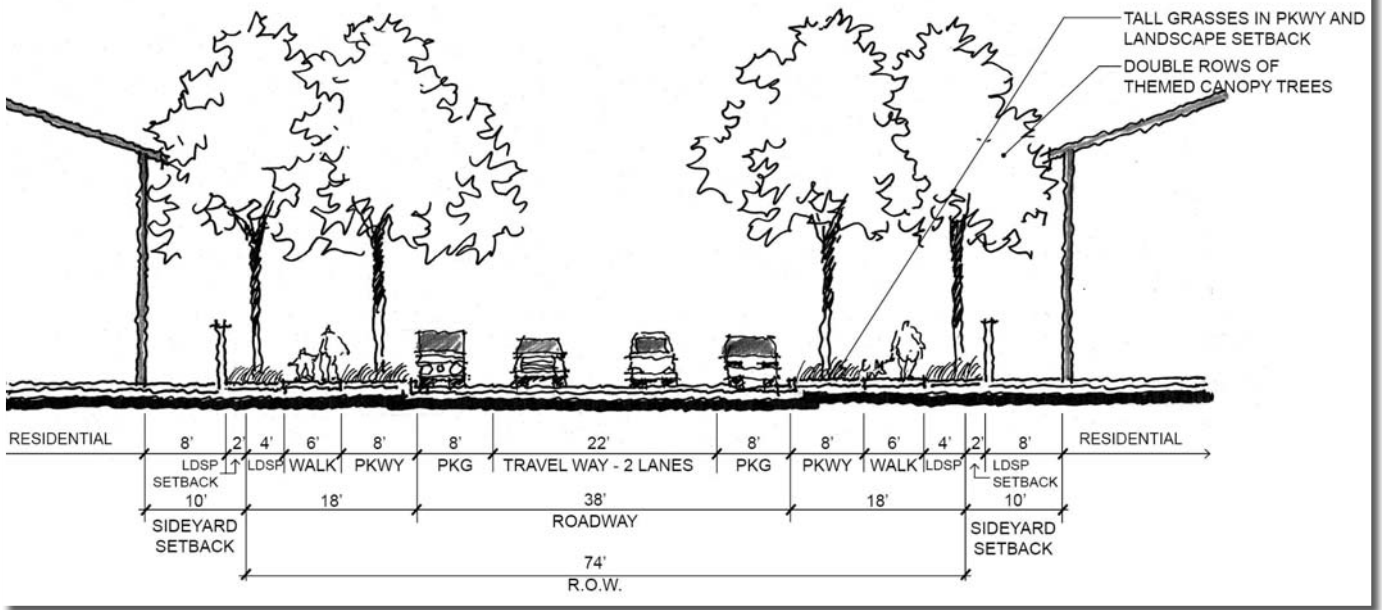


Figure 13-12: A Street (Side-On Condition)

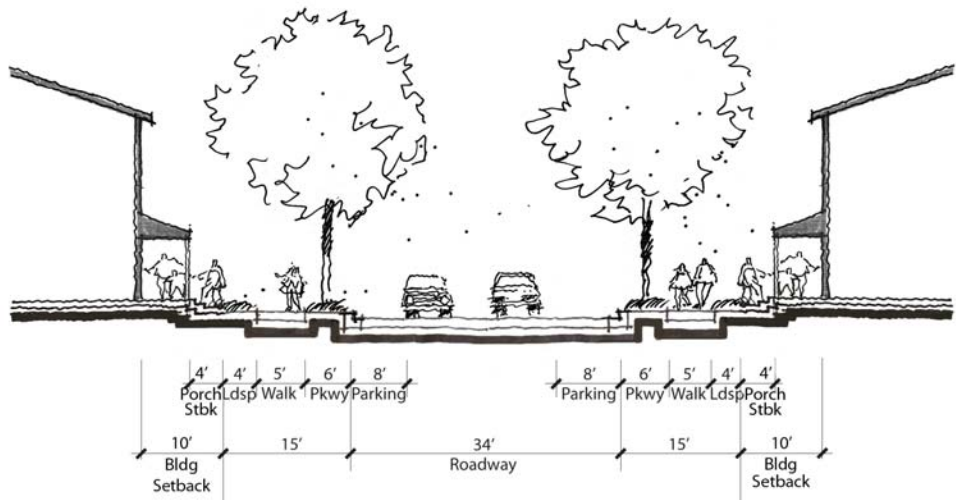


Figure 13-13: Icon Street

Variable Front Yard Setbacks

Variations of front yard setback create a more attractive and engaging streetscene. Different massing solutions or staggering front yard setbacks help achieve this goal.

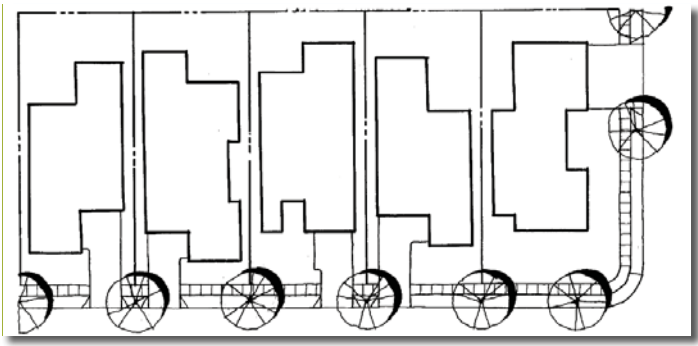


Figure 13-14 Variable Front Yard Setbacks

Variable Front Setback Criteria

Policy 13-10

At least 20 percent of the detached lots shall have an extra 5 feet front yard setback. These homes are not required to have one story elements.

Policy 13-11

Exceptions to Policy 13-12 include homes in Urban Neighborhoods and those that front on a square, a neighborhood park or an Icon Street where uniformity in building setbacks is preferred.

Variable Lot Widths

Variable lot widths provide a more interesting street scene and efficient use of the land. Therefore; variable lot widths within an individual product line is encouraged. This allows large units to be plotted on wider lots and smaller units on narrower lots.

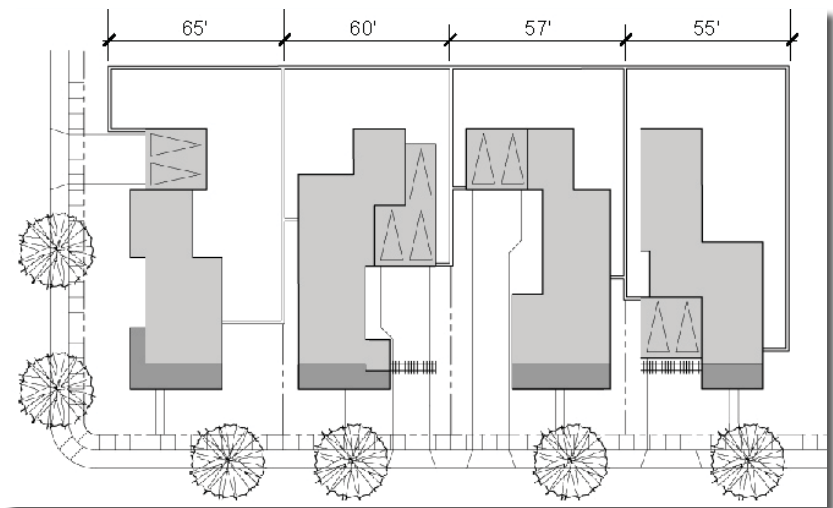


Figure 13-15 Variable Lot Widths

Accessory Structures

Accessory structures should conform to the design standards, setbacks and height requirements of the primary structure. If visible from the front or side lot line, the visible elevation should be considered a front elevation and should meet the design criteria of the applicable architectural style.

Corner Lots

Houses on corner lots shall be designed for a two-sided corner exposure. These lots provide the opportunity to embellish the house elevations and possibly add square footage.



Corner Lots Criteria

Policy 13-12

Corner lots shall be a minimum of 10 feet wider than standard lots. Five feet shall be dedicated for a one story element and 5 feet shall be provided for additional side yard setback.

Alley

Alley-accessed housing allows more house, porch and front door exposure to the street by relocating the driveway and garage towards the back of the lot. Alleys may be designed to accommodate either one-way or two-way traffic. While the primary function of an alley is to serve garage access and trash collection, it will be experienced daily by neighborhood residents and should include appropriate landscaping and design solutions to adequately screen trash and utility locations to avoid a “utilitarian” character. Lighting wall packs are allowed in alleys.

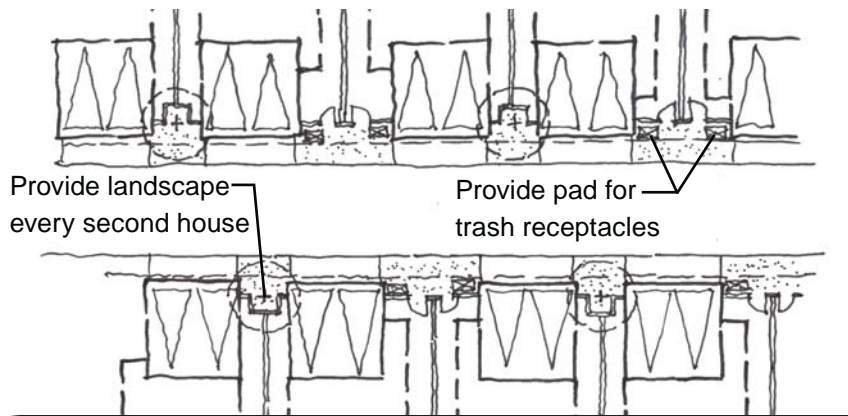


Figure 13-16 Alley Plan

Alley Criteria

Policy 13-13

Land pockets of at least 5'x5', which would include groundcover and a large shrub, is required every second house; products 26 du/ac. or greater shall be excluded from this policy. Designated areas are required for garbage cans for all product.

Policy 13-14

Alleys terminating, but not connecting to public street shall have landscape or wall barrier.

Policy 13-15

Along alleys, at least 1/3 of the units or length of the building shall have a 2 foot massing offset such as the 2nd story stepped back 2 feet.

Policy 13-16

Alleys shall not exceed 400 feet in length.

Residential Architectural Guidelines

The purpose of these architectural guidelines is to provide design direction to home builders in the Fairfield Train Station Community to help elevate the quality of the living environments through thoughtful architectural design that demonstrates individuality, a high quality of design and construction and a value-added appeal. Overall compatibility throughout the project will be achieved by combining engaging architectural design with innovative site planning techniques and a community-wide landscape program. These guidelines are not intended to be overly restrictive or limiting, but to help achieve neighborhoods with a higher level of living quality.

Design Character

The architectural design character of the neighborhoods will be one of hometown appeal characterized by simple, yet charming homes, curb separated sidewalks and parkways lined with street trees. Street scenes will reflect an overall feeling of quality and tradition. These guidelines do not prescribe a set palette of architectural styles but rather provide an overall framework within which builders can choose styles as long as they are reflective of the overall regional context of Fairfield.

Architectural Design Criteria

The following architectural criteria have been created to help develop architecture that reflects quality in design, simplicity in form and plan and contributes charm and appeal to the neighborhoods in the Fairfield Train Station Community.

Architectural Style

Although this Specific Plan does not dictate architectural styles, the following architectural style criteria shall be used.

Architectural Style Criteria

Policy 13-17

In the Master PUD Permit stage of approval, a minimum of four (4) styles for each detached product line and two (2) styles for attached product shall be defined with for each architectural style, (6) key differentiated features shall be identified. Each plan shall have at least four (4) of these features. These differentiating features may include: roof form and pitch, building massing, building materials, window shape and type, and other architectural details.



Single-Family Residential

Authenticity

The design criteria in these guidelines is offered to prevent “false front” architecture. Although detail elements may be used to further convey the character of a style, the overall massing and appropriate roof forms should be used to establish a recognizable style. Proper scale and proportion of architectural elements, technical definition of style, materials and appropriate choice of details are all factors in achieving authenticity. Roof form and pitch need to match the architectural style of the home.

Architectural Massing

Creating street scenes that function well and have visual interest are primary community objectives. The following basic elements and criteria are intended to develop variations in appearance and a sense of individuality for each home. Neighborhoods that have nearly identical homes and streets without variation in product placement and form will not be allowed.



Architectural Massing Criteria

Policy 13-18

A minimum of 30 percent of all homes shall have at least two significant front facing one story element of at least four feet in depth.

Policy 13-19

At least three floor plans within a product line shall incorporate substantially varied front elevation massing utilizing two of the following:

- a. Front porches
- b. Increased setback with two story straight-up massing (min. 5 feet clear)
- c. Swing in garages
- d. Recessed garages
- e. Front elevation balconies
- f. Breaks in vertical and horizontal planes of 2 feet minimum

Policy 13-20

A minimum of 50 percent of the single family residential plans shall have a front porch of at least five feet clear in depth.

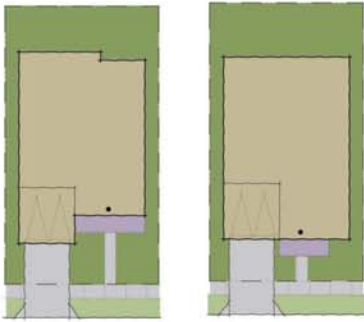
Garage Placement and Treatment

The impact of repetitive, street front garages can be reduced by using visual character diversity of architecture. The focus of the aesthetics of front elevation of the house should be on the living spaces of the home rather than the garage. Appropriate treatment of garage doors will further enhance the elevation and decrease the utilitarian appearance of the garage. Various garage door patterns, window and/or color schemes may be included as appropriate to individual architectural styles.

Vary garage door placement and treatment

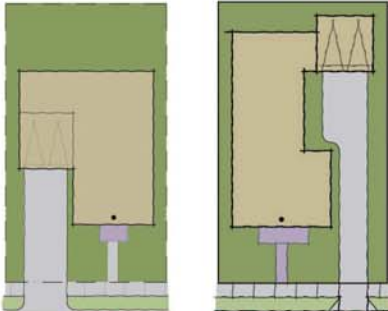
Introducing variation in the placement of the garage helps to create a dynamic streetscene experience. The different garage placement approaches that can be used include:

- Forward Garage – The garage plane extends beyond the front living space wall plane is not allowed.
- Flush Garage – The garage plane is in line with the front living space wall plane. A porch, courtyard or second story projection should extend forward of the garage plane.
- Recessed Garage – The garage plane is set back behind the front living space wall plane.
- Deep-Recessed Garage – The garage plane is set back far behind the front living space wall plane towards the rear side of the lot.
- Swing-In Garage – The garage plane faces the side lot line. The street-facing elevation of the garage should be articulated with the same level of detail as the front façade of the home.
- Side Entry Garage – Typically on a corner lot, the front entry of the building faces one street and the garage faces the other street.
- Motor Court – Consolidation of multiple garages facing an interior court accessed by a single driveway. Dimensions of the motor court should be appropriate to accommodate the number of homes served and allow sufficient space for manoeuvring.
- Alley-Loaded Garage – Garage accessed from an alley at the rear of the lot allowing for greater density and greater presence of living space of the street.



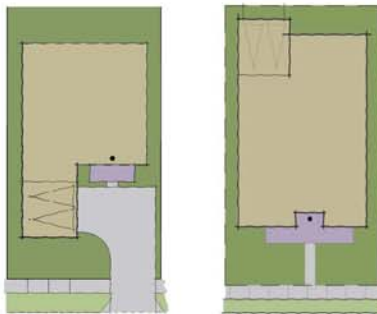
Forward Garage

Flush Garage



Recessed Garage

Deep-recessed Garage

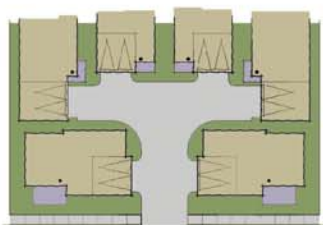


Swing-in Garage

Alley-loaded Garage



Side-entry Garage



Motor Court

Garage Placement and Treatment Criteria

Policy 13-21

No homes shall have a forward garage condition except swing-in garages.

Policy 13-22

No more than 25 percent of the homes on a street shall have a flush garage condition; a porch is required at least 5 feet deep clear.

Policy 13-23

All other plans shall have shallow garage setbacks of minimum 5 feet or a mid recessed or deep recessed garage setbacks.

Policy 13-24

All garages shall have a 1 feet setback between building face and the garage door.

Policy 13-25

For RL development, garages for homes proposed on lots less than 50 feet in width shall not exceed 60 percent of the front elevation for the home and shall be recessed a minimum of five feet from the habitable portion of the home, not including the front porch.

Policy 13-26

Single car and tandem garages are permissible.



Three car garage placement

When a three car garage is planned, an attempt should be made to reduce the impact on the streetscape.

The following optional treatments are highly encouraged to reduce or buffer the direct view of garages and garage doors from the sidewalk or street.

- **Porte Cochere**
 - A deep-recessed garage plan with a porte cochere provides an additional, partially covered, parking space and also serves as an outdoor private space
- **Garage Door Screening Elements**
 - Install elements such as attached trellis' beneath single story garage roof fascias and/or above garage door header trims.
 - Span the driveway forward of the garage with a gated element or an over-head trellis.



Three Car Garage Criteria

Policy 13-27

- Offset the single door at least 1 feet in distance and plane from the double door.
- Decor on garage doors shall be consistent with the architectural style of the home.
- Three-car garages opening directly to a street shall not occur on lots less than 55 feet in width. Side entry garages or tandem garages are exempt.

Corner Homes

Neighborhood quality is enhanced by adding home plans designed specifically for the corner of a street or open space, or by enhancing an interior lot plan for use on corners with additional architectural elements and/or details found on the front elevation.

Plans should have the ability to be easily altered for use on interior lots. Corner homes should include porches or expanded single story living spaces that wraps around or occupies the corner side yard.

The following elements should also be considered for use:

- A corner related front entry door placement
- A garage placement that allows entry from the side street or alley

Corner Home Criteria

Policy 13-28

All residential corner lots shall have the following:

- Primary and secondary field materials from the front elevation wrapped along the side facade. All material changes must occur on an inside corner or a logical point.
- Full window and other trim along the secondary frontage

Policy 13-29

At least one of the following shall be provided:

- Single story element on secondary frontage at least 2 feet in depth
- Porch extending around the corner at least 10 feet in length from inside corner of porch

Policy 13-30

At least two elevation alternatives per product line.

Policy 13-31

At least two plans per neighborhood shall be designed for wrapped porches on corner lots.

Policy 13-32

On corner lots, side yard walls adjacent to a street or open space shall not extend beyond 40% on the length of the building. This side yard wall shall be set back 2 feet when adjacent to a local street.



Materials and Color

The colors and materials used in the Fairfield Train Station Community will reflect a general theme of environmental harmony with the surrounding topography and neighborhood character. A variety of natural looking materials and colors will provide the diversity required for visual interest while unifying the homes with their settings and creating a timeless appeal. The primary purpose of the architectural color palette selection is to avoid monotony, provide a variety of color schemes and promote visual diversity.

Materials and Color Criteria

Policy 13-33

No more than 60 percent of the total number of front elevations shall be "stucco dominant" (more than 50% of the surface area) within a single builder product line.

Policy 13-34

Each elevation shall have a minimum of 3 colors. For example, 1 body color, 1 trim color and 1 accent color. A shift in materials qualifies as a change in color.

Policy 13-35

No two adjacent single family homes shall have the same color scheme.

Variable Setbacks on Individual Elevations

Where authentic to style variable setbacks shall be provided for different parts of the home to encourage vertical and horizontal massing breaks. Alternative garage locations from shallow to deep as well as the use of alternate plan configurations within each neighborhood shall help in providing visual variation.

Rear Elevation Articulation: Vertical & Horizontal Plane Brakes

The second story portion of the rear elevation of all homes which back onto roads, parks, trails, public open space, must include a variety of window treatments, roof projections, etc. to provide variety on the rear elevation. Where these conditions exist, a variety of hipped and gabled roof forms must be used in each neighborhood. One story homes or one story elements should be used to help provide this variety.

Attached & Multifamily Homes

The following general concepts should be considered when planning for and designing attached and multifamily housing.

- Design and site buildings with a strong physical relationship to public areas of the community.



- Emphasize pedestrian access and connections to public sidewalks, trails and open space systems when preparing site plans.

Building Massing

The tendency to “build out” to the maximum building envelope without articulated treatment on wall planes or one and two story elements should be avoided. This results in a multi-story box, without vertical or horizontal relief. Consideration should be given to the authenticity of intended styles while developing floor plans, massing and roof forms.

- Minimize building “ends” and bland, singular planes oriented toward public views. Provide breaks in plane and/or other massing on each side of a building.
- Consider intended styles in conjunction with the development of building plans, massing forms, elements, details and color.
- Carefully consider the building massing, details, and color, in developing an appropriate architectural character for the project.

Attached Building Massing Criteria

Policy 13-36

Provide breaks of 2 feet minimum in plane or other massing elements on each side of a building every 25 feet maximum.

Policy 13-37

For all attached product except apartments, 15% of the roof area shall be one level of massing lower than the overall height of the building. For instance, three story buildings shall have at least 15% of two story elements.

Attached Building Type- Design Criteria

The design criteria outlined in this section regarding the plotting, massing, site plans and garage placement should be used to help develop the plans for the different attached multifamily building types:

Townhomes

Plotting

- Project a front door image and access to the street front. Articulation of end unit elevations is required.

Massing

- Provide front porches for a minimum of 50% of the units, where style-appropriate
- Vary setbacks and roof pitches so townhomes read as individual dwelling



Garages

- Provide rear accessed garages for all townhomes on Thematic Streets

Townhome Criteria

Policy 13-38

At least 2 building forms shall be demonstrated for each project. Building form shall vary in roof form, massing, building length and style.

Policy 13-39

At least three different, yet compatible color schemes for each neighborhood shall be provided.

Policy 13-40

Vary garage treatments, overhangs and recesses to create visual interest and variety along the alley.

Auto Court Clusters

Plotting

- Project a front door image and access to the street front or auto court depending on building layout
- Place/access garages from front or side in auto courts

Massing

- Provide porches or balconies at front, side or rear where style-appropriate and possible for stepped massing and transition to public spaces or two story facades.

Garages

- Provide front or side accessed garages for auto court plans.
- Tandem garages may be used to accommodate covered parking requirements.
- Vary plans, Placing garages and different locations relative to the auto court. Avoid a straight lineup of garages doors adjacent to one another.

Green Court Cluster

Plotting

- Project a front door image and access to the green court space.

Massing

- Provide porches or covered entries for stepped massing and transition to public spaces





Garages

- Provide rear accessed garages for alley loaded green court units
- Tandem garages may be used to accommodate covered parking requirements
- Vary plans, placing garages at different locations relative to the alley

Auto Court & Green Court Cluster Criteria

Policy 13-41

At least 2 building forms shall be demonstrated for each project. Building form shall vary in roof form, massing, building length and style.

Policy 13-42

At least 3 different, yet compatible color schemes for each neighborhood shall be provided.

Policy 13-43

Recess garage doors a minimum of 12" from face of garage.

Apartments and Condos

Plotting and Massing

- Project a front door image on units that open onto courtyard space or street front
- Place/access garages at rear alleys or auto courts
- Provide porches or covered entry spaces for stepped massing and transitions to courtyards or public spaces

Plans and Styles

- Buildings consisting of all garages or garages with storage/office/carriage units above will not be considered a separate building type

Apartment and Condo Criteria

Policy 13-44

For apartment communities of 200 units or less, a minimum of 2 building types shall be provided with moderately differentiated roof lines, building style and massing.

Policy 13-45

For apartment communities of more than 200 units, 3 building types shall be provided with moderately differentiated roof lines, building style and massing.

Policy 13-46

A single condo project shall not exceed 150 units.

Policy 13-47

At least 2 architectural styles and related color schemes per product shall be provided.

Policy 13-48

Condo buildings are required to vary garage treatments, overhangs and recesses to create visual interest and variety along alley frontage.

Policy 13-49

Condo and apartment buildings must have an offset in building plane of a minimum of 4 feet every 120 feet minimum. Roof plane breaks are required every 45 feet maximum.

Policy 13-50

The ground floor condo units facing Thematic Streets shall have individual sidewalk entries into the unit from the street.

13.5 Non-Residential Guidelines

Non-Residential Guidelines Overview

As with the residential neighborhoods; the design philosophy for the non-residential uses of Fairfield Train Station Community is similarly based on “town building principles” gleaned from traditional small towns of Northern California. The purpose of this sub-section is to provide general design guidance for the non-residential uses at Fairfield Train Station Community. The guidelines are intended to:

- Define the character and quality of non-residential uses within the Fairfield Train Station Community



- Promote the human and pedestrian scale of non-residential development and ensure compatibility between non-residential and residential uses
- Strengthen the pedestrian environment and improve connectivity
- Minimize potential negative visual impacts from the scale, bulk and mass inherent to large non-residential buildings
- Minimize negative impacts from on-site activities to adjoining uses
- Allow for needed flexibility to respond to conditions and constraints inherent to specific sites and uses
- Promote site, building and landscape designs that are consistent with Fairfield Train Station Community's sustainable principles

This section of the design guidelines contains two (2) general types of information. The first is a set of site planning guidelines that pertains to all categories of non-residential uses at the Fairfield Train Station Community including:

- Office
- Industrial
- Recreational/Public

The second provides a general description of the land use character and architectural guidelines for each of the Non-Residential land use categories.

Non-Residential Site Planning Guidelines

The Non-Residential Site Planning Guidelines set forth in this section pertain to non-residential uses within Fairfield Train Station Community. The Town Center and Lake District chapters provide additional detail on site planning and architecture for those districts. Retail and commercial buildings in the Town Center and Lake District are controlled by additional guidelines in Chapters 5 and 6. The guidelines address the site planning components that are important to promote high aesthetic quality, efficient use of the site, environmental responsibility and public safety throughout the non-residential use areas including:

- Connectivity
- Building Placement and Orientation
- Access and Circulation
- People Places and Site Amenities
- Parking
- Services, Refuse Collection and Utilities
- Drive-Through Facilities

Connectivity



As hubs of activities and employment, the non-residential use areas should be designed to allow for the safe and convenient movement of pedestrians, bicycles, vehicles, and public transit traffic including:

- Individual development parcels are encouraged to make internal connections to adjoining sites to encourage walking instead of driving to the same destination
- The pedestrian network should be distinct and easily identifiable through usage of one or more of the following elements where

Connectivity Criteria

Policy 13-51

Bicycle parking shall be accessible and located near a building's main public entrance and should be designed to complement the parcel's landscape and building architecture or be visually inconspicuous.

Policy 13-52

A sidewalk should link the primary building entrance and the adjacent public street.

practical: different paving materials, patterns, colors, or pavement heights; decorative bollards; well defined crosswalks and raised median walkways with landscaped buffers

Building Placement/Orientation

The location and orientation of buildings within the network of streets, pedestrian connections and open spaces on a site largely establish the character of a development parcel. Special attention should be paid to the impacts of visibility, massing, height and skyline and, where applicable, the animation of street life. Environmental consideration such as natural light and shade should also play an important role on building placement, orientation and setbacks including:

- Building orientation shall be coordinated to establish positive relationships with adjacent streets and structures
- Building location should be used to frame and enclose interesting outdoor gathering spaces
- Building location should be optimized to take advantage of off-site views
- Building orientation should allow for natural light and ventilation, when feasible
- Continuous, blank building elevations should not be placed adjacent to principal street



Building Placement/ Orientation Criteria

Policy 13-53

Building entrances shall be oriented toward the street(s) to encourage street life, where practical.

Policy 13-54

Parking structures shall not dominate street frontages.

Policy 13-55

For non-industrial buildings shall be oriented to frame the corner of an adjacent street intersection.

Sensitive Edges

The character of elevations of the industrial pads that are exposed to public view from either Vanden Road or the City of Vacaville must be addressed in a manner that is as aesthetically pleasing as possible. The visible edges of industrial pads shall be designed to follow one of edge conditions defined in the landscape guidelines (Chapter 12).

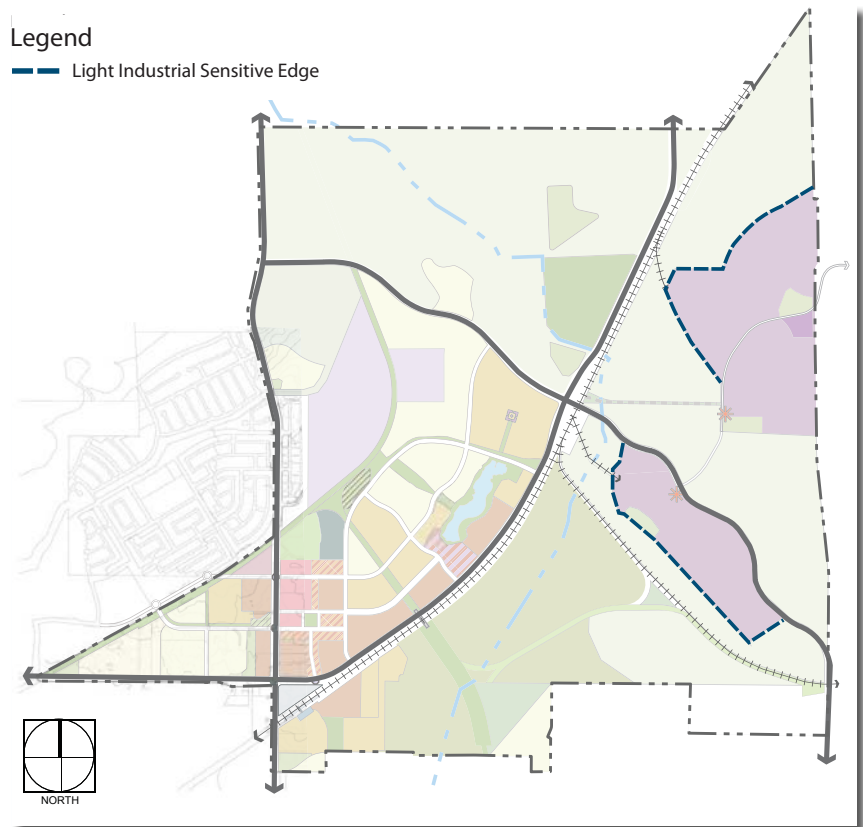


Figure 13-17 Light Industrial Sensitive Edges

Building Entries

Building entries should be prominent physical statements through orientation, architecture or signage and be visually appealing and identifiable to users.

Building Entry Criteria

Policy 13-56

In light industrial park development:

- Buildings shall have their primary entrances facing the adjacent public street
- Building entrances should be clearly visible and easily identifiable as visitors access the sites
- Pedestrian connections from the parking areas to the building entrance should be well defined



Vehicular Access and Site Circulation

Access points, site circulation and parking areas on a single parcel and between adjoining parcels should be coordinated to the extent possible to maximize site efficiency and to reduce the dominance of vehicular traffic on the community landscape. Simultaneously, pedestrian movement should be reinforced and supported by site plans to enhance the walkability of the non-residential uses.

The following are encouraged:

- Access to the primary entry of each parcel should be planned to create a distinct series of spatial and arrival experiences which may include an entry statement, a landscape corridor, a separate visitor drop-off and a fore-court or building entry plaza
- Driveways and parking areas should be designed to provide for sufficient vehicular stacking during peak hours
- Access points should be coordinated with adjacent parcels to maximize joint access for parking and circulation
- Access points should align with that of development parcels across the street whenever practical



Vehicular Access and Site Circulation

Policy 13-57

Access from arterial streets shall be minimized and may be limited to right-in, right-out access only.



People Places/Site Amenities

“People places” in non-residential areas are encouraged to promote a vibrant and interactive environment for residents, employees and visitors alike. These common use areas include, but are not limited to, plazas, streets with awning and architecture covers, outdoor cafés, building entry forecourts and courtyards. These spaces provide opportunities for activities such as outdoor eating, reading, casual meetings, and small group gathering.

Specific requirements for the Town Center and Lake District are included in Chapters 5 and 6. The sub-core service areas in the industrial park provide opportunities for plazas and courtyards for local workers.

The following are encouraged:

- Buildings should be carefully placed and arranged to create and enclose a variety of outdoor “people places”
- “People places” should be designed in proportion to the surrounding buildings and activities
- “People places” should be large enough to be usable, however not so large as to appear empty or barren
- “People places” should be furnished with appropriate site amenities such as benches, low walls, shade trees and/or shade structures, water elements and bollards to facilitate pedestrian uses
- “People places” should accommodate solar orientation to allow for sunny outdoor spaces in winter and shade in summer

People Places/Site Amenities Criteria

Policy 13-58

A plaza or courtyard of at least 4,000 sf shall be incorporated in each sub-core service area within the industrial park.

Parking

Individual development parcels are encouraged to seek opportunities and incorporate design features or transportation management strategies that include shared parking programs and strive to reduce automobile use such as enhanced accessibility to public transit, enhanced pedestrian connectivity and trip reduction programs.

The following are encouraged:

- Large parking areas should be dispersed as opposed to creating singular expanse of pavement
- Redundant circulation should be avoided and pavement widths reduced whenever possible in favor of greater landscaped areas
- Landscaped pedestrian paths should be provided through parking areas to building entrances

Parking Criteria

Policy 13-59

Designated carpool spaces shall be provided in a convenient location to the primary entry.

Policy 13-60

Parking areas shall be screened from the view of public streets by means of landscape walls, berms and/or planting materials of a minimum 3 feet height.

Utilities, Services and Refuse Collection

Utilities, services and loading areas should be provided on each non-residential parcel sufficient to serve the business and activities conducted on the parcel. Utility appurtenances include, but are not limited to, electrical transformers, electrical switchgear, electrical service sections, gas meters, back flow preventers, fire risers, fire connections, communication cabinets, etc. Service spaces and loading areas include, but are not limited to, outdoor storage, special equipment, maintenance, loading and refuse collection areas.

Utility Infrastructure:

- The location of above ground utility facilities should be identified early in the design process and shall be thoughtfully located such that they are generally placed at the side or rear of the building(s) in a location that is not highly visible from the street or pedestrian routes
- Utility cabinets and pedestals should be thoughtfully located within parking lot landscape islands or public right-of-ways where they can be screened, are not exposed to damage from vehicles and /or do not present a hazard to drivers and pedestrians
- The need for wireless communication facilities should be integrated directly into the architecture of buildings early in the design process with freestanding facilities fully screened and integrally designed with the parcel



Utility Infrastructure

Policy 13-61

The location of above ground utility facilities shall be such that they are generally placed at the side or rear of the building(s) in a location that is not highly visible from the street or pedestrian routes.

Policy 13-62

All rooftop mechanical equipment and communication equipment shall be completely screened.

Policy 13-63

Utility cabinets, pedestals and other above ground utility infrastructure shall be clustered and screened with landscape materials, berms, walls and/or other architectural elements and painted a tone that is neutral to their setting.

Policy 13-64

Infrastructure equipment including, but not limited to, drainage structures, sewer lines, gas lines, water lines, telephone and communications wires and equipment shall be installed and maintained underground.

Policy 13-65

All rooftop mechanical equipment and communication equipment shall be completely screened from abutting roadways by parapet walls or roof structures.



Service and Refuse Collection

- In larger commercial and business park developments; service and refuse collection areas should be separated from main circulation and parking areas to avoid conflict between service vehicles and regular traffic flow
- When possible; the service, loading and refuse collection areas should be designed to allow for on-site truck maneuvers to eliminate backing of trucks onto a public street
- Aprons associated with truck docks shall be designed to allow adequate maneuverability in and out of the facility
- The building mass of warehousing and distribution facilities should be softened by appropriate landscape and screening of the truck and trailer parking and loading dock areas

Service and Refuse Collection Criteria

Policy 13-66

Service and refuse collection areas shall be located within interior, side or rear yards, oriented away and screened from public view.

Policy 13-67

Exterior storage within and below solid screen walls may be permitted, provided the storage of all such materials is below the screen wall that surrounds them.

Policy 13-68

Service, loading areas and refuse enclosures shall be screened by a solid wall with materials of appropriate color and texture compatible to the adjoining building.

Policy 13-69

Screen walls shall be higher than the equipment, service or refuse collection areas that they are screening.

Policy 13-70

Gates for refuse enclosures shall be solid or opaque and should be made of durable low maintenance materials.

Policy 13-71

Shopping carts, dollies, flatbed carts, etc. shall be stored within the building or behind a screen wall that is integral to the architectural design of the adjoining building.

Policy 13-72

Storage of miscellaneous materials and merchandise should be contained within the buildings and should not be visible from off-site.

Drive-Through Facilities

Drive-through facilities should be located and designed to minimize their visibility from public streets and impact on the adjacent properties including.

- The location of drive-through facilities should provide the necessary vehicular stacking spaces
- The vehicular stacking should be located so that waiting vehicles do not block parking stalls or interfere with vehicular and pedestrian traffic





- The drive through windows should be designed to incorporate architectural coverings consistent with the building design in order to break up the built mass and provide shelter
- Conveyance systems used to connect remote drive through stations with the building should be architecturally incorporated or placed below grade

Drive Through Facility Criteria

Policy 13-73

No wall or fence exceeding three feet in height shall be constructed along the public street frontage.

Policy 13-74

Lighting beneath canopies shall be shielded and fully recessed to minimize glare.

Policy 13-75

Landscaped screening should be provided for the drive through windows and the order boxes if they face the street.

Walls and Fences

Walls and fences for screening and security purposes should be designed as integrated parts of the overall architectural and site design. All materials should be durable and finished in textures and colors complementary to the overall architectural design.

Non-Residential Architectural Guidelines

The variety in type and form of non-residential development will foster the vertical and horizontal mixed-use nature of the Project to provide a range of neighborhood serving retail, commercial, office and community oriented uses. The physical and visual integration of these elements will activate the urban mixed-use character of the Fairfield Train Station Community.

Image, character, quality and the aesthetic interest of a place is solidified by the architectural design of the building as an individual entity and as an element in the community composition. Architectural design of non-residential development at the Fairfield Train Station Community should be based on regional examples of quality architecture of enduring character representing the best of Solano County and the coastal bay area style.

The following are encouraged:

- Express the character of regional examples while using local construction practices and materials as possible

- Be of appropriate scale and authentic to the location and use of the building
- Present a unified development character without creating repetitious or redundant forms or design

All non-residential buildings should be designed to complement the quality and design vocabulary of the Fairfield Train Station Community. The following guidelines apply to all non-residential development.

General Guidelines

Building Form

- Building placement, form and detail should complement adjacent buildings to create an aesthetically interesting streetscape viable for pedestrian and business activity
- Building forms should be well-proportioned resulting in a balanced composition of elements
- Modulation and variation of massing between adjacent buildings is encouraged
- Buildings with simpler massing shall be incorporated texture, relief or offsets in plane to achieve heightened aesthetic architectural design
- Layering of wall planes and volumes should provide a rhythm of dynamic building forms and shadows
- Building located on the corners of major intersections should include:
 - Increased massing as prominent design element(s) or tower(s) to engage corridor views or
 - Step down massing elements to interface with the streetscape

Building Form Criteria

Policy 13-76

Building massing shall consist of a mix of heights, within a building, to provide visual interest.

Policy 13-77

Building shall have a well articulated elevation along public right of ways or pedestrian corridors.

Roof Considerations

Roofs shall be designed for functionality and to enhance/complement the overall architectural design of the building including:

- Vertical roof plane breaks, changes in building/ridge heights or other accent roof forms



- Form and materials integrated with the overall design vocabulary of the development
- Fascia and/or cornice elements consistent with the primary design
- Parapets, when used, that are contiguous and incorporate side/rear elevation returns to eliminate false front/unfinished appearance

Facade Treatments

Buildings should have articulation along vehicular and pedestrian corridors to create a pedestrian scale and visual interest along the streetscape including:

- Avoidance of blank walls of a maximum of 150', especially along principal pedestrian walkways
- Inclusion of additional architectural detailing such as massing offsets and articulation, variation in texture and color and banding on buildings comprised of one building material
- Use of projections, overhangs and recesses to provide shadow articulation and scale to building elevations
- Incorporation of comparable architectural treatment on building elevations exposed to streets or pedestrian thoroughfare

Facade Treatment

Policy 13-78

Any single building frontage may not be longer than 150 feet without a break of at least 15 feet in width and 5 feet in depth.

Policy 13-79

The front entry shall have an enhanced articulation.

Recreational / Public Buildings

Community buildings often act as focal points or landmarks and therefore will be held to a high degree of quality in both design concept and execution. The architectural expression of these buildings must be one of substance that conveys a sense of permanence and importance. Sensitive designed and implemented; the image associated with these structures will become 'public' or 'civic' in nature, contributing to a sense of "civic pride" in the Fairfield Train Station Community.

Specific architectural styles are not identified or required for public or quasi-public buildings in the specific plan area, however a clarity and authenticity of style shall be achieved for each building project. Generic, blended and unidentifiable styles are discouraged. Each building and project shall identify a historic regional style or a clearly defined contemporary vernacular, with identified precedents.



The following are encouraged:

- Elevations of all buildings on a single site should be elevated in a unified design
- Scale and mass of buildings should complement the surrounding neighborhood, although vertical elements and towers on public and quasi-public buildings are appropriate and encouraged
- A primary building entry should be oriented toward the major street frontage and emphasized with architectural treatment
- Monolithic building forms are discouraged
- Vertical and horizontal offsets are encouraged
- Accent roof forms and treatments are encouraged

Recreational/Public Building Criteria

Policy 13-80

All elevations visible from a public street or park shall be fully elevated with glazing and a variation in massing and roof form. When adjacent to primary pedestrian movement corridors and Thematic Streets, pedestrian-scale architectural elements, details and windows are required.

Policy 13-81

Public buildings shall be four-sided architecturally meaning that the design of all elevations should be equally considered.

Policy 13-82

Use of two (2) or more materials is required and should complement not complicate or detract from, the architecture.

Office

Office uses require a wide variety of building types ranging from garden-type office to Class A multi-story office. Office developments in the specific plan area are required to be well-designed with a focus on community image and corporate identity. Strong entrance treatments, good visitor access, attractive landscaping, clear graphics and signage are important elements. Plazas and courtyards should be incorporated on-site to provide visual interest as well as outdoor eating and gathering space for employees and visitors. Proximity to support uses and amenities is encouraged.

Where possible, office buildings should be placed in the most visible part of a development parcel close to major arterials, transit routes and commercial services. Architectural design of offices should consider the need for strong individual identity for tenants while complementing the design of adjacent buildings and uses. Office buildings require





more well-defined and aesthetic architectural consideration than light industrial buildings.

The following are encouraged:

- Office building should be located directly adjacent to the closest primary street and avoid site plan solutions that result in a building surrounded by parking. Corner locations are preferred if available within the parcel.
- Layered wall planes, banding, other architectural details and/or varying materials should be used to articulate the building form

Office Building Criteria

Policy 13-83

Monolithic masses of singular form, height, wall plane or material shall be avoided.

Policy 13-84

Entry statements for pedestrian/user identification shall be highly articulated.

Policy 13-85

Elevations along public streets shall incorporate glazing on at least 40% of the wall area.

Policy 13-86

At least two (2) of the following techniques shall be used to enhance building architecture and reduce overall mass:

- At least two (2) different building materials
- Vertical/horizontal wall plane projections/recesses (minimum 2-foot offset)
- Variation of roofline (height or form)
- Articulated pilasters
- Major trellis or awning elements (proportional to massing of building)

Industrial

Industrial uses may include multi-tenant, heavy manufacturing, light manufacturing and assembly, warehousing and distribution and web and computer-related facilities. Parcel development and building design should consider building placement and architectural treatment of entries, storage, loading bays, service areas and work spaces as necessary. Architectural design of industrial buildings may be more utilitarian in materials selection and form; however, consideration should be given to blending the quality of development and façade

elevation into the context of the Fairfield Train Station Community design vocabulary.

The industrial areas within the Specific Plan are highly visible from Vanden Road and other major streets due to the large amount of open space being preserved directly adjacent. Although the mix and type of businesses in the park will inevitably be diverse, the goal is to create a cohesive and attractive visual character to the extent practical when viewed from distant external viewpoints. This requires that parcels along the edge of the industrial park shall have a:

- Consistency of screening wall treatments along the industrial park edge
- Building color palette that provides flexibility and individual expression while maintaining a visual harmony when buildings are viewed as a group from a distance
- Consistency of landscape treatment so the industrial park appears as a unified whole rather than dramatically contrasting projects.

The following are encouraged:

- Tilt-up panels should utilize textured forms, sand blasting or scoring for visual relief although smooth panels, without the above elements, may be used in conjunction with color variation
- Metal panels, elements or wall systems should be finished to reduce reflection and glare
- To the extent possible, buildings should be designed to orient loading and storage areas away from major corridors or residential view with appropriate screening/shielding when not possible
- Loading and service areas should be designed with appropriate loading aprons and service access to facilitate appropriate maneuverability for the facility



Industrial Building Criteria

Policy 13-87

Although industrial buildings need to be highly functional and efficient, they shall still incorporate architectural elements, offset wall planes and changes in building massing/height on elevations visible from internal streets or when visible from surrounding external arterial streets.

Policy 13-89

Architectural elements or details, such as materials, colors, massing or similar elements, shall be used to highlight and accentuate entries.

Policy 13-90

Building parcels along the exterior edges of the two industrial parcels shall:

- Utilize building wall field colors consistent with the official Edge Condition Color Palette for the Specific Plan industrial zones (see City Planning Department for specific colors).
- Utilize the boundary wall design identified in Chapter 12 for all walls directly adjacent to external open space areas.
- Incorporate tree plantings per Chapter 12 within the rear portions of the parcel to assist in screening loading and processing areas from external views.





14.0 IMPLEMENTATION & ADMINISTRATION



14.0 IMPLEMENTATION & ADMINISTRATION

July 26, 2011



14.1 Overview

This section describes the mechanisms for implementation of the Specific Plan and subsequent development projects within its boundaries. Since the City of Fairfield is the public agency responsible for the administration of the FTSSP, the implementation procedures described in this section shall be consistent with all City rules, regulations, and policies.

The City of Fairfield shall implement and administer the provisions of the Specific Plan in accordance with the City General Plan and City Municipal Code and Specific Plan requirements of the State of California Government Code Section 65451. The City has directed the preparation of a specially tailored Train Station Specific Plan PD Overlay District to be adopted as part of the Municipal Code to direct development within the FTSSP where it differs from the Municipal Code to facilitate the shared vision of the City and community for development of this unique area.

This section includes a description of the relationship of the FTSSP to other city documents and sets out the process for approval of future projects in addition to describing the Phasing and Financing plans for the FTSSP.

14.1.1 Approvals by Other Public Agencies

Development of the Specific Plan area is dependent on the approvals of other public agencies, subsequent to adoption of the Specific Plan. Those approvals are described in greater detail in Chapter 3.0. They include but are not limited to the following:

- Reorganization actions approved by Solano Local Agency Formation Commission, including annexation to City of Fairfield
- Modification of the boundary of the Vacaville-Fairfield-Solano Greenbelt, by the Greenbelt Authority

- Construction of a trunk sewer line by Fairfield-Suisun Sewer District to serve the Specific Plan area

Policy 14-1

City shall coordinate and cooperate with other public agencies which have discretionary approval authority regarding certain aspects of development within the Specific Plan area.

14.1.2 Annexation and Greenbelt Boundary Modification

Two key prerequisites to urban development within FTSSP under City jurisdiction are: (a) annexation to the City and (2) modification of the boundaries of the Vacaville-Fairfield-Solano Greenbelt. It is likely City will first proceed with application for Greenbelt modification and then annexation application.

Greenbelt Boundary Modification. As shown in Figure 3-6, certain portions of the Specific Plan area lie within the boundaries of the Vacaville-Fairfield-Solano Greenbelt. There is a conflict between the Specific Plan and the Greenbelt Plan, developed in the early 1990's, for the northern portions of Planning Area 5 and Planning Area 6. Portions of these areas are designated for urban development by the Specific Plan but restricted to open space uses by the Greenbelt Plan. Conversely, portions of areas designated for urban development by the Greenbelt Plan are designated for open space by the Specific Plan. City has proposed modification of the existing Greenbelt in a manner that is consistent with the Specific Plan and with the intent of the Greenbelt Plan adopted by Fairfield, Vacaville and Solano County in 1994. To date, there has been no formal implementation of the Greenbelt Plan. This proposed modification to the Greenbelt boundary is shown in Figure 3-6. The Greenbelt Authority will consider this proposed modification subsequent to City's certification of the EIR and adoption of Specific Plan.

Annexation. The City anticipates annexation of those portions of the Specific Plan which are designated for urban development or open space areas that are substantially surrounded by urban development. The portions of the Specific Plan area proposed for annexation are shown on Figure 3-3. Those areas annexed to City will be subject to the land use policies and regulations of FTSSP and Fairfield Municipal Code.

The areas which remain in the unincorporated portion of Solano County shall be subject to the zoning and development regulations of the County. FTSSP shall have an advisory function for the County in the unincorporated areas within the boundaries of the Specific Plan.

City may file one or more annexation applications with LAFCO. LAFCO will consider the annexation application(s) subsequent to City's certification of EIR and adoption of Specific Plan and likely subsequent to Greenbelt modification. The annexation applications shall ensure



that necessary community park and industrial lands are annexed concurrently with residential areas of the Specific Plan.

Policy 14-2

No Master PUD Permit, tentative subdivision map or other development entitlement shall be approved by City for any portion of the Specific Plan area until both of the following has occurred: (1) annexation of the Specific Plan area has been approved by LAFCO, substantially in accordance with Figure 3-3, and (2) the boundary of the Vacaville-Fairfield-Solano Greenbelt has been modified substantially in accordance with Figure 3-6, provided the Greenbelt Authority is in effect at the time of annexation.

14.2 Relationship to Related Documents Adopted by the City

14.2.1 General Plan

The City of Fairfield General Plan was adopted by the City Council in September of 2002. The General Plan provides the framework for future growth and development within the City limits. Figure 3-1 in Chapter 3.0 presents the General Plan Land Use Map.

The General Plan establishes policies intended to reflect the values of the community and facilitate the achievement of their vision. As part of the planning process directed by the Fairfield City Council, the City recognized that certain amendments to the General Plan were necessary and appropriate to fully capture the public benefit of the Train Station. The amendments to the General Plan Land Use Diagram text amendments are found in Appendix B. The FTSSP is consistent with the requirements of the General Plan, as amended. A detailed discussion of the Specific Plan conformance with each of the applicable General Plan Policies is included in Appendix A.

14.2.2 Fairfield Municipal Code

Chapter 25 of the Fairfield Municipal Code contains the Zoning Ordinance of the City of Fairfield (Zoning Code). It is one of the most important tools used to implement the General Plan, identify allowable land uses, determine allowable intensity of use and development, and establish performance standards applicable to particular areas of the city.

The City has directed the preparation of a specially tailored Train Station Specific Plan PD Overlay District to be adopted as part of the Municipal Code to direct the development of the area consistent with the shared vision of the City and community for development of the FTSSP. Within the Municipal Code, the Zoning Code regulates the development and redevelopment of properties within the City. To facilitate the unique goals of the Specific Plan area, the Train Station Specific Plan PD Overlay District adds to the Zoning Code according to



particular special conditions within the Specific Plan area, i.e., development of appropriate residential density within a five minute walk of the Train Station.

The Train Station Specific Plan PD Overlay District identifies certain development regulations that are different from current Zoning Code requirements where necessary to achieve General Plan, and by extension, Specific Plan goals for the Project area. If Train Station Specific Plan PD Overlay District development standards do not address a particular issue, the Zoning Code and other existing City land use regulations will apply.

14.2.3 Final Environmental Impact Report and Mitigation Monitoring Program

The Specific Plan is a project, as defined by the California Environmental Quality Act (CEQA) and is subject to environmental review and documentation as specified in CEQA. CEQA requires that lead agencies disclose and consider the environmental consequences of projects for which they have discretionary authority prior to taking action on approval. CEQA also requires that lead agencies (either local or State government agencies) avoid significant environmental impacts wherever feasible, and mitigate impacts to less-than-significant levels wherever feasible. An Environmental Impact Report (EIR) is the appropriate document to address the impacts of the FTSSP.

Though environmental issues are addressed in certain sections of this Specific Plan, readers are directed to the FTSSP EIR and supporting documentation for a more thorough evaluation of environmental impacts of Specific Plan implementation.

CEQA Policies

Policy 14-3

For future development within the FTSSP, each project shall be reviewed to ensure compliance with CEQA. The FTSSP EIR is a programmatic EIR; additional environmental review pursuant to CEQA may be needed for subsequent entitlements with the Specific Plan area. City shall review development applications on a project-by-project basis to determine whether the impacts of that project on the environment were adequately addressed in the FTSSP EIR, or whether additional environmental review is required.

Policy 14-4

No subsequent or supplemental EIR shall be required for a future development project within the Specific Plan area unless the City of Fairfield determines that the impacts of a future development project were not addressed in the FTSSP EIR, or substantial evidence exists that supports findings set forth in CEQA Guidelines Section 15612, Subsequent and Supplemental EIRs. In those cases, the Director of



Community Development shall determine what appropriate subsequent or supplemental environmental documentation is needed.

Policy 14-5

In accordance with CEQA Guidelines Section 15182, Residential Projects Pursuant to a Specific Plan, no EIR or Negative Declaration shall be required for any residential project undertaken in conformity with an adopted Specific Plan for which an EIR has been certified.

Policy 14-6

A Mitigation Monitoring and Reporting Program (MMRP) shall be adopted in accordance with CEQA Guidelines Section 15097 to ensure implementation of the EIR mitigation measures. Future development within the FTSSP shall be required to implement the mitigation measures in the MMRP, to the extent they are applicable to a particular development project.

14.2.4 Development Agreements

Development Agreements may be prepared between the City of Fairfield and land owners to control and define certain aspects of project development within the FTSSP area. These Development Agreements would outline and clarify responsibilities of the developers, the property owners, and the City. Development Agreements would be reviewed by the Planning Commission. The Planning Commission then would make any recommendations which would be forwarded to the City Council for final action.

14.3 Amendment to Specific Plan

Pursuant to the California Government Code Section 65453, a Specific Plan may be amended as often as deemed necessary by the legislative body. An application to amend this Specific Plan (“Major Amendment”) may be filed by a landowner within FTSSP or may be initiated the Fairfield City Council. All Major Amendments to FTSSP shall be processed in the same manner as the adoption of the initial Specific Plan, unless the amendment is considered a Minor Amendment. At the time an application is submitted, the Community Development Director shall determine whether an amendment meets the criteria for a Minor Amendment.

14.3.1 Minor Amendments

A Specific Plan may need to be revised in order to appropriately respond to changing conditions and expectations during the course of its implementation. To address this aspect of the planning process, the Specific Plan provides for Minor Amendments. A Minor Amendment may be initiated in the same manner as a Major Amendment. It can be processed administratively, subject to approval by the City Community Development Director, although the action taken by the Community





Development Director may be appealed to the Planning Commission. In order for the City to approve a Minor Amendment, the revision must be in substantial conformance with the goals and policies of the FTSSP and the City of Fairfield General Plan. The environmental impacts of a Minor Amendment must be within the scope of the FTSSP EIR.

Examples of Minor Amendments to the FTSSP include but are not limited to:

- The addition of new or updated technical information that does not modify policies or standards of Specific Plan;
- Minor adjustments to land use and land use area boundaries within the established Specific Plan area as well as minor adjustments to the location of facilities, and street alignments where the overall land use pattern remains generally consistent with the Specific Plan objectives;
- Minor changes to infrastructure that does not adversely affect capacity or Level of Service;
- Modifications to the design concepts, architectural details, landscape treatments, fencing, lighting, trails and entry features and signage monumentation treatments, provided that they comply with the development standards contained within the Train Station Specific Plan PD Overlay District or City Zoning Code and the intent of the design guidelines;
- Transfer of permissible dwelling units between property owners within the same Planning Area, subject to conformance with the standards and intent of the Specific Plan;
- Transfer of dwelling units between density categories, subject to conformance with the standards and intent of the Specific Plan; and
- Transfer of dwelling units between Planning Areas, subject to conformance with the standards and intent of the Specific Plan.

Minor Amendments do not include:

- Any revisions to FTSSP policies or standards;
- A change in land use affecting more than two acres; and
- Revisions to Figures 10-1 through 10-27 in the Transportation Chapter.

14.4 Development Entitlements

Individual development projects proposed within the Specific Plan area are subject to review and subsequent approval of permits and entitlements by the City of Fairfield. Application, fee, and processing requirements shall be in accordance with the City's Zoning Code and other applicable regulations, unless modified by this Specific Plan. The Community Development Department will conduct an initial review

of the application for completeness and consistency with the adopted FTSSP before distributing it to other City departments for review.

Project applications will be reviewed for consistency with all pertinent development standards/policies, design guidelines, EIR mitigation measures, and other applicable conditions of approval adopted as part of the FTSSP. Applications, such as tentative maps, use permits, and variances, will be reviewed by the Community Development Department using established procedures. All subsequent development projects as part of the FTSSP, public improvements, and other activities shall be consistent with this Specific Plan.

14.4.1 Subdivision Maps

Property owners shall file applications for Tentative Maps in accordance with the State Subdivision Map Act and City's Subdivision Ordinance. Such applications shall be filed concurrently with or subsequent to a Master PUD Permit application. As required by California Government Code, no Tentative Map shall be approved unless City determines it is consistent with the Specific Plan. Upon approval of a Tentative Map, property owner may submit a Final Map. As specified in California Government Code Section 66474.1, the Final Map must be approved if found to be in substantial compliance with the approved Tentative Map.

14.4.2 Zoning Permits

A landowner shall file the applicable permit application(s) required by Section 25.40 of the Fairfield Zoning Ordinance and FTSSP prior to the commencement of any development activities or change in land use. No building permit shall be issued by City until all applicable permit application(s) required by Section 25.40 has been approved.

14.4.3 Building Permits

Following approval of a Final Map and all applicable Zoning Permits, project applicants within the Specific Plan area may apply for building permits. All Project structures must be consistent with the Specific Plan and approved Tentative Subdivision map and must comply with all the applicable codes adopted the City of Fairfield.

14.4.4 Master PUD Permits

Following adoption of the Specific Plan and Zoning, implementation of proposed development within the Specific Plan will then occur through approval of Master PUD Permits. For each Planning Area, the Master PUD Permit is the "bridge" between the overall Specific Plan and the more detailed subdivision maps. It is a conceptual land use and circulation plan for each Planning Area that will refine the intent and purposes of FTSSP for that geographic area. The Master PUD permit is also the key instrument the City will use to determine phasing requirements related to infrastructure construction and other goals of



FTSSP. No subdivision map, zoning permit, grading permit or other development entitlements will be granted until City has approved a Master PUD Permit.

Master PUD Permit Policies

Policy 14-7

A Master PUD Permit shall be prepared for Planning Areas 1-6. Preparation of Master PUD Permits is the responsibility of the landowner(s) within each Planning Area.

Policy 14-8

14-8-1 Each Master PUD Permit, or amendment to an adopted Master PUD Permit, shall be prepared by one or more landowners within that Planning Area and shall be submitted to City for the review and approval of the Planning Commission. The Master PUD Permit or Master PUD Permit amendment submittal shall include an application and processing fee, which are in effect at the time of application submittal.

14-8-2 A Master PUD Permit application may be filed by any property owner within the boundary of that Planning Area. However, City encourages the filing of joint applications among multiple property owners within each Planning Area. City shall provide notice to landowners within the Planning Area subject to the Master PUD Permit application that a Master PUD Permit submittal has been filed. Community Development Director shall conduct at least one informational meeting with Planning Area landowners prior to the Planning Commission's hearing on the application. The draft Master PUD Permit submitted to the Planning Commission shall demonstrate reasonable accommodation of all parcels within the Planning Area regardless of landowner participation in the preparation or submittal of a Master PUD Permit or Master PUD Permit amendment by one or more landowners.

Policy 14-9

A Master PUD Permit shall contain the following components regarding development within the boundary of its Planning Area:

1. Conceptual Land Plan which designates the location of various land uses, residential densities and housing types, consistent with the Specific Plan land use designations and policies; for parcels which are not owned by the Master PUD Permit applicant, the Conceptual Land Plan shall indicate the land uses as shown in the FTSSP Land Use Plan;
2. Conceptual Circulation Plan, showing the alignment of all streets and pedestrian/bicycle paths, except local residential streets and alleys;



3. Conceptual Park Plan, showing the location, layout and facilities of all park and trail facilities. Master PUD Permit application for Planning Area 5 shall include a conceptual park plan for the 50-acre Great Park;
4. Master Landscape Plan, including landscaping within public rights-of-way and any special edge treatments;
5. Master Wall and Fence Plan;
6. Master Exterior Lighting Plan;
7. Conceptual Water, Sewer, and Drainage Plans;
8. Requests to adjust Dwelling Unit Allocation among parcels;
9. Phasing of private development within Planning Area; and
10. Phasing of construction of public improvements both within and outside of the Planning Area.

Policy 14-10

For Planning Areas having multiple landowners at the time of the initial Master PUD Permit application, the Master PUD Permit shall be used by City to determine:

1. Alignment of streets which traverse various parcels which have different owners;
2. Location of pocket parks, detention basins and similar public or quasi-public facilities which serve that Planning Area;
3. Transfer of density within a Planning Area, if requested by property owners; and
4. Construction responsibilities for public improvements that benefit multiple property owners, including but not limited to detention basins, parks, streets located on existing property lines or which traverse or benefit more than one parcel, and utilities. Determination of construction responsibilities shall include cost sharing ratios and cost reimbursement obligations.

Policy 14-11

Through approval of a Master PUD Permit, City shall determine the timing and phasing of construction of public improvements and facilities by landowners. These landowner responsibilities shall be a condition of approval of each Master PUD Permit. These phasing responsibilities shall include the following:

1. Construction of public roads, utilities, parks and buildings which are within the boundary of a Master PUD Permit; and
2. Construction of public roads, utilities, parks and buildings which are outside the boundary of a Master PUD Permit.





Policy 14-12

The phasing required for each Master PUD Permit shall ensure that adequate public facilities and services will be available in a timely manner, in accordance with adopted City standards and policies. The factors to be considered in the imposition of conditions of approval for phasing shall include but are not limited to the following:

1. Construction of public road improvements such that Level of Service standards in City's General Plan will be satisfied throughout development of FTSSP and each Planning Area;
2. Construction of public roads, pedestrian and bicycle paths to promote adequate access for emergency vehicles and to promote safe and convenient alternate modes of transportation;
3. Construction of water and sewer utilities to ensure City and State regulations are satisfied; and
4. Construction of public and private park and recreation facilities to correspond to the rate of residential construction.

Policy 14-13

In order to meet City's economic development objectives for job creation, it is necessary to link the phasing of residential development in Planning Areas 4 and 5 with the construction of public improvements for industrial development in Planning Area 6. No later than City's issuance of the building permit for the 1750th residential unit in Planning Areas 4 and 5, the following improvements shall be constructed:

- The portion of New Canon Road from Vanden Road to Travis Air Force Base North Gate, including the railroad overcrossing;
- McCrory Road, from New Canon Road to existing Canon Road;
- Extension of the full compliment of utilities (water, sewer, drainage, natural gas, electrical and communication) into Planning Area 6; and
- Development of a minimum of 50 acres of industrial land; development means the following:
 - Final map has been recorded, creating one or more parcels;
 - Street improvements have been constructed to serve each of the parcels created in Final Map;
 - Rough grading has been completed; and
 - Utilities have been installed to serve each of the parcels created in Final Map.

Policy 14-14

Findings for Approval of a Master PUD Permit. No Master PUD Permit shall be approved unless City finds the Master PUD Permit is consistent with:

1. City's General Plan goals, objectives and policies;

2. Specific Plan goals, policies, standards and design criteria; and
3. Train Station Specific Plan PD Overly Zone District regulations.

14.4.5 Consistency with Land Use and Planning Area Map

The Land Use Plan (Figure 1-2) and the Land Use Summary (Table 4-1) of this Specific Plan illustrate the vision, goals and the overall land use distribution within the FTSSP. As such, the exact delineation, configuration and acreage of the land use areas and Planning Areas depicted on these maps may be refined as part of future detailed development proposals to the City. This is provided that such proposals remain consistent with the intent of this Specific Plan as described herein and the planning and design concepts, relationships, and ratios evident in these Plans (Figure 1-2 and Table 4-1) are not diminished.

14.4.6 Consistency with Illustrative Plan

The Conceptual Illustrative Plan shown on Figure 4-1 depicts development of the Specific Plan area in a manner consistent with its policies and standards. Alternative patterns of development may also be consistent with the Specific Plan. Development proposals which vary from the Conceptual Illustrative Plan may be approved by the City, provided City finds their design quality to be equal to or better than the design quality demonstrated in the Conceptual Illustrative Plan.

14.4.7 Approvals by City Council

The City of Fairfield City Council shall be the approving body for all Final Maps, Specific Plan Amendments, and any deviations from City's standard specifications for public improvements.

14.4.8 Approvals by Planning Commission

The City of Fairfield Planning Commission shall be the approving body for all Master PUD Permits, Tentative Subdivision Maps, development entitlements not consistent with the Conceptual Illustrative Plan; and any deviations from development standards.

14.4.9 Approvals by Community Development Director

The Community Development Director shall be the approving body for all Minor Specific Plan Amendments, development entitlements consistent with Conceptual Illustrative Plan and Parcel Maps.

14.5 Transfer of Dwelling Units

The FTSSP is expected to be built out over an estimated period of 20 years. During such a length of time, it is likely that some of the residential products initially proposed may change due both to broad changes in the national and regional housing markets and to local conditions. In addition, residential products may change due to





continuing architectural innovations particularly in the mid density ranges. To remain economically feasible, the housing types proposed for development within the residential Planning Areas will require continuing refinement in response to these market conditions. Also, the detailed planning, layout, and design of these residential neighborhoods and the precise delineation of Planning Area boundaries is complicated by multiple property owners within the FTSSP area. This typically creates a need for land swaps and other adjustments since the Planning Area boundaries and street grids do not always align with parcel ownership lines.

Thus, it is necessary to provide a mechanism for flexibility in regard to the permissible number of dwelling units and target density for each property as well as each Planning Area. This flexibility is achieved by allowing Transfer of Dwelling Units—the transfer of the right to build permissible dwelling units between properties within an individual Planning Area as well as between Planning Areas. This Transfer of Dwelling Units is subject to the limitations established in Table 14-1, Density Flexibility by Land Use, and Table 14-2, Permitted Dwelling Units within Planning Areas per Land Use. These limitations provide development flexibility; yet, more importantly, they ensure the achievement of the Specific Plan vision which is dependent on an appropriate balance and distribution of residential density in relation to the train station.

Transfer of Dwelling Units allows for the redistribution of residential units within the established Density Flexibility limitations. For instance, if the number of residential units developed within a property and/or Planning Area is planned to be below the designated target, then all or a portion of the remainder of those units may be transferred to another property or Planning Area. The Director of Community Development shall approve the transfer upon a determination that the transfer meets all of the following conditions:

- The resulting total number of dwelling units proposed for both the donor and recipient Planning Areas is within the mandatory maximum and minimum limits established in Table 14-1, Density Flexibility by Land Use, and Table 14-2, Permitted Dwelling Units within Planning Areas per Land Use.
- The resulting total number of dwelling units proposed within each density category for both the donor and recipient Planning Areas is within the mandatory maximum and minimum limits for each density category established in Table 14-1, Density Flexibility by Land Use, and Table 14-2, Permitted Dwelling Units within Planning Areas per Land Use.
- There is no adverse effect to the Level of Service of local or regional transportation improvements.
- There is no significant adverse effect on projected demands on parks, schools, infrastructure, and community facilities.

Table 14-1: Density Flexibility by Land Use

Land Use	Target Density du/ac	Minimum Density du/ac	Maximum Density du/ac
LR - Low Density Residential	7	4	10
MR - Medium Density Residential	14	10	20
HR - High Density Residential	28	20	50
CM - Mixed-Use Commercial	12	10	50

Table 14-2: Permitted Dwelling Units within Planning Areas per Land Use

Planning Area	Land Use	Target Dwelling Units	Minimum Dwelling Units	Maximum Dwelling Units
PA1	LR	462	264	660
	MR	196	140	280
	HR	392	280	700
	CM	0	0	0
	<i>PA 1 Subtotal</i>	1050	684	1640
PA2	LR	56	32	80
	MR	182	130	260
	HR	1092	780	1950
	CM	245	204	1021
	<i>PA 2 Subtotal</i>	1575	1146	3311
PA3	LR	26	16	40
	MR	588	420	840
	HR	308	220	550
	CM	0	0	0
	<i>PA 3 Subtotal</i>	924	656	1430
PA4	LR	329	188	470
	MR	592	423	846
	HR	743	531	1327
	CM	0	0	0
	<i>PA 4 Subtotal</i>	1664	1142	2643
PA5	LR	637	364	910
	MR	700	500	1000
	HR	0	0	0
	CM	0	0	0
	Potential Vacaville Water Treatment Site	140	80	140
<i>PA 5 Subtotal</i>	1477	944	2050	
PA9	PA9A	105	60	105
	PA9B	5	1	5
	<i>PA 9 Subtotal</i>	110	61	110
Project Total		6800	4633	6800



- Grading and landform alteration substantially complies with that previously approved for the Specific Plan.
- No new significant environmental impacts results from the transfer.

Policy 14-15

Approval of any Transfer of Dwelling Units shall be considered by City concurrently with a corresponding development entitlement application, such as tentative subdivision map or development review.

14.5.1 Transfer within a Planning Area

The AGA includes the net developable acreage as well as acreage for local streets, open space, parks less than 2 acres in area, and infrastructure per Table 4-1, Land Use Summary and the tables prepared for the Master PUD Permits. The Land Use Summary table is subject to minor refinement as approved by the Community Development Director as detailed delineation of the Planning Areas is established. However, in no case may the minimum required number of dwelling units for the overall FTSSP not be met or the maximum permissible number of dwelling units for the overall FTSSP be exceeded. (See Tables 14-1 and 14-2)

The Community Development Director shall maintain the FTSSP Density Flexibility as approval is granted and throughout the phased development of the FTSSP. Transfer of Dwelling Units provision outlined above shall apply to transfer of units within a Planning Area, provided that the target number of dwelling units for that Planning Area is not exceeded and the maximum and minimum limits established for density categories in Table 14-1, Density Flexibility by Land Use are maintained for that Planning Area overall. If these provisions are not met, a Transfer between Planning Areas may be required.

14.5.2 Transfer between Planning Areas

An increase in the number of units in any Planning Area beyond the target shall be offset by a decrease in the number of units in another Planning Area of the FTSSP under same ownership (or another ownership by agreement) as approved by the Community Development Director. The Transfer of Dwelling Units provision outlined above shall apply to transfer of units between Planning Areas, provided that the target number of dwelling units for the FTSSP overall is not exceeded and the maximum and minimum limits established for density categories in Table 14-1, Density Flexibility by Land Use, and Table 14-2, Permitted Dwelling Units within Planning Areas per Land Use are maintained for the FTSSP overall.

14.6 Density

The density flexibility plan established in Table 14-1, Density Flexibility by Land Use, and Table 14-2, Permitted Dwelling Units within Planning Areas per Land Use, allows for each Planning Area to have both a



target number of dwelling units and a range of permitted units above and below the target, with a mandatory maximum and a mandatory minimum number of dwelling units. The target number of dwelling units, calculated based on the AGA, is a projection of density that will be built in a planning area based on the Specific Plan Vision and the product types envisioned for that Planning Area. Also, the minimum and maximum number of dwelling units permissible on each property in effect targets that property to a specific density category.

Any tentative map or site plan for a planning area that has a residential density below or above the target may be granted a density transfer by the City provided that the proposed density conforms to the maximum and minimum density ranges limitations established in Table 14-1, Density Flexibility by Land Use, and Table 14-2, Permitted Dwelling Units within Planning Areas per Land Use. An increase in the density in any planning area beyond the target shall be offset by a decrease in the density in another area.

14.6.1 Calculation of Density

The density of a property shall be calculated by dividing the number of dwelling units provided by the Adjusted Gross Acreage (AGA).

14.6.2 Minimum Density Standards

The achievement of the Specific Plan vision is dependent on an appropriate concentration, balance and distribution of residential density. The FTSSP includes three categories of residential density to guide the implementation of this vision: Low Residential (LR), Medium Residential (MR) and High Residential (HR). The density ranges are as follows: LR—4 to 10 du/acre, MR—10.0 to 20.0 du/acre, and HR—20.0 du/acre up to 50.0 du/acre. The zoning designations for the parcels within these broad ranges are delineated in the Zoning Code and the Train Station Specific Plan PD Overlay District to be adopted as part of the Municipal Code. All development proposals for properties designated as such shall comply with the minimum density standards as established in Table 14-1, Density Flexibility by Land Use, and Table 14-2, Permitted Dwelling Units within Planning Areas per Land Use.

14.7 Phasing

The FTSSP is planned to be built out over an estimated period of 20 years in response to market demands and based on City's ability to provide an orderly extension of roadways, infrastructure, public services and utilities. The build-out will likely occur in multiple phases that may occur sequentially and/or concurrently. Infrastructure, services and amenities will be installed as needed to serve the incremental addition of future development phases.

The exact timing and sequencing of the construction of Backbone Fee Transportation improvements cannot be determined at this time due to





the uncertainty regarding the timing of: (a) roads constructed by Villages at Fairfield project, (b) funding of Jepson Parkway improvements by STA, (c) timing of development in the southeastern portion of Vacaville which impacts Specific Plan roads, and (d) timing and sequencing of development within the Specific Plan area. The phasing policies below provide a flexible structure for ensuring adequate infrastructure is constructed concurrently with development within the Specific Plan area.

For purposes of the policies in Chapter 14.0 a “Development Project” is defined to mean development pursuant to a subdivision map or, if no subdivision map is filed, then development authorized by a development review application.

Phasing Policies

Policy 14-16

Phasing of Backbone Transportation Improvements shall occur as follows:

1. Each Development Project shall fund or construct its proportionate share of the Backbone Transportation Improvements. A Development Project’s proportionate share is equal to the number of residential units within the development project multiplied by the dollar amount of the per unit Contemplated Northeast Fee.
2. Through the conditions of approval of each Development Project, City shall determine: (a) what portion of the Backbone Transportation Improvements shall be constructed with that Development Project and (b) timing of the funding or construction of those Backbone Transportation Improvements within the context of the build out of that Development Project.
3. The conditions of approval of each Development Project shall require that its proportionate share of the Backbone Transportation Improvement is constructed or funded by that Development Project no later than 67 percent of its build out.

Policy 14-17

Road improvements shall be constructed such that City’s Level of Service is maintained for each intersection within the Specific Plan area throughout its build out.

Policy 14-18

Notwithstanding any other Specific Plan policy, City shall have the right to require a Development Project to construct the improvements along its frontage of a Backbone Transportation Improvement consistent with its respective street section, as shown in Chapter 10.0. If the cost of those frontage improvements exceeds its proportionate share of the Backbone Infrastructure, City shall provide reimbursement to the Development Project for the excess cost, in accordance with City’s reimbursement policies and Northeast Fee program.

Policy 14-19

Through the conditions of approval of each Master PUD Permit and Development Project approvals, City shall determine the timing, location, and design of off-site and on-site drainage, water and sewer improvements that shall be constructed by the development within that Master PUD Permit, in order to comply with City standards.

Policy 14-20

Through the conditions of approval of each Development Project, City shall determine the timing, location, and design of off-site and on-site public parks that shall be constructed by the Development Project. Those conditions of approval shall be consistent with these general policies regarding the phasing of construction of public parks within the Specific Plan area:

14-20-1 The construction of the Lake Park shall be completed prior to the buildout of Planning Area 4. The timing of specific phases of construction of the Lake Park shall be determined through the terms of a development agreement with the landowners of PA 4 or, if no development agreement, through the conditions of approval for the Master PUD Permit for PA 4.

14-20-2 The construction of a minimum of 16 acres of the Great Park shall be completed prior to the buildout of Planning Area 5. The timing of this park construction shall be determined through the terms of a development agreement with the landowners of PA 5 or, if no development agreement, through the conditions of approval for the Master PUD Permit for PA 5.

14-20-3 The construction of the approximate 5-acre neighborhood park in Planning Area 3 shall be completed prior to the build out of that Planning Area, unless City establishes a later date for completion through the conditions of approval for the Master PUD Permit for PA 3.

14-20-4 The timing of the Linear Park improvements located east of Peabody Road shall be determined through terms of a development agreement with the landowners in PA 4 and PA 5. The timing of the Linear Park improvements west of Peabody Road shall be determined through the conditions of approval for the Master PUD Permit for PA 1. The portion of the Linear Park abutting New Canon Road and Peabody Road shall be constructed concurrently with the construction of those road segments.

Policy 14-21

The phasing of fire protection services shall be done in accordance with Policies 9-2 through 9-7.





14.8 Financial

14.8.1 Overview

The recent recession and resulting loss of local revenue has severely affected the City's ability to provide municipal services, particularly those supported by the General Fund. Revenues are not expected to rebound to pre-recession levels in the foreseeable future. It is the City's goal that each new development project provides sufficient revenue to pay for the cost of the services it demands and not further degrade services to existing residents and businesses. For FTSSP, the Guiding Principles (Sections 1.2.6 and 1.2.7), state it is critical that new development in FTSSP pay for the cost of ongoing municipal services and construction of new public improvements. The financial issues associated with FTSSP have three key components.

1. Construction of public improvements and facilities;
2. Financing of public improvements and facilities; and
3. Financing of ongoing municipal services.

While this section provides a general framework for financial issues, a comprehensive Financial Plan shall be adopted concurrently with the Specific Plan, which will be incorporated by reference or adopted as a supplemental section. The Financial Plan will:

1. Estimate the financial obligation of new development within FTSSP to pay for cost of Backbone Infrastructure, by estimating the cost to construct Backbone Infrastructure and deducting any existing funding sources for that infrastructure; this obligation will be calculated on an "equivalent dwelling unit" basis; and
2. Estimate the financial obligation of new residential development within FTSSP to pay for the cost of municipal services it demands, by estimating the cost of municipal services and deducting estimated residential tax revenue.

"Backbone Infrastructure" is a term used throughout this chapter. Backbone Infrastructure is defined to mean major public improvements designed to serve the entire Specific Plan area or substantial portions of the Specific Plan area. Examples of Backbone Infrastructure include but are not limited to the following:

1. New arterial streets or widening of existing arterial streets within FTSSP, as described in Chapter 10.0, including bridges and overcrossing structures;
2. New arterial streets or widening of existing arterial streets outside FTSSP which were part of the assumptions used in the traffic analysis of the EIR; these include:
 - a. Widening of Peabody Road to 6 lanes from the vicinity of Markeley Road to Air Base Parkway

- b. Construction of Walters Road extension from Cement Hill Road to Air Base Parkway; this may be 2 to 4 travel lanes, as will subsequently be determined by Solano Transportation Authority.
 - c. Construction of off-site road improvements identified as mitigation measures in the EIR
3. Municipal buildings which serve Specific Plan area, including new fire station building (relocation of Station 39) and neighborhood recreation center, as described in Chapter 9
 4. Major parks consisting of the Great Park, Linear Park and Lake Park, as described in Chapter 9
 5. Storm water detention/retention basins in excess of a volume of 5 acre-feet;
 6. Water transmission or distribution lines in excess of 12-inch diameter; and
 7. Wastewater collector lines in excess of 10-inch diameter.

While the City wants to ensure there is adequate financing for the construction of Backbone Infrastructure and ongoing Municipal Services, City also does not want to place an undue financial burden on future residents within FTSSP.

14.8.2 Financing of Municipal Services

It is the goal of the Specific Plan that new development within FTSSP:

1. Will have adequate municipal services
2. Generates sufficient tax revenue equal to or greater than the cost of municipal services it requires
3. Does not result in a future reduction in municipal services to existing residents and businesses within the city

“Municipal Services” means services provided by City of Fairfield to residents and businesses within Specific Plan, in accordance with adopted City standards. Municipal services include but are not limited to the following:

1. Police protection;
2. Fire protection;
3. Maintenance of public parks and public open space areas;
4. Maintenance of landscaping and street lights within public rights-of-way;
5. Maintenance of storm water conveyance facilities and detention basins;
6. Maintenance of public streets and roads;



7. Operation and maintenance of a neighborhood or recreation center; and
8. Maintenance of public open space lands within or adjacent to FTSSP.

The financing of municipal services for FTSSP shall be based on multiple sources of revenue:

- Property taxes and sales taxes from new development;
- State subventions based on population;
- Establishment of Community Facilities Districts to pay for the cost of public safety services, parks maintenance and open space maintenance;
- Establishment of Lighting, Landscaping and Maintenance Districts to pay for the cost of maintenance of landscaping, streetlights, sidewalks in public rights-of-way of arterial streets; Linear Park maintenance and other public property; and
- Establishment of Homeowners Associations to pay for the cost of maintenance of landscaping, streetlights and sidewalks in “pedestrian realm” abutting non-arterial streets.

Through the establishment of the CFDs, LLMDs and HOAs required in this chapter, sufficient revenues will be generated to the City to pay for the full cost of services provided to new development within the Specific Plan, including:

- Public safety services (police and fire protection)
- Maintenance of new public parks;
- Street lights, sidewalks, walls and landscaping abutting public streets within the Specific Plan;
- Storm water detention basins and water quality features; and
- Maintenance of public open space.

Financing Municipal Services Policies

New residential development within Specific Plan shall generate tax revenue to City equal to or greater than the cost of Municipal Services. In order to accomplish this, City shall establish or require the establishment of the following:

1. Community Facilities Districts (“CFD”) to pay for the cost of:
 - Providing public safety services (police and fire);
 - Maintaining public parks; and
 - Maintaining public open space.
2. Lighting, Landscaping Maintenance Districts (“LLMD”) to pay for the cost of maintaining:

- Streetlights, sidewalks, and landscaping within the rights-of-way of arterial streets;
- Stormwater detention and water quality facilities, including the water portion of the Lake Park; and
- Linear Park.

3. Homeowners Associations (“HOA”) to pay for the cost of:

- Maintaining the pedestrian realm abutting local streets;
- Maintaining pocket parks; and
- Providing certain private recreational services to supplement services traditionally provided by the City.

Property Tax Limitations

The property tax burden for residential property owners, at the time of establishment of each CFD, shall not exceed an annual combined total tax rate for all CFDs of 1.5 percent of the estimated assessed valuation of those residential properties. The estimated assessed valuation of residential properties shall be determined by City at the time of formation of each CFD. This 1.5 percent “tax target” consists of the combined total of the 1 percent property tax, any existing supplemental property taxes and the proposed CFDs. The 1.5 percent tax target excludes HOA dues, LLMD assessments, and any utility charges, utility user tax, parcel tax, sales tax or any other similar charges, taxes or fees that currently exist or may be subsequently adopted by City or other public agency. Given the City’s determination that it is necessary to establish CFDs provide adequate municipal services and given the City’s desire to avoid an excessive rate of property taxes for its residents, the City does not support the formation of a CFD within the Specific Plan by a school district or any other public agencies. The amount of tax capacity that is available to finance public improvements shall be the residual of CFD revenue available after the municipal services CFDs, as described below, have been satisfied within the 1.5 percent tax target.

Community Facilities Districts

All land within the Specific Plan area designated for urban development shall be included in one or more of the following Community Facilities Districts (CFDs):

CFD 2006-1. All land designated as residential or commercial land uses shall be included within CFD 2006-1. The tax revenue from CFD 2006-1 shall be used to provide police and fire protection services and to provide maintenance of certain public parks. The parks within the Project Area which will be maintained by this tax revenue consist of the land portion of the Lake Park, the 5-acre Neighborhood Park in PA 3, and the Great Park.

Park and Open Space CFD. All land designated for residential land use shall also be included within a new Park and Open Space CFD, in





addition to CCFD 2006-1. This CFD is in lieu of CFD 2004 and its initial tax rate shall be the same tax in effect at the time of its formation as CFD 2004. The new CFD shall have an annual adjustment in its tax rate based on an index. The tax revenue from this new CFD shall be used for the following purposes: (a) maintenance of a portion of the Great Park, (b) maintenance of open space land, and (c) acquisition of open space land.

Non-residential Public Safety and Open Space CFD. All land designated for industrial land use, located in Planning Area 6, shall be included in a new Non-residential Public Safety and Open Space CFD. The tax revenue from the Non-residential Public Safety CFD shall be used for Public Safety (municipal police and fire) services, including but not limited to expenses for personnel, equipment, and supplies and may be used for the maintenance of public open space.

CFD Policies

Policy 14-22

No Development Project with residential or commercial land uses shall be approved by City unless its conditions or approval require it to annex to CFD 2006-1 and to request the establishment of the new Park and Open Space CFD, or annex to it if the CFD has already been formed.

Policy 14-23

No Development Project with industrial land uses shall be approved by City unless its conditions of approval require it to request the formation of the new Non-residential Public Safety and Open Space CFD, or annex to it if the CFD has already been formed.

Policy 14-24

A Development Project applicant shall submit all required applications and submittal requirements and pay all costs associated with the establishment of any new CFD or annexation to an existing CFD.

Policy 14-25

None of the following categories of land use or ownership shall be included in or subject to payment of CFD taxes described herein:

- a. Public open space land,
- b. City owned right-of-way,
- c. Land owned by City for municipal purposes, or
- d. Land covered by a conservation easement established in accordance with the provisions of Section 5.16.

Policy 14-26

All CFDs shall include an annual escalation factor, as determined by City at time of formation of each CFD.

Lighting, Landscaping Maintenance District(s)

A Lighting, Landscaping Maintenance District(s) (“LLMD”) shall be established to pay for the maintenance of the following facilities:

- a. The Linear Park within Specific Plan area;
- b. The street lights, sidewalks, landscaping, street furniture and other architectural features within the rights-of-way of those portions of Vanden Road, Peabody Road, Cement Hill Road, and New Canon Road which are within the Specific Plan area; and
- c. Water portion of the Lake Park, including Storm water detention and conveyance facilities.

A single LLMD shall be formed for all of Planning Areas 4 and 5. Planning Areas 1, 2 and 3 may be annexed to that LLMD or may be annexed to existing LLMDs which abut those areas, as determined by City.

LLMD Policies

Policy 14-27

All land within Specific Plan designated for residential, commercial, office, industrial or private recreational development shall be included within a Lighting Landscaping Maintenance District (“LLMD”). Each LLMD shall be established by City in accordance with California Streets and Highways Code Section 22500, *et seq.*, and the City’s regulations and ordinances.

Policy 14-28

No Development Project shall be approved by City unless its conditions of approval require it to form or annex, as appropriate, to a LLMD. City shall determine whether a new LLMD shall be formed or whether the Development Project property shall be annexed to an existing LLMD.

Policy 14-29

Each Development Project applicant shall submit all required applications and submittal requirements and pay all costs associated with the establishment of any new LLMD or annexation to an existing LLMD.

14.8.3 Homeowner Association Responsibilities

Certain services will be provided to future FTSSP residents through one or more Homeowner Association(s) (“HOA”). Purposes of the HOA include the following:

- Providing a cost-effective method of providing certain, limited services



- Allowing residents to determine the level of service they wish to receive
- Reducing the cost and scope of the municipal services within the Specific Plan
- Playing an important role in maintaining the high quality standards of the Project.

HOA Policies

Policy 14-30

All land designated for residential development by FTSSP, including residential uses in mixed-use development, shall be included in one or more Homeowner Association(s) (“HOA”), except PA9b and PA9c. Such associations shall be established through the conditions of approval of each Development Project.

Policy 14-31

The duties and responsibilities of each HOAs shall include but are not limited to the following:

- Maintain all common areas which are designed and intended for use by residents, including pocket parks, landscape corridors, trails, plazas, private streets and private alleys;
- Maintain all walls or fences which abut arterial streets or which abut any public park or any other wall or fence which is not maintained by a private property owner;
- Operate and maintain Village Club recreation facility in PA 4, recreation center in PA 5 and any other recreation center in any other Planning Area;
- Maintain the “Pedestrian Realm” abutting all non-arterial streets. “Pedestrian Realm” means those portions of all street rights-of-way, within Project Area and as shown in Specific Plan, located between the curb and right-of-way line, but excluding Vanden Road and New Canon Road. The Pedestrian Realm shall be parcels owned by the Master HOA, and shall be separated from the public street rights-of-way. The Pedestrian Realm parcels shall be encumbered by public access and utility easements in favor of City through its approval of subdivision maps; and
- Maintain all parks with the exception of the Linear Park, Great Park, Lake Park, and the 5-acre neighborhood park in Planning Area 3.

Through conditions of approval of a Development Project, City may require that an HOA maintain other facilities not listed above. In no case shall City maintain any fences or walls abutting streets, parks or open space.

Policy 14-32

There shall be a single, Master HOA for PA4 and PA5. That HOA shall operate and maintain the Village Club and any other private recreation and park facilities within PA4 and PA5.

Policy 14-33

Prior to their recordation, the City will review and approve any CC&Rs and any articles of incorporation of the HOA, including the facilities maintained and operated by the HOA. An HOA may allow joint use of certain HOA facilities by the City through a subsequent agreement between the City and HOA.

Policy 14-34

In lieu of separate HOAs for PA1, PA2, PA3, and PA9, property owners within those Planning Areas may request annexation to the HOA for PA4 and PA5. Any such annexation is subject to subsequent approval by the PA4 and PA5 HOA.

Policy 14-35

In addition to a Master HOA for a Planning Area, neighborhood HOAs shall be allowed to operate and maintain facilities which benefit only that neighborhood.

Policy 14-36

Each HOA CC&Rs shall include standards for maintenance of the items listed in Policy 14-31. Those HOA maintenance standards shall not be less than City’s standards of maintenance for comparable facilities.

14.8.4 Construction of Infrastructure

Policy 14-37

New development within FTSSP is required to construct or fund all Backbone Infrastructure needed to serve the FTSSP area, as identified in FTSSP or FTSSP EIR or which are required as a mitigation measure by the FTSSP EIR. Backbone Infrastructure are those improvements listed in the Contemplated Northeast Fee program.



Policy 14-38

The determination of the phasing of construction of Backbone Infrastructure shall be in accordance with the Phasing Policies.

Policy 14-39

City shall not require new development within FTSSP to construct the following public improvements or facilities:

1. Solano County shall be responsible for the construction of a branch library building;
2. Fairfield-Suisun Sewer District shall be responsible for the construction of the relief sewer trunk link from the vicinity of Peabody/Vanden Road intersection to the vicinity of E. Tabor/Walters Road intersection;
3. Travis Unified School District shall be responsible for the construction of any school facilities needed to serve students from FTSSP area; and
4. Public improvements or facilities which were constructed by City or other public agencies in advance of the timing for construction by FTSSP landowners.

This policy shall not preclude: (a) any agreement between a Development Project applicant and another public agency regarding the construction of public improvements or facilities by a Development Project applicant, or (b) exercise of any statutory authority of another public agency to require the construction of a public improvement or facility by a Development Project applicant.

Policy 14-40

The City's conditions of approval of a Development Project shall require the applicant to construct all public improvements within its boundary and any off-site improvements in the time, manner and location as required by the Specific Plan and Master PUD Permit. These include but are not limited to:

1. Local residential streets, collector streets, connector streets, local industrial, and alleys;
2. Water and sewer lines of lesser diameter than described in Section 14.8.1;
3. Public parks and trails required by FTSSP which are not listed in Section 14.8.1, including pocket parks; and
4. Any other public improvements required by FTSSP or by other City regulation which is not listed in Section 14.8.1.

14.8.5 Financing of Public Improvements and Facilities

The financing for construction of public improvements and facilities required by FTSSP have multiple sources. These sources include but are not limited to the following:

1. Existing City development impact fees, including AB 1600 fees and construction license tax;
2. A Contemplated Northeast Fee, which will amend the existing Northeast Fee;
3. A Contemplated AB 1600 Traffic Impact Fee which will amend the existing AB 1600 Traffic Impact Fee
4. A Contemplated AB 1600 Park Impact Fee which will amend the existing AB 1600 Park Impact Fee
5. New Infrastructure Community Facilities District(s) which will finance the construction of infrastructure which is not included in the fee program of an adopted Development Impact Fee;
6. Travis Unified School District Impact Fees (school facilities);
7. Solano County Public Facilities Impact Fee (branch library);
8. State and Federal grants, including those administered by Solano Transportation Authority for Jepson Parkway;
9. Fairfield Suisun Sewer District (sewer relief line); and
10. Private financing by landowners within FTSSP.

Financing Policies

Policy 14-41: Contemplated Northeast Fee

For the purposes of Chapter 14.0, the term “Contemplated Northeast Fee” shall mean an amendment to the existing Northeast Fee that includes a revision in the amount of new development within the boundary of the Northeast Fee and infrastructure needed to accommodate that development. The Contemplated Northeast Fee includes the new development allowed by the Specific Plan as well as the infrastructure needed to support that development.

14-41-1 City shall use its best efforts to consider adoption of the Contemplated Northeast Fee within 270 days of adoption of the Specific Plan.

14-41-2 In addition to the public improvements which benefit all development within the Northeast area, the Contemplated Northeast Fee will also fund certain public improvements which primarily benefit new development within Specific Plan area. New development within the boundary of the Contemplated Northeast Fee boundary but outside Specific Plan area will be excluded from paying that portion of the fee which funds those

improvements which primarily benefit the area within the Specific Plan.

14-41-3 Until City has adopted the Contemplated Northeast Fee, no Master PUD Permit, tentative map, zoning permit or other development entitlement within the Specific Plan area shall be approved, unless an alternate financial obligation with equivalent dollar value has been included in the terms of a development agreement adopted by City Council.

Development Impact Fee Credits

The timing and manner of the fee credits or reimbursements shall be in general in accordance with the following policies. In the case of conflict with these policies and the provisions of the subsequently adopted Northeast Fee program, the provisions of the adopted Northeast Fee program shall prevail.

Policy 14-42: Contemplated Northeast Fee Credits

1. When a developer constructs improvements which are part of the fee programs for City's Development Impact Fees, including the Contemplated Northeast Fee, Developer shall receive fee credits or reimbursement from City.
2. A Fee credit shall be earned by a developer when:
 - a. A developer has entered into an improvement agreement with City to construct a public improvement which is included in the fee program for the Contemplated Northeast Fee.
 - b. A developer has dedicated right-of-way or provided biological mitigation, to the extent that such right-of-way dedications and biological mitigation are included in the adopted Northeast Fee program.
 - c. A developer has paid for the Specific Plan and EIR or similar studies, to the extent such documents are included in the adopted Northeast Fee program.

The fee credit will be the amount of the fee otherwise due and payable at the time of building permit issuance, and shall be deducted from each permit until the total amount of the fee credit has been exhausted.

3. The amount of the Contemplated Northeast Fee credit associated with each subdivision improvement agreement shall be equal to the estimated cost of the improvement being constructed, as described in the adopted fee program for the Contemplated Northeast Fee. The amount of the fee credit, as determined by City, shall be included in the subdivision improvement agreement.
4. Any Fee credits earned by a Development Project shall be applied by City only to that Development Project and may not be transferred to another property, unless agreed to by both City and Developer.

5. If a developer defaults on an improvement agreement described in the subsection, Developer shall lose all unused fee credits and shall be liable to compensate City for fee credits used in lieu of paying fees. The compensation due City shall be equal to:
 - a. 100 percent of the dollar amount of the fee credits used, and
 - b. Accrued interest from the date fee credit was used, compounded at an annual rate of 6 percent, and
 - c. A penalty payment equal to 20 percent of the dollar amount of the fee credits used.

Policy 14-43: Park Impact Fee Credits

1. A developer shall receive Park Impact Fee credits for the public parks it constructs, as provided in this subsection.
2. The public park improvements eligible for fee credit are: Great Park, Lake Park, Linear Park, 5-acre neighborhood park in PA3 and pocket park meeting the criteria in Policy 9-13. The types of fee credits provided by City for public park improvements constructed by a developer consist of the following:
 - a. City's AB 1600 Park Impact Fees
 - b. 25 percent of City's park and Recreation Bedroom Tax
 - c. Northeast Fee for Linear Park; 75 percent of the estimated cost of Linear Park construction, as established in the Contemplated Northeast Fee shall be a credit against the Northeast Fees to be paid by Developer and 25 percent of the cost shall be a credit against the AB 1600 Park Impact Fees to be paid by Developer.
3. When a developer is eligible for an AB 1600 Park Fee or Bedroom Tax credit, the amount of the fee otherwise due and payable at the time of building permit issuance, shall be deducted from each permit until the total amount of the fee credit has been exhausted.
4. Fee Credits or reimbursement for construction costs for public park improvements shall be determined and fixed by the City Engineer at the time construction commences on the improvements listed above. A developer will earn fee credits when the improvements agreement is executed and the construction bonds are in place for the public park improvement covered by such improvement agreement.
5. If the amount of fee credit is less than the amount of the otherwise applicable fee, a developer shall thereafter pay an amount which, when added to the credit received for the construction of facilities, equals the fee obligation.
6. If the amount of fee credit is greater than the amount of the otherwise applicable fee, a developer shall be paid the difference from the appropriate Development Impact Fee fund. The timing of

reimbursement to a developer shall be subject to subsequent reimbursement agreement between City and a developer for improvements constructed. City shall not unreasonably withhold reimbursement to a developer if funds are available, but City may defer reimbursement if such funds are needed for other Development Impact Fee projects. Reimbursement from City to a developer is available only to the degree funds are available.

7. City shall use Park Impact Fees collected within Specific Plan Area and Northeast Area to reimburse a developer for the improvements which exceed the amount of the fee credits. This reimbursement shall occur on an annual basis and shall be paid only to the extent the City has available Park Impact Fees which: (a) had been collected by City during the prior calendar year, (b) are not subject to reimbursement to other developers in City, and (c) are described in a reimbursement agreement.

Policy 14-44: Reimbursement Agreements

In order for a developer to receive reimbursement from city, City and a developer shall first enter into a reimbursement agreement. In addition to its other terms, the agreement shall provide that:

- a. The general fund of the City is not liable for payment of any obligations arising from the agreement;
- b. The credit or taxing power of the City is not pledged for the payment of any obligations arising from the agreement;
- c. The developer shall not compel the exercise of the City taxing power or the forfeiture of any of its property to satisfy any obligations arising from the agreement;
- d. The obligation arising from the agreement is not a debt of the City, nor a legal or equitable pledge, charge, lien, or encumbrance, upon any of its property, or upon any of its income, receipts or venues, and is payable only from the fees deposited in the appropriate City Development Impact Fee fund; and
- e. The reimbursable amount shall be adjusted annually in accordance with the engineering news record index applicable to the fees themselves, or otherwise adjusted by agreement between the Parties.

Policy 14-45: Infrastructure Community Facilities District

A Development Project applicant may request that City form an infrastructure CFD(s) for the purpose of funding the construction of public improvements required by the Specific Plan. The infrastructure funded by a CFD shall not be a public improvement funded by a

Development Impact Fee, including the Contemplated Northeast Fee. The amount of any taxes levied by one or more infrastructure CFDs available shall be limited in accordance with the policies in Chapter 14.0.





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APPENDIX A: FAIRFIELD GENERAL PLAN CONSISTENCY ANALYSIS



APPENDIX A GENERAL PLAN CONSISTENCY

July 26, 2011



The Fairfield Train Station Specific Plan (FTSSP) is consistent with the City of Fairfield General Plan and serves to implement all aspects of the General Plan relevant to the designated area.

A Specific Plan is a plan adopted either by ordinance or resolution for a particular area of land. A Specific Plan may contain its own development standards (zoning), and thus provides greater flexibility in the distribution of the land uses.

The FTSSP is intended to be supportive of, and consistent with, the goals and policies of the Fairfield General Plan. It has been written to implement the Specific Plan designation assigned to FTSSP area by the General Plan (2002).

The purpose of this section is to ensure that the Specific Plan is consistent with the relevant goals and policies of the City of Fairfield's General Plan as required per Government Code Section 65454.

The Fairfield General Plan includes ten elements:

1. Agriculture Element
2. Circulation Element
3. Economic Development Element
4. Health and Safety Element
5. Housing Element
6. Land Use Element
7. Open Space, Conservation, and Recreation Element
8. Public Facilities and Services Element
9. Travis Air Force Base Protection Element
10. Urban Design Element

The goals, objectives, policies, and programs for each General Plan element have been evaluated in the following pages. Following each goal



of the General Plan elements, specific objectives, policies, and programs are laid out. Following each goal, policy, and objective, a statement is made in italics, indicating how the Specific Plan is consistent with that goal, objective and/or policy. In some cases, it is indicated that the specific goal, objective, or policy is not applicable to the Specific Plan.

A.1 Agriculture Element

Policy AG 1.1: Preserve agricultural enterprises by supporting right-to-farm policies, and separating and buffering agricultural lands from new development areas. (See Policy OS 1.1)

Adoption of FTSSP does not affect the City's existing right-to-farm policies.

Under the proposed Project, on-site areas currently used for grazing would be designated as a Greenbelt/Conservation Area. The Specific Plan also includes establishment of on-site and off-site areas for habitat conservation and mitigation. The proposed Project would not place urban land uses adjacent to off-site agricultural operations.

Policy AG 1.2: Provide economic support for permanent commitment to agricultural uses. (See policy OS 1.2)

Adoption of FTSSP does not affect the City's ongoing commitment support agricultural uses.

Policy AG 1.3: Maintain current agricultural zoning in the area east of Travis Air Force Base, including the Jepson Prairie. (See Policy OS 1.3)

Adoption of FTSSP does not affect the current agricultural zoning in the area east of Travis Air Force Base.

Policy AG 1.4: Permanently preserve productive agricultural lands within the Suisun Valley by continuing to direct new urban development away from the Suisun Valley. (See Policy LU 2.1, OS 1.4 and ED 8.3)

This policy is not applicable because the Project site is not located within the Suisun Valley.

Policy AG 1.5: Strongly reaffirm the City of Fairfield's commitment to the basic goals of the 1974 Agreement between the City of Fairfield, Solano County Flood Control and Water District, and the Solano Irrigation District (the "SID Agreement"), and actively participate in any local measures that promote preservation of agriculture adjacent to Fairfield. (See Policy OS 1.5)

FTSSP does not apply to any land within the area addressed by the 1974 Agreement and would not affect City's ongoing compliance.

Through adoption and implementation of FTSSP, over 2000 acres of grazing or agricultural land will be in permanent open space through conservation easements.

Policy AG 1.6: What is Urban shall be municipal, and what is rural shall be within the County. Any urban development requiring basic municipal services shall occur only within the incorporated City and within the urban limit line established by the General Plan. (See Objective LU3, Policy LU 3.1, and Policy OS 1.6)

All land designated for urban development by FTSSP is proposed for annexation to the City of Fairfield. See Figure 3-3. All land designated for urban development by FTSSP is within the urban limit line established by the General Plan. See Figure 3-4.

Policy AG 1.7: Annexation areas contained in the City's 1998 Comprehensive Annexation Plan which contain prime agricultural soils shall be given a lower priority than annexation areas without prime agricultural soils with the same land use designation or intended use unless: a) this would not result in orderly development patterns (i.e. pockets of prime agricultural soils surrounded by land developed with urban uses), or b) the annexation area is within an area designated for development by the General Plan, or c) the prime agricultural soils contained within the annexation area are not planned for urban development.

The 1998 Comprehensive Annexation Plan is not in effect and has been superseded by the City's Municipal Service Review. The FTSSP does not contain prime agricultural soils so there is no effect on the priority of annexation based on this factor.

Policy AG 1.8: Annexation proposals should not be processed while there is an active California Land Conservation Act ("Williamson Act") contract(s) on a substantial portion, being 50 percent or more, of the subject property or until a cancellation has been filed with Solano County and less than five years remain on the life of the contract. Exceptions to this policy may be made under either of the following circumstances: a) for properties where development will result in the permanent preservation of substantial (50 percent or more of the subject annexation area) amounts of agricultural land or open space; or b) for those properties whose Williamson Act Contracts were protested by the City prior to adoption of the City's 1998 Comprehensive Annexation Plan.

The proposed Specific Plan area contains 288 acres of land under active Williamson Act contracts. This amount of land under Williamson Act contracts is well below 50 percent of the proposed annexation area and is therefore consistent with Policy AG 1.8. In addition, the City of Fairfield protested the establishment of Contract #508.

Implementation of the proposed Project could require the cancellation of 61 acres (out of 288 acres) of Williamson Act contract lands in areas proposed for residential development and roadway improvements since these uses would not be permitted under the existing contracts. It is unknown if cancellation of Williamson Act contracts would be consistent with California Government Code Sections 51282(a) and



51282(b), and therefore, no feasible mitigation measures are available at this time. The remainder Williamson Act contract lands would be designated under the proposed Project as a Greenbelt/

Conservation Area, which is consistent with Solano County's intent to protect agricultural and open space land uses through conservation contracts.

Policy AG 1.9: Annexation proposals that are within one of the City's Open Space Planning Areas (OSPA's) that contain prime agricultural soils shall be first reviewed by the Open Space Commission for their review and comment.

The proposed Project site does not include any Important Farmland, Prime Farmland and Unique Farmland about the Project site's eastern boundary in an area north of McCrory Road, east of North Gate Road, and southeast of Vanden Road. However, this portion of the Specific Plan would be designated as a Greenbelt/Conservation Area, and the lack of development will result in less pressure to convert active farming at the edges of the Specific Plan boundary to urban use.

Policy AG 2.1: Cooperatively work with farmers, property owners, universities, colleges, and agricultural organizations and agencies to enhance the viability of agricultural uses and activities. (See Policy ED 8.1)

The Specific Plan will not impact the City's active support of agricultural uses in the Fairfield area.

Policy AG 2.2: Coordinate open space preservation and conservation programs with economic development programs to enhance agriculture. (See Program ED 8.2)

The FTSSP will preserve significant agricultural and habitat protection lands. However, there are currently no active programs focusing on promoting agriculture in specifically this area, which is dominated by low intensity grazing and residential land uses. .

Policy AG 2.3: Development shall not encroach upon or consume productive cropland in areas such as the Suisun Valley. (See Policy OS 1.4, Policy OS 1.5, and Policy ED 8.3)

The proposed Project would not encroach upon or consume productive cropland. Implementation of the Specific Plan would not result in changes to the physical environment that could result in conflict with agricultural uses. Under the Specific Plan, on-site areas currently used for grazing would be designated as a Greenbelt/Conservation Area or could potentially be used as a mitigation bank. The Specific Plan would not place urban land uses adjacent to off-site agricultural operations. Therefore, the City does not anticipate compatibility conflicts between the proposed Project and on- or off-site agricultural operations.

Policy AG 2.4: Attract industry that complements and supports the local agricultural economy. (See Policy ED 8.4)

This is a City-wide policy.

Policy AG 3.1: Encourage the preservation of agricultural land surrounding the City and permanently preserve agriculture in the Suisun Valley. (See Policy LU 2.1, Objective OS 1, Policies OS 1.1, OS 1.2, OS 1.3, OS 1.4 and OS 1.5, Programs OS 1.2 A, OS 1.2 B, OS 1.2 C, OS 1.4 A, and Rancho Solano North Master Plan Area Policy 6)

Implementation of the Specific Plan would not result in changes to the physical environment that could result in conflict with agricultural uses. Under the Specific Plan, on-site areas currently used for grazing would be designated as a Greenbelt/Conservation Area or could potentially be used as a mitigation bank. The Specific Plan would not place urban land uses adjacent to off-site agricultural operations. Therefore, the City does not anticipate compatibility conflicts between the proposed Project and on- or off-site agricultural operations.

A.2 Circulation Element

Policy CI 1.1: Develop a network of roads that is compatible with the general land use patterns of the City.

As part of the proposed Project, a roadway network has been developed that includes multiple connections to the key arterials traversing the site, Peabody Road and Cement Hill Road/Vanden Road, and an internal network of two-lane roadways that will efficiently distribute traffic while providing a high level of mobility for vehicles, bicycles, pedestrians and transit vehicles. Canon Road would be realigned to provide a new connection that would serve both internal Project trips and trips between the Specific Plan Area and external destinations such as Vacaville and Travis Air Force Base.

Policy CI 1.2: Establish a mix of land uses throughout the City that will be conducive to the use of alternative modes of transportation, such as transit, paratransit and bicycles. Pedestrian travel shall be encouraged through the location of employment centers and commercial development within proximity of residential areas. (See Policy LU 1.2)

Policy CI 1.3: Coordinate development of the circulation system with land use planning.

The proposed Project creates a compact project providing a diverse mix of housing, shopping, employment, cultural, and recreational opportunities to encourage walking, bicycling, and other alternative transportation trips. A bicycle and pedestrian network is a part of the proposed Project and includes an extensive network of pedestrian pathways and a cohesive bicycle path system that will enable residents to access the Project's public transportation facilities, the Lake Park, the numerous parks and other public amenities, consistent with the safety and sensitive resources protection priorities.

Policy CI 1.4: Acquire the ultimate right-of-way for streets during early stages of development.





As a master planned development, road rights of way will be acquired prior to development. An ultimate right-of-way is designed for Vanden Road. Please refer to Section 10.0, Transportation Plan, of the Specific Plan for additional information.

Policy CI 1.5: Control the spacing of access points to adjoining properties along arterials to assure the free flow of traffic on the arterial.

The proposed Project includes a Transportation Plan with general development standards (please refer to Section 10.0), which will dictate construction and upgrade of the roadway facilities consistent with City standards. All roadway and intersection designs will comply with City standards providing safe and efficient traffic patterns and circulations.

Policy CI 2.1: Local circulation system improvements shall be consistent with the goals and objectives stated in the Metropolitan Transportation Commission (MTC) Regional Transportation Plan. (See Objective LU 5 and Policy LU 5.1)

The proposed Project includes a Transportation Plan with general development standards (please refer to Section 10.0), which will dictate construction and upgrade of the roadway facilities consistent with City standards. All roadway and intersection designs will comply with City standards providing safe and efficient traffic patterns and circulations.

Policy CI 2.2: Cooperate with local and regional jurisdictions in the preparation of State-mandated regional plans, including the Clean Air Plan and the Solano County Congestion Management Program. (See Objective LU 5 and Policy LU 5.1)

The FTSSP does not directly impact this Policy.

Policy CI 2.3: Work with Caltrans to identify needed improvements to its highway/interstate facilities in the City and implement necessary programs on the state highway system and its interchanges/intersections with local roadways.

Policy CI 2.4: Work with Caltrans and adjacent jurisdictions to improve the operational performance of I-80, I-680 and State Route 12 as regional facilities.

Policy CI 2.5: Work with Caltrans in analyzing the performance of freeway interchanges located in the General Plan area, and seek appropriate improvements.

The FTSSP does not directly impact CALTRANS State Highways. However, implementation of this Project will result in completion of the next phase of Jepson Parkway (Peabody Road), a regional alternative/reliever route for freeway traffic.

Policy CI 2.6: Cooperate with adjacent jurisdictions to plan and construct a loop system of arterial streets and roads so that traffic can flow around the periphery of the city and not cause congestion in central Fairfield.

Implementation of this Project will result in completion of the next phase of Jepson Parkway (Peabody Road), a regional alternative/reliever route for freeway traffic.

Policy CI 2.7: Discourage vehicles from using I-80 to travel between Cordelia and Fairfield. Proposed roadways paralleling I-80 to the south should be developed to ease congestion. Any roadway development shall be consistent with Land Use Element Policy LU 2.1, which protects the Suisun Valley agricultural lands.

The FTSSP is located northeast of Central Fairfield, and development in the area will not directly impact travel between Cordelia and Central Fairfield. Nor will the FTSSP affect development in Suisun Valley. Other projects independent of this specific plan currently in planning or construction will reduce this congestion, including completion of the North Connector Project in late 2010. In addition, the FTSSP will facilitate implementation of the Jepson Parkway Project, which will provide an alternative to Interstate 80 for regional and intra-local traffic.

Policy CI 2.8: Cooperate with adjacent jurisdictions to improve the principal arterial gateways to Fairfield to facilitate the movement of traffic flowing into and out of the City. Improvements shall be consistent with the Urban Design Element and Urban Design Plan. (See Policy UD 1.1 and Policy UD 1.4)

Policy CI 2.9: Continue to support and participate in efforts led by the Solano Transportation Authority to evaluate and mitigate traffic flowing between cities in Solano County.

The FTTSP includes improvements to major roadways, including Peabody Road and Vanden Road, which will improve traffic movement into and out of the City. The FTSSP is being coordinated with Vacaville, Suisun City, and the Solano Transportation Authority. Implementation of the Specific Plan will specifically include completion of the Jepson Parkway Concept Plan, a multijurisdictional plan prepared by STA to facilitate intra-local traffic flows in central Fairfield.

Policy CI 3.1: Prioritize transportation projects based on reducing traffic congestion and improving traffic circulation.

Policy CI 3.2: Prioritize street improvements based on current and forecasted service levels. Roadways experiencing or forecast to experience worse than applicable Level of Service conditions (unstable or forced traffic flows) shall require improvements, unless other public health, safety, or welfare factors determine otherwise.

Policy CI 3.3: Reduce traffic congestion at key intersections throughout the City.

Policy CI 3.4: When a traffic study is required for an application for new development, the City will require that the study include an analysis of the appropriate local and collector intersections that may be affected by the proposal.





The FTSSP and the associated Environmental Impact Report analyze traffic circulation within the Project area and traffic impacts created by the Project elsewhere in the City and adjacent communities. The traffic improvements planned for the Project area are designed to limit congestion. The EIR includes mitigation measures to reduce or eliminate congestion, to the extent feasible.

The Traffic Report and EIR also evaluate traffic at intersections within the proposed Project area and intersection impacts created by the Project elsewhere in the City. Mitigation measures have been included within the Project's EIR to ensure that these policies are met to the extent feasible.

Policy CI 4.1: Coordinate local transportation plans with the Solano County Congestion Management Program to ensure eligibility for state and federal funding. (See Policy LU 5.1)

Policy CI 4.2: Coordinate local funding with the Regional Transportation Plan (RTP) to ensure eligibility for maximum available funding under the Regional Transportation Improvement Plan (RTIP).

The FTSSP implements the Solano County Congestion Management Program by emphasizing compact, pedestrian-friendly, and transit-oriented development near a major public transportation hub, reducing the need for private vehicles for regional commuting and movement within the planning area.

In addition, the City of Fairfield has received a "Potential Priority Development Area (PDA)" designation from ABAG and MTC. Current regional planning efforts emphasize the directing regional funds to PDAs, and the City will continue to work with ABAG and MTC staff to obtain regional funds for the Project area.

Policy CI 5.1: Provide off-street parking to employees; however preferential parking at several locations in the City shall be made available to vanpools, carpools and other transit users.

On a case by case basis, the City will work with major employers to determine the need for preferential parking at employment centers. The train station will also accommodate vanpools and carpooling.

Policy CI 5.2: Encourage shared parking facilities for both private businesses and public agencies.

The proposed Project encourages shared parking facilities in mixed-use and commercial areas, such as the Lake Park and library.

Policy CI 5.3: Reserve on-street parking in commercial areas for short-term users.

The City does not have the ability to enforce parking limits outside the City center. However, during project review, adequate employee parking will be designated and provided, and TSM programs will be designed to limit the need for extensive on-street long term parking.

Policy CI 5.4: Work with the various government agencies to provide secure parking at park and ride lots and transit stations.

There are 16 formal park-and-ride locations in Solano County with four in proximity to the Specific Plan Area.

Policy CI 6.1: Encourage the use of carpooling and vanpooling to maintain an acceptable level of service on City streets and highway/interstate facilities.

There are four park-and-ride lots in proximity to the Specific Plan Area.

Policy CI 6.2: Require that all large public and private employers develop TSM plans to encourage their employees to use some form of collective transportation to commute to and from work. These plans should include not only information regarding rideshare lists and available transit, but also provision of transit passes, preferential parking and other incentives to participating employees.

Policy CI 6.3: Implement TSM plans in conjunction with development in order to prevent future traffic congestion in the City.

The above policies are City-wide policy that would be apply on this area, as appropriate.

Policy CI 6.4: Work with Caltrans to implement the use of high-occupancy vehicle (HOV) lanes on I-80 through Fairfield and to study the use of reduced tolls or license fees for carpools and vanpools on highway/interstate facilities.

The I-80 HOV lane has been implemented by CALTRANS in central Fairfield. The FTSSP does not address this issue.

Policy CI 6.5: Coordinate with ride-sharing programs, such as RIDES and Solano Commuter Information, and other transportation agencies to provide up-to-date lists of potential riders and to educate the public on commuting options.

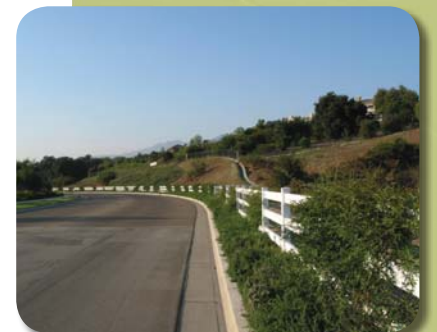
Residents and businesses within the FTSSP area will participate in existing city-wide programs that promote ride sharing. Solano Commuter Information will incorporate information about the new train station in their publications and outreach.

Policy CI 6.6: Encourage the development of employer-funded vanpool and shuttle bus services to new employment centers.

The City will work with employers locating in office and industrial areas within the FTSSP area. Incentives may include parking reductions to encourage the provision of these services.

Policy CI 6.7: Encourage employers to offer flex time arrangements to their employees in order to reduce the percentage of trips made during the peak hours.

Policy CI 6.8: Strive to achieve a 30 percent reduction in the total number of peak period employee trips.





This policy is not applicable because it is a City directed policy.

The City will work with FTSSP area employers to reduce peak hour travel. Incentives may include shared parking, bus pass subsidies, and reduced parking requirements when appropriate.

Policy CI 7.1: Encourage maximum utilization of the existing transit system in Fairfield.

The FTSSP development is designed to take advantage of the new Fairfield-Vacaville Multi-Modal Train Station, which includes a new Amtrak Capitol Corridor passenger rail station, bus transfer facility, and park-and-ride lot to improve accessibility to the Capitol Corridor intercity train service. The City will coordinate its bus and transit system with the train station and major employers and will also provide bus service between the FTSSP and central Fairfield.

Policy CI 7.2: Design transit stops that provide good access to major public facilities and employment centers within the City.

Working with the Fairfield and Suisun Transit (FAST), appropriate transit stops will be provided as development occurs.

Policy CI 7.3: Develop well-defined transit corridors linking all areas of the City. The transit system shall promote the Central Business District as the cultural and social hub for the City. (See Policy LU 2.2)

The proposed Project's Transportation Plan incorporates well-defined transit corridors which will connect with the city's overall transit system. These corridors include facilities for pedestrians and bicyclists. They will provide direct linkages to the Project's commercial and major employment centers, as well as the Lake Park. Local transit will continue to serve Fairfield's Central Business District as well, and the Fairfield Linear Park will provide a direct connection between the Project area and downtown Fairfield. .

Policy CI 7.4: Provide transit services between major employment centers in each area of the City and surrounding residential communities, coordinating transit services with the particular land uses proposed for each center.

The proposed Project's Transportation Plan incorporates well-defined transit corridors linking residences to employment centers, retail, and services.

Policy CI 7.5: Provide express transit service between the western, central and eastern areas of the City, when the demand is sufficient.

Fairfield and Suisun Transit (FAST) continuously evaluate transit demand and service. The higher density development planned for the FTSSP area may generate substantial new demand for transit service. It would be connected to central Fairfield via express service as development occurs and demand rises. The new train station will serve as a focus for regional transit users who may provide another customer base for express service to and from central Fairfield.

Policy CI 7.6: Integrate regional transit with local transit to make the entire system more user-friendly. Coordinate the integration of local and regional transit with the Solano County Transportation Authority and other cities.

The FTSSP development is designed to take advantage of the new Fairfield-Vacaville Multi-Modal Train Station, which includes a new Amtrak Capitol Corridor passenger rail station, bus transfer facility, and park-and-ride lot to improve accessibility to the Capitol Corridor intercity train service. STA is a partner in this program.

Policy CI 7.7: Coordinate with regional transit planners to determine the feasibility of developing fixed guideway systems for interregional passenger traffic, making use of existing rail lines whenever possible.

The FTSSP development is designed to take advantage of the new Fairfield-Vacaville Multi-Modal Train Station, which includes access to the Capitol Corridor intercity train service.

Policy CI 7.8: Continue to support efforts to expand ridership on the Capital Corridor rail line between Sacramento and the Bay Area.

The FTSSP development is designed to take advantage of the new Fairfield-Vacaville Multi-Modal Train Station, which includes a new Amtrak Capitol Corridor passenger rail station, bus transfer facility, and park-and-ride lot to improve accessibility to the Capitol Corridor intercity train service. The 6,800 new housing units planned for north-eastern Fairfield will provide a substantial new customer base for the train service.

Policy CI 7.9: Encourage studies examining the feasibility of extending Bay Area Rapid Transit (BART) to the City. Support the development of potential routes to the current system and the analysis of potential station locations in the City (e.g. central Fairfield or Cordelia).

BART service is unlikely to be expedited by this Project alone. However, Capital Corridor service at the core of the Project will encourage interest in regional and long distance transit.

Policy CI 8.1: Participate in any regional studies for additional airport facilities. (See Objective LU 5)

Airport expansion is not an element of the FTSSP Project. However, through providing substantial new development land, the Project preserves land nearby which will ensure the viability of Travis Air Force Base, including the potential for future for commercial joint use at the Air Force base.

Policy CI 9.1: Expand the City's north-south and east-west bikeway network through the use of Class I, II and III bikeways in accordance with the factors outlined in Table CI-1.

A mix of Class I and Class II bicycle paths are proposed in the FTSSP roadway network to address the needs of the proposed Project. FTSSP



will coordinate with the City to provide points of access to present and anticipated future regional trails

A Class I bike trail will run through the western portion of the Project site and will continue along the roadway alignment until New Canon Road. On-street bike paths are planned along the following arterials: New Canon Road, Peabody Road, and Cement Hill Road. They will also extend into the industrial areas of the Specific Plan Area, as well as from Center Elementary School to Vanden High School. Off-street bike paths are planned along Vanden Road as well as on many of the connector roads throughout the FTSSP area.

Policy CI 9.2: Provide bikeways which link residential areas with major employment centers, parks, opens space areas and other recreational amenities, educational facilities, and commercial areas.

FTSSP will create a compact project that provides a diverse mix of housing, shopping, employment, cultural and recreational opportunities to encourage walking, bicycling and other alternative transportation trips as well as shortening the length of required vehicle trips. In addition, FTSSP will create a mobility infrastructure system that integrates a range of transportation modes, such as providing pedestrian pathways and Class I and II bicycle paths, to reduce dependence on the automobile and vehicle trips and miles travelled.

Policy CI 9.3: New arterial streets should provide adequate right-of-way width for Class I Bike Paths or Class II Bike Lanes where appropriate.

The FTSSP development's Transportation Plan includes a cohesive bicycle path system that will enable residents to access the Project's public transportation facilities, the Lake Park, the numerous parks and other public amenities, consistent with the safety and sensitive resources protection priorities. The development standards for the Specific Plan include adequate right-of-way width for Class I and Class II bike paths, where appropriate.

Policy CI 9.4: Class III Bike Routes shall function as a linkage between Class I and Class II bikeways and be designated on roads where adequate roadway width does not exist for a separate lane or path.

Class III bike paths are not included in the FTSSP.

Policy CI 9.5: Continue to provide multi-use trails which accommodate pedestrian and bicycle use where appropriate and work toward providing separate trail facilities for pedestrian and bicycle use. (See Policy OS 11.2)

The FTSSP development will include an extensive network of pedestrian pathways and a cohesive bicycle path system. A connector with multi-use trails and on-street bike lanes is designed for the proposed Project. A Class I bike trail will run along the western portion of the site and will continue along the roadway alignment until New Canon Road. A multi-use trail is proposed to extend from the northern portion of the site, circle the Lake Park, and connect back to the Class



I bike trail. The multi-use trail will also extend from the Class I bike trail towards Center Elementary School.

Policy CI 9.6: Cooperate with surrounding jurisdictions and regional agencies to establish a countywide bikeway network throughout Solano County which provides linkages with regional networks. (See Policy OS 11.5 and Objective LU 5)

The FTSSP development will include an extensive network of pedestrian pathways and a cohesive bicycle path system. This will include connections to the Fairfield Linear Park, a regional bicycle facility.

Policy CI 9.7: Establish bicycle safety as a priority through on-going public education.

Project students will receive ongoing education programs on bicycle safety at Travis USD.

Policy CI 9.8: Minimize bicycle/pedestrian/motor vehicle conflicts by providing proper trail, street and intersection design and separation.

The FTSSP development will include an extensive network of pedestrian pathways and a cohesive bicycle path system designed to share the road and to minimize conflicts. Please refer to Section 10.0, Transportation Plan, of the FTSSP for more information.

Policy CI 9.9: Establish funding mechanisms for construction and maintenance of bicycle facilities, and bicycle education and enforcement programs.

As the Project develops, fees and infrastructure construction will complete key bicycle facilities. .

Policy CI 9.10: Actively enforce bicycle traffic violations to promote the safe and proper use of bicycles in the community.

Citywide efforts to ensure safe bicycling will continue in the Project area.

Policy CI 9.11: Encourage bicycle storage and support facilities in major commercial office and business park developments to encourage the use of bicycles for commuting.

As individual commercial and industrial buildings are developed in the Project area, bicycle lockers and support facilities can be integrated into the new buildings. The Train Station will have bicycle lockers. .

Policy CI 9.12: Require that all multi-family, commercial, educational and public facilities provide bicycle racks to facilitate bicycle parking.

This requirement would continue to be implemented through the Development Review process on a case by case basis.

Policy CI 9.13: Require that all Transportation System Management (TSM) plans address bicycling as a viable alternative mode of transportation and provide incentives for bicycle use.





As a citywide policy, this requirement would continue to be implemented through the Development Review process on a case by case basis.

Policy CI 9.14: Integrate bicycle use with public transit use.

The FTSSP development will include a cohesive bicycle path system that is integrated with the overall Transportation Plan, including public transit. Bicycles are currently allowed, space permitting, on the Capital Corridor trains and local FAST busses.

Policy CI 10.1: Provide pedestrian facilities that are safe and pleasant to use. (See Policy UD 3.2)

Policy CI 10.6: Require new commercial and residential developments to provide walkways that are safe and pleasant to the user.

The FTSSP development will include an extensive network of pedestrian pathways that will enable residents to access the Project's public transportation facilities, the Lake Park, the numerous parks and other public amenities, consistent with the safety and sensitive resources protection priorities. Design standards will ensure that these facilities are safe and pleasant to use through landscaping, pathway placement, and physical separation where appropriate.

Policy CI 10.2: Implement street standards that include sidewalk or walkways on both sides of streets, where appropriate.

City development standards require sidewalks, including on both sides of the street where appropriate.

Policy CI 10.3: Consider using landscaping or physical barriers on high-capacity arterials to separate vehicles and pedestrians. (See Policy UD 3.2)

The roadway cross sections in Chapter 10.0 of the FTSSP document identifies parkways, sidewalks, regional trails, landscape buffers, multi-use trails, and bike lanes in relation to the roadways.

Policy CI 10.4: Consider constructing pedestrian overpasses where heavily travelled pedestrian routes cross busy intersections.

The FTSSP does not include construction of pedestrian overpasses.

Policy CI 10.5: Design access ways to school facilities that will ensure the safety of children.

The Central Elementary School will be accessible via safe sidewalks and separated bicycle and multiuse paths.

Policy CI 10.7: Encourage existing facilities and require future facilities to provide access to disabled persons.

The FTSSP development will meet all City, federal, and State standards to provide access to disabled persons.

Policy CI 10.8: Encourage the location of basic shopping and services within approximately 1,300 feet of residential and industrial areas.

FTSSP will create a compact development that provides a diverse mix of housing, shopping, employment, cultural, and recreational opportunities to encourage walking, bicycling and other alternative transportation trips as well as shortening the length of required vehicle trips.

Policy CI 11.1: Road and highway accident records shall be maintained to monitor the safety of local facilities.

This City-wide policy will continue to be implemented in the FTSSP area.

Policy CI 11.2: Route roadways in careful relationship to adjoining land uses to minimize noise, visual, and other impacts.

Project design will consider all of these factors. Roadways generating high noise levels will

Policy CI 11.3: Discourage through-traffic in residential areas.

Design review will ensure subdivisions integrate the objective of discouraging through traffic while providing neighborhoods which are well integrated into the overall community. Providing multiple travel paths and modes will reduce the negative impacts of through traffic.

Policy CI 11.4: Provide for adequate spatial separation and landscaping for development along freeway rights-of-way.

This policy is not applicable because it is not located along a freeway right-of-way. Major roadways (Peabody Road and Vanden Road) will feature significant landscape buffers where appropriate to reduce negative impacts.

Policy CI 12.1: Cooperate with the regional air quality planning agency (the Bay Area Air Quality Management District) and agencies that deal with issues directly related to air quality, such as the Association of Bay Area Governments (ABAG) and the Metropolitan Transportation Commission (MTC), in the development and implementation of regional air quality strategies. (See Objective LU 5, Policy LU 5.1, Policy OS 8.1, and Policy OS 8.2)

Compact development as envisioned in the FTSSP will help reduce the City's contribution to regional air quality problems. .

Policy CI 12.2: Support subregional coordination with other cities, counties and planning agencies concerning land use, jobs/housing balance and transportation planning as a means of improving air quality. (See Objective LU 5, Policy LU 5.1, and Policy OS 8.2)

This Project has been coordinated with STA, ABAG, MTC, and other agencies. The mixed use, high density community will also incorporate land for major employment-generating uses, reducing the need for long distance community and maintaining Fairfield's job/housing balance.



Policy CI 12.3: Balance jobs and housing in future development to provide Fairfield residents the opportunity to work in the City near their homes and reduce long-distance commuting either to or from Fairfield. Jobs should be balanced (to the maximum extent possible) both in numbers and in salary range/housing cost. (See Objective LU 4)

The compact Project will provide a diverse mix of housing, shopping, employment, cultural and recreational opportunities to encourage walking, bicycling and other alternative transportation trips as well as shortening the length of required vehicle trips.

Policy CI 12.4: Support the expansion and improvement of transit systems and ride sharing programs to reduce the production of automobile emissions. (See Policy OS 8.3)

The FTSSP development is designed to utilize the new Fairfield-Vacaville Multi-Modal Train Station, which will include a new Amtrak Capitol Corridor passenger rail station, bus transfer facility, and park-and-ride lot to improve accessibility to the Capitol Corridor intercity train service.

Policy CI 12.5: Minimize the number, properly space, and interconnect traffic signals in order to minimize the acceleration/deceleration that produces significantly higher vehicular emission levels.

Project design will minimize the number of required traffic signals through street layout and location. .

A.3 Economic Development Element

Policy ED 1.1: Target and attract industries and businesses which contribute to diversification and stabilization of the local economy. (See Policy LU 14.2)

The FTSSP area includes significant land for employment-generating light industrial uses. This development will contribute to the City's jobs-housing balance and will provide jobs within walking or bicycling distance of high density housing and the new train station.

Planning Area 6 is the employment hub for the Fairfield Train Station Community. It will include a Light Industrial/Employment zoning designation designed to support the campus-style development of light industrial with business park, research and development (R&D), manufacturing, and service uses. It is envisioned to include a rich mix of employers including incubator new business, multi-tenant commercial, light industrial, and service retail. The Commercial Mixed-Use zoning designation applies to areas around the Town Center and would allow for a range of uses that may include retail, neighborhood services, small office, live/work units, and residential above commercial buildings.

Policy ED 1.2: Encourage through special programs, including financing, the expansion and attraction of small and medium size firms with good growth potential.



The City's ongoing economic development efforts will be extended to the FTSSP area. The land use plan for the Project will support a variety of building types-including those suitable for smaller and medium size firms. The industrial areas will provide a good location for expanding firms looking for larger spaces .

Policy ED 1.3: Identify and mitigate, where appropriate, obstacles to the formation and expansion of local businesses.

The City's economic development programs address this issue throughout the City. The FTSSP will assist in these efforts by providing affordable land for new commercial and industrial development and a substantial supply of land zoned for housing suitable for employees.

Policy ED 1.4: Encourage, through implementation of the Zoning Ordinance, office and professional home occupations. (See Policy LU 13.5)

This is a general policy which applies throughout the City and will apply to residential units in the planning area.

Policy ED 1.5: Support new business development which does not overburden infrastructure capacities. Provide adequate infrastructure capacity to support new business development, while not negatively impacting operations of the existing business base. (See Objective LU 4 and Policy LU 4.2)

The infrastructure plan for the FTSSP area will ensure that there is adequate capacity for new industrial and office development. The Project EIR provides further information on this topic, In addition, project review will ensure that unusual or unanticipated impacts are mitigated through fees or the actual construction of new infrastructure by the developer.

Policy ED 1.6: Recruit and retain a diversity of businesses and industries which meet the skill levels of the community's broad labor pool. At the same time, work with the various job training and educational agencies to develop and match the skill levels of the labor pool to the needs of the economy as a whole.

As noted above, the planned commercial areas will facilitate the development of a broad range of businesses. The city's economic development team will continue to work with training and educational programs to extend their efforts to the FTSSP area.

Policy ED 1.7: Facilitate entitlement processing for all businesses, large, medium, or small, which conform with the objectives in this Element.

Adoption of this plan facilitates this Policy in that the Specific Plan provides substantial opportunities for new development to occur in a well-planned, balanced community.

Policy ED 1.8: Development incentives should only be used for projects that conform with the Economic Development Element, provide

significant private capital investment and/or create a large amount of jobs and public revenues.

The commercial and industrial development envisioned in the FTSSP will complement and support the goals of the Specific Plan and the General Plan more generally. The Specific Plan has reserved two large areas for industrial park development which will create jobs and public revenues.

Policy ED 1.9: Continue to support the role of Travis Air Force Base as a significant contributor to the economic base of the community. Discourage development which could negatively impact the Base. (See Policy LU 13.3, Phasing Area D Policy 24, Phasing Area E Policies 1, 8-11, Policy CI 8.1, Objective HS 5, and Policy HS 9.3)

This Specific Plan will indirectly implement this Policy by providing a clear land use plan for the area nearest Travis Air Force Base, reducing speculative pressure on other land outside the plan area. Areas outside the Specific Plan will continue to remain within the "Travis Reserve."

Policy ED 1.10: In the event of a change in the status of Travis Air Force Base, work to create joint use or commercial use of the airport. (See Objective CI 8)

The industrial development within the Specific Plan area would complement any commercial or joint use program.

Policy ED 1.11: Support development projects which broaden and enhance the City's economic base and produce viable long-term land use patterns.

The City's economic base will be expanded by the proposed Project through a variety of revenue streams, such as property taxes, licenses and impact fees. Planning Area 6 constitutes the employment center for the development and will include a rich mix of employers including incubator business, multi-tenant commercial, light industrial, local service retail, with potential to include corporate headquarters, research and development (R&D) and Class A office uses along with residential uses. Planning Area 2 will be a mixed-use district that will function as the Fairfield Train Station Community's "downtown." The Town Center will include retail, commercial and residential uses.

Policy ED 2.1: Protect existing businesses within areas designated by the General Plan for existing or future industrial and commercial uses and discourage encroachment by noncompatible uses. Encourage the preservation and expansion of existing industrial uses in areas designated as industrial. (See Policy LU 13.2, Program LU 13.2 A, Policy LU 13.4, and Policy LU 14.1)

This policy is not applicable because it pertains to existing businesses in older parts of the City.

Policy ED 2.2: In order to provide a stable economic base, provide sufficient tracts of land at a variety of sizes available for industrial and commercial uses. (See Policy LU 14.1, Policy LU 14.2)

The proposed Project's objective is to provide opportunities for new jobs within industrial development east of the railroad and retail and office employment west of the railroad. Commercial uses would also be provided in the Town Center and Lake Park.

Policy ED 2.3: The Zoning Ordinance shall be implemented to recognize and provide for ongoing and long term changes in business operations and market trends. For example, some facilities may be permitted to include combinations of manufacturing, distribution, wholesaling, and limited retail on one site. The integrity of both the overall area and project must be protected. (See Policy LU 13.4 and Program LU 13.4 A)

Policy ED 3.2: As the community grows, marketing efforts shall be targeted to attract uses which will complement the City's economic development objectives.

Planning Area 6 is the employment hub for the Fairfield Train Station Community. It will include a Light Industrial/Employment zoning designation designed to support the campus-style development of light industrial with business park, research and development (R&D), manufacturing, and service uses. It is envisioned to include a rich mix of employers including incubator new business, multi-tenant commercial, light industrial, and service retail. The Commercial Mixed-Use zoning designation applies to areas around the Town Center and would allow for a range of uses that may include retail, neighborhood services, small office, live/work units, and residential above commercial buildings.


Policy ED 2.4: Areas shall be identified in the General Plan to provide for a concentration of industrial and business uses. Areas shall be designated to provide for primarily manufacturing uses (such as the Tolenas Industrial Park), areas which allow a mixture of uses (such as the Solano Business Park), and areas which are primarily office, research and development uses (such as Fairfield Corporate Commons).

The proposed Project's objective is to provide opportunities for new jobs within industrial development east of the railroad and retail and office employment west of the railroad.

Policy ED 2.5: To minimize traffic impacts, industrial and business activity should be located in areas close to major transportation. Such areas should have room for expansion and sufficient buffers to prevent conditions of potential incompatibility with surrounding uses. (See Policy LU 13.2)

The proposed Project's objective is to provide opportunities for new jobs within industrial development east of the railroad and retail and





office employment west of the railroad. The proposed FTSSP roadway network includes multiple connections to the key arterials traversing the site, Peabody Road and Cement Hill Road/Vanden Road, and an internal network of two-lane roadways (with additional turn lanes where needed at intersections) that will efficiently distribute traffic while providing a high level of mobility for vehicles, bicycles, pedestrians and transit vehicles.

Policy ED 3.1: Retail uses which increase the City's revenues by expanding the community's regional retail market share should be attracted to the community. They should not create unmitigable levels of traffic congestion or other undesirable impacts.

The Specific Plan includes commercial, light industrial, office, and research and development (R&D) land uses, thus creating a variety of employment and economic opportunities for new residents of the Project as well as existing residents of the City and the surrounding region. A traffic study was prepared as part of the EIR to address potential traffic congestion related to land uses within the Fairfield Train Station Community.

Policy ED 3.3: In the Downtown area, retail, office, financial, government, and professional uses shall be encouraged to expand the current business mix and increase the area's economic viability as a destination point for shopping, work, dining, and entertainment. (See Policy LU 2.2)

This policy applies to the City's existing traditional downtown. However, the FTSSP provides a unique opportunity to create a second "town center" for the community. The Town Center District is considered the "downtown" area of the Fairfield Train Station Community. It integrates residential, commercial and recreational spaces in a central urban node that embodies the traditional town crafting, smart-growth and TOD principals of the plan. Occurring directly across from the Train Station, this District has the unique opportunity to establish a high-density residential and commerce corridor where business, entertainment, community festivals and daily life can coincide with traditional neighborhood design to reduce the need for vehicle trips. The design and integration of the land use has been carefully planned to function in support of the Train Station, to increase accessibility to transit, provide high-quality public spaces, decrease vehicle trips and increase quality of life.

Policy ED 3.4: Assist in the retention and expansion of existing businesses which conform with the goal and policies of this element by providing and coordinating available financial and non-financial resources. Examples are financing and loan programs, cooperation with lenders, providing capital loans, technical assistance, and counseling.

While this policy applies throughout the City, implementation of the FTSSP will provide new opportunities for the expansion of existing businesses in Fairfield. The employment centers will provide additional land for office and industrial users interested in developing new facilities.

Policy ED 4.1: New commercial land use designations shall be of sufficient size and shape to meet existing and future market and service needs of the overall area in which they are located. (See Policy LU 14.3)

The FTSSP designate several commercial areas which will together meet the varying needs of the train station community:

- *Community Commercial (CC) with 12 acres of service-intensive larger-scale development within the Town Center District*
- *Mixed-Use Commercial (CM) with 12 acres of mixed office, retail, and neighborhood services within the Town Center District*
- *Neighborhood Commercial (CN) with 12 acres of smaller-scale, neighborhood service and boutique retail within the Lake Park District*

Policy ED 4.2: New commercial areas are encouraged to cluster in identified areas to prevent and discourage strip development. Where appropriate, locate commercial uses at focal points along major arterial streets or expressways. (See Policy LU 14.4)

New commercial areas are clustered within the two focal districts of the Fairfield Train Station Community – the Town Center District and the Lake Park District. Commercial areas are located along Main Street and iconic streets within these districts to provide a diverse range of businesses at a pedestrian-scale.

Policy ED 4.3: The location, size, scale, and design of neighborhood commercial uses shall complement and meet the needs of the surrounding neighborhood. The neighborhood concept of providing pedestrian, bicycle, and other non-motorized access shall be encouraged. (See Policies UD 3.2 and UD 3.3 and Objective CI 10)

The daily needs of the Fairfield Train Station Community residents will be met by commercial spaces of a pedestrian-scale and may include such diverse businesses as grocers, pharmacies, florists, personal services, gas stations, food services, laundry services, independent businesses or offices, and financial institutions. Accessibility, including non-motorized accessibility, pedestrian scale and site/building design are of great importance to the success and suitability of these commercial developments.

Policy ED 4.4: To minimize traffic generation impacts, new commercial development shall be located to meet the needs and convenience of the customer base and promote compatibility between land uses. (See Policy CI 10.8)





The Project's compact and mixed-use design will promote the reduction of vehicle trips and vehicle miles travelled as commercial services will be available to residents within the Project Site. All the elements of the Town Center District have been designed and sited to serve as an activity catalyst to promote multimodal transportation and shorter vehicular trips.

Policy ED 5.1: Encourage a mix of office and professional uses targeted to different areas throughout the community. Back office projects such as data processing, financial services, and other such uses, shall be encouraged to locate in a business park environment, while professional offices should be encouraged in the urban core areas such as Downtown, Gateway, the Northeast Activity Center and other identified areas.

The FTSSP has designated Planning Area 6 (PA6) as an employment hub for the Fairfield Train Station Community suitable for back office uses and manufacturing. Strategically located in the eastern portion of the site, PA6 will include the FTSSP Light Industrial/Employment zoning designation designed to support the campus-style development of light industrial, research and development (R&D), manufacturing, and service uses. PA6 is envisioned to include a rich mix of employers and building types, including incubator space for new businesses and flexible multi-tenant space for heavy commercial and light industrial and limited retail. With its strategic location between Fairfield, Vacaville and Travis Air Force Base and high visibility, the Planning Area also has the potential to attract high end corporate headquarters, and Class A office uses. Business park and campus-type uses that integrate office, research and manufacturing into will be pursued.

Professional offices will be concentrated in the Town Center District and Lake Park Districts. These uses will complement the community-serving retail in these urban cores.

Policy ED 5.2: Provide for ancillary commercial development in business parks, such as banks and restaurants, in order to provide amenities necessary to attract office uses in these areas.

The zoning for Planning Area 6 (PA6, the employment hub for the Fairfield Train Station Community, will permit ancillary commercial development that supports and benefits from proximity to employment-generating uses.

Policy ED 5.3: Encourage clustered, smaller scale office and professional uses along major streets and in neighborhood centers in a variety of areas dispersed throughout the community to meet the needs of nearby neighborhoods.

New commercial areas are clustered within the two focal districts of the Fairfield Train Station Community – the Town Center District and the Lake Park District. Commercial areas are located along Main Street and iconic streets within these districts to provide a diverse range of businesses at a pedestrian-scale. The Neighborhood Commercial (CN)

designation provides smaller-scale, neighborhood service and boutique retail within the Lake Park District.

Policy ED 5.4: As part of an overall economic development strategy, attract and retain a variety of office and professional uses to not only meet the needs of the community, but to establish a strong office market base servicing the regional and sub-regional area.

A mix of office and professional uses are planned for the Town Center District as well as the Lake Park District, including Community Commercial (CC) with 12 acres of service-intensive larger-scale development within the Town Center District, to Mixed-Use Commercial (CM) with 12 acres of mixed office, retail, and neighborhood services within the Town Center District, and Neighborhood Commercial (CN) with 12 acres of smaller-scale, neighborhood service and boutique retail within the Lake Park District.

In addition, Planning Area 6 (PA6) will serve as an employment hub for the Fairfield Train Station Community.

Policy ED 6.1: Coordinate economic development planning with land use planning.

Through the designation of varied commercial districts and a major office campus style employment center, the land use plan for the FTSSP facilitates economic development in the planning area, with opportunities for new jobs, and a variety of employment and economic opportunities.

Policy ED 6.2: In areas such as the Downtown and the Fairfield-Vacaville Train Station area, encourage mixed-use development that provides opportunities for a jobs and housing balance at the community, neighborhood, and project level.

The proposed Project is expected to add approximately 4,000 jobs and 16,276 additional residents within the City of Fairfield. Although there is no guarantee that residents would work locally and vice versa, since the Specific Plan would help to create a more balanced jobs/housing index, the City does not anticipate that there would be a substantial adverse change related to the jobs/housing balance. Planning Area 2 is planned as a mixed use "Town Center" and is envisioned as the FTSSP community's "downtown".

Policy ED 7.1: Encourage economic development activities which provide, either through initial stages or through expansion, the opportunity for employment of local residents and/or increased municipal revenues.

The FTSSP permits a variety of commercial, light industrial, and office land uses, thus creating diverse employment and economic opportunities for new residents of the proposed Project as well as existing residents of the City of Fairfield and the surrounding region. PA 6 will provide space for industrial and large scale office users to locate new or expanded facilities in Fairfield. Together, these industrial

and commercial users will generate substantial municipal revenues from sales taxes and property taxes.

Policy ED 7.2: Encourage industrial or business uses which create large numbers of new, well paying jobs, and maximize private capital investment based upon the size and scope of the project.

Planning Area 6 will provide land for larger employment-generated uses. In addition, the Town Center and Lake Park communities will provide for professional office uses that complement and support the community.

Policy ED 7.3: Preserve and enhance the community's assets and character, including education, agriculture, open space, recreational amenities, cultural amenities, and other factors, which make the community an attractive area to live, work, and invest. (See Objective LU 1 and Objective PF 1)

The FTSSP develops a community form that reflects regional character and heritage, is rich with memorable places and implements sustainable planning and design.

Policy ED 7.4: Require an equitable sharing of the full cost of public improvements between the public and private sectors. Improvements specifically and originally attributed to a single development, development area, or business shall be borne by those entities. (See Policy LU 4.2, Phasing Area D Policies 6 and 10, Phasing Area E Policy 5, Objective PF 2, and Policies PF 2.1, PF 2.2, and PF 2.3)

The FTSSP includes a full accounting of all public improvements required to support the Project. A Development Agreement will specify by whom, how, and when infrastructure will be constructed.

Policy ED 7.5: Work to remove impediments to gainful employment, such as lack of transportation, child care, job training, vocational education, and other factors. Coordinate efforts with local, state, federal, and private agencies/organizations. (See Program HO 7.3 A)

Residents of the FTSSP will benefit from existing local and County programs designed to remove these impediments.

Policy ED 7.6: Encourage increased cooperation between the public and private sectors, such as the Chamber of Commerce, plant managers, and business associations, in formulating economic development plans and programs.

Employers in the FTSSP area will be eligible to participate in existing City, County, and regional strategic planning for economic development.

Policy ED 7.7: Discourage businesses that have a net negative impact to the community. Businesses shall create minimal and/or mitigable levels of noise, fumes, odors, hazardous waste, traffic and other

negative factors for the community as a whole and their location in particular.

The FTSSP envisions high quality commercial and industrial areas which minimize these impacts. Impacts will be addressed in more detail as specific users propose new facilities. .

Policy ED 7.8: Support a strong, quality local education system. (See Objective PF 20)

Development within the FTSSP will pay school impact fees to Travis Unified School District to insure that local schools provide a quality education and facilities. An elementary school is proposed within Planning Area 2. The City will work with Travis Unified School District to ensure available capacity to serve the Project's student projections.

Policy ED 8.1: Cooperatively work with farmers, property owners, universities, colleges, and agricultural organizations and agencies to enhance the viability of agricultural activities and uses.

Policy ED 8.2: Coordinate open space preservation and conservation programs with economic development programs to enhance agriculture.

Policy ED 8.3: Development shall not encroach upon or consume productive cropland in areas such as the Suisun Valley. (See Policy OS 1.4 and Policy OS 1.5)

There is limited productive cropland in the vicinity of the FTSSP. However, as part of the FTSSP, a permanent greenbelt will be preserved between Fairfield and Vacaville. Agricultural uses will be encouraged and permitted within the greenbelt.

Policy ED 8.4: Attract industry that complements and supports the local agricultural economy.

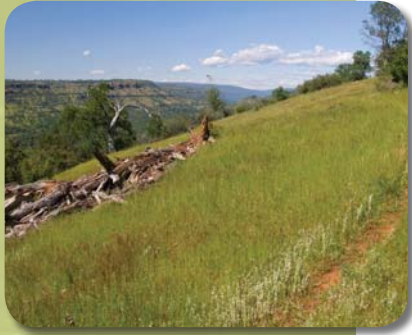
The City has an active program to pursue industries that support local agriculture and food processing activities. Such agricultural processing industries will be able to locate within the FTSSP's planned employment center.

Policy ED 9.1: Provide for and encourage the maintenance and long term revitalization of existing commercial areas, such as Texas and North Texas Streets. Where appropriate, incentives shall be used to encourage and assist the private sector to maintain and revitalize these areas.

The FTSSP addresses a new development area. As commercial uses planned within the FTSSP will primarily serve local residents and commuters, there will be limited impact on older commercial districts in central Fairfield.

Policy ED 9.2: Leverage redevelopment funds and private funds by targeting federal, state, and local resources to the existing commercial areas.





The proposed Project is a new planning area. In addition, the FTSSP is not a redevelopment project area.

A.4 Health and Safety Element

Policy HS 1.1: Existing buildings, structures, and walls within the City shall meet minimum seismic safety standards.

Policy HS 1.2: All new buildings, structures, and walls shall conform to the latest seismic and geologic safety structural standards of the California Building Code as a minimum standard.

This Plan addresses new construction. Design and construction of all new buildings will be in accordance with the California Building Code.

Policy HS 1.3: Comply with the requirements of State law and the recommendations of a certified geotechnical consultant when determining setbacks from an active fault trace for new development.

The Specific Plan Area is not located within an Alquist-Priolo Earthquake Fault Zone. However it is located in a seismically active area. If a fault investigation confirms the segment of the Vaca Fault through the Project site is active (Holocene), potentially active, or the investigation is inconclusive, then it may be necessary to establish a structural setback zone. . FTSSP will implement proper seismic design to minimize exposure to hazards caused by strong seismic strong shaking and potentially to hazards from surface rupture of the Vaca Fault.

Policy HS 1.4: Require detailed geologic studies by a Registered Geologist (RG), Certified Engineering Geologist (CEG), and/or Geotechnical Engineer for projects within areas of potential seismic activity. All studies prepared shall identify the location of all surface fault traces within 100 feet of any proposed structure and determine their relative activity. Adequate provisions for mitigation of potential hazards to human life or property shall also be included.

A geotechnical report was prepared in 2009 by ENGEO. In addition, before building permits are issued and construction activities begin on any project development phase, the project applicant(s) of each project phase will hire a licensed geotechnical engineer to prepare a final geotechnical subsurface investigation report at a design level for the project facilities, which shall be submitted for review and approval to the City.

Policy HS 1.5: The City shall restrict the crossing of Alquist-Priolo Act Special Studies zones by new public and private transmission facilities, including power, water, sewer, gas, and oil lines. Owners of all existing transmission facilities which cross active faults shall be required to file an operations plan with the City describing the probable effects of

transmission line failure at the fault and various emergency facilities and procedures which exist to assure that failure does not threaten public safety.

Policy HS 1.6: In the event that a transmission facility crosses an Alquist-Priolo Act Special Study Zone, facility design shall include sufficient provisions for valves, switches and other appropriate equipment for minimizing adverse impacts to nearby development from fire, disruption of service, spillage, etc. as a result of fault displacement.

Policy HS 1.7: Any existing natural gas well which feeds pipelines crossing an Alquist-Priolo Act Special Study Zone shall be equipped with adequate emergency devices to shut off gas flows in the event of pipeline rupture due to fault displacement.

Policy HS 1.8: Any existing facility which is located within the boundary of a Special Study Zone and attracts large number of people, is open to the public, or provides essential community services, shall be investigated by a structural engineer for potential hazards to life and property due to fault displacement. Cost of such services shall be borne by the building owner. If hazards are identified, appropriate hazard mitigation actions, subject to City approval, shall be implemented.

The Specific Plan Area is not located within an Alquist-Priolo Earthquake Fault Zone. It is unlikely that new utilities directly serving the planning area will cross a fault zone.

Policy HS 1.9: The City should retain a Registered Geologist, Certified Engineering Geologist and/or Geotechnical Engineer to evaluate geologic reports required where seismic conditions warrant special attention. The cost of such services shall be borne by the applicant.

A geotechnical report was prepared for the project by ENGEO in 2009. As specific development projects come forward for review, further geological studies will be completed as required.

Policy HS 1.10: Geologists who conduct studies along the upper Cordelia Fault shall contact the California Division of Mines and Geology for early input prior to finalizing the status of this portion of the fault relative to the Alquist-Priolo Special Study Zone.

This policy is not applicable because the Project is not located near the Cordelia Fault.

Policy HS 2.1: No critical structures such as utilities, communications facilities, hospitals, emergency relief facilities, high occupancy structures, and fire and police stations shall be located in areas of high ground failure potential.

FTSSP will implement proper seismic design to minimize exposure to hazards caused by strong seismic strong shaking and potentially to hazards from surface rupture of the Vaca Fault. Critical structures will not be located in areas of high ground failure potential.

Policy HS 2.2: Require soils and geologic studies by qualified professionals for development within Slope Stability Zone 2 identified on Exhibit HS-1.

Before approval of subdivision improvement plans within the Specific Plan and off-site infrastructure required to support Specific Plan buildout, FTSSP shall hire a licensed geotechnical engineer to prepare a final geotechnical subsurface investigation report at a design level, including analysis of slope stability, which shall be submitted for review and approval to the City.

Policy HS 2.3: Require detailed geologic studies and detailed mitigation measures for development projects located within Zone 3 and Zone 4 of Exhibit HS-1. Property insurance for development in Zone 3 and Zone 4 should be considered as a potential risk mitigation measure.

Before approval of subdivision improvement plans within the Specific Plan and off-site infrastructure required to support Specific Plan buildout, FTSSP shall hire a licensed geotechnical engineer to prepare a final geotechnical subsurface investigation report at a design level, including analysis of slope stability, which shall be submitted for review and approval to the City.

Policy HS 2.4: Development is discouraged on slopes in excess of twenty (20) percent and/or unstable soils.

No development is proposed in slopes exceeding 20 percent.

Policy HS 2.5: Require strict engineering standards for construction on soils subject to significant shrink/swell and areas of high ground failure potential.

A geotechnical investigation report will be prepared and will include design recommendations to reduce damage from expansive soils. A geotechnical or soils engineer will provide on-site monitoring to make sure that earthwork is being performed as specified in the plans.

Policy HS 2.6: Require strict engineering standards for development projects located in identified landslide prone areas.

No development is proposed in identified landslide-prone areas.

Policy HS 2.7: Require a detailed geotechnical report, including borings, for projects involving construction on soils and substrate subject to potential liquefaction, and implement the recommendations of the report by making them condition of project approval.

A site-specific design level geotechnical investigation would be performed by a licensed geotechnical engineer. It would include a determination of liquefaction potential as required by the CBC, and all recommendations made by the engineer regarding building and foundation design would be implemented.

Policy HS 2.8: Require an erosion control and rehabilitation plan to be prepared for projects requiring substantial groundbreaking activities to

control short-term and long-term erosion and sedimentation in nearby streams and rivers.

Construction activities during project implementation would involve grading and movement of earth over a large area in soils subject to wind and water erosion hazard and on slopes. Therefore, before grading permits are issued, the project applicant(s) of each project phase shall retain a California Registered Civil Engineer to prepare a grading and erosion control plan.

Policy HS 3.1: Work with other jurisdictions to ensure stability of dams at Madigan, Frey, and Curry Lakes where dam failure could result in property damage or personal risk. (See Policy PF 8.4)

This policy is not applicable. .

Policy HS 3.2: Require development within flood plain areas to comply with FEMA regulations by providing adequate flood mitigation and financial protection in the event of flooding. (See Policy PF 8.1)

Specific project design standards listed in Section 4.9, Hydrology and Water Quality, of the FTSSP EIR would provide flood protection to meet FEMA 100-year flood protection criteria.

Policy HS 3.3: Investigate potential for earthquake-induced liquefaction of Putah South Canal. Areas of special concern include where Putah South Canal crosses Ledgewood Creek on Suisun Valley Road in Solano County, where it crosses Laurel Creek in north Fairfield, and those portions of the canal constructed on landfill along the base of Cement Hill.

The FTSSP and associated Draft EIR do not specifically address the issue of liquefaction of the Putah South Canal, which traverses the northwest corner of the Specific Plan Area. Before approval of construction plans potentially impacted by Putah South Canal liquefaction, the developer will be required to hire a licensed geotechnical engineer to prepare a final geotechnical subsurface investigation report at a design level, including analysis of liquefaction, which shall be submitted for review and approval to the City.

Policy HS 3.4: Continue to update FEMA flood control studies based upon development and ongoing improvements. (See Program PF 9.4 A)

Specific project design standards listed in Section 4.9, Hydrology and Water Quality, of the FTSSP EIR would provide flood protection to meet FEMA 100-year flood protection criteria.

Policy HS 3.5: Development that interferes with channel capacity or causes erosion and siltation shall not be allowed.

Construction activities during project implementation would involve grading and movement of earth over a large area in soils subject to wind and water erosion hazard and on slopes. Therefore, before grading permits are issued, the project applicant(s) of each project



phase will be required to retain a California Registered Civil Engineer to prepare a grading and erosion control plan which addresses impacts on channel capacity.

Policy HS 3.6: The City should consider adopting road construction standards for areas designated as flood prone which allow for the passage of flood water under the road surface unless a diking effect is desired.

During project development, the City will design all roads crossing flood plains to permit floodwater conveyance and avoid exacerbating flood conditions.

Policy HS 4.1: Prohibit residential development in areas of Extreme Wildfire Risk (represented on Exhibit HS-2).

Policy HS 4.2: Development projects in areas of High Wildfire Risk (see Exhibit HS-2) shall be reviewed by the Fire Chief to ensure that fire protection will not be excessively difficult or dangerous and that mitigation measures are included to minimize risk to acceptable levels.

Policy HS 4.3: Require landowners to maintain firebreaks around existing residences. Require greater buffer widths in areas of High and Extreme Wildfire Risk. Maintain buffer areas along all major roadways and around structures in areas of High Grassfire Risk identified on the Fire Hazards Map, Exhibit HS-3. (See Policy PF 15.2)

Policy HS 4.4: Maximum residential density for High Grassfire and High Wildfire Risk Areas shall be one dwelling unit per five acres unless appropriate mitigation measures are included to minimize risk to acceptable levels.

Policy HS 4.8: Require mitigation for development in high fire potential areas, including site planning to reduce dangers, fire-resistant building materials and plantings adjacent to structures, and insurance requirements in the event of property damage.

Policy HS 4.9: Hillside development shall take into consideration the recommendations developed by the California Department of Forestry and Fire Protection in order to minimize the risk of loss due to wildfires.

There are no areas considered to have "Extreme Wildfire Risk" in the FTSSP planning area. All subdivisions will be reviewed by the Fire Department and recommendations from CalFire will be incorporated into project design.

Policy HS 4.5: Ensure the ability to provide fire protection within areas of new development. (See Objective PF 15)

Before the approval of grading permits and improvement plans, the City will identify a site, based on the recommendations in the Citygate study, for the relocation of Station 39. The selected site shall be located such that 80 percent of the service area for the station would be within a 5-minute service range, as required by the City's standard.

Policy HS 4.6: Require remote hillside developments to maintain sufficient water supplies onsite in the form of pools, ponds, or storage tanks for wildfire protection.

This policy is not applicable because there are no remote hillside developments in the FTSSP.

Policy HS 4.7: Avoid siting structures on hilltops and upper slopes in areas of high fire potential due to danger and difficulty of providing adequate fire protection.

The Project applicant(s) will not locate, design, and construct any new structures in areas of high fire potential. Recommendations developed by the CDF will be incorporated.

Policy HS 5.1: New development within Compatibility Zones A, B1, B2, C, D, and the Height Review Overlay Zone should comply with the basic and supporting Land Use Compatibility criteria shown in Chapter 2, including Table 2A, and all other applicable criteria of the Land Use Compatibility Plan (LUCP) for Travis AFB. New development within Compatibility Zones should comply with the Primary Compatibility Criteria shown in Table 2A of the Land Use Compatibility Plan for the Travis Aero Club.

Policy HS 5.2: Projects within the Travis AFB LUCP Area of Influence Compatibility Zones A, B1, B2, or C as shown on Exhibit HS-3 of the General Plan shall be referred to the Solano County Airport Land Use Commission (ALUC) as required by the Solano County Airport Land Use Compatibility Review Procedures. Projects that exceed the height limits specified in Table 2A of the LUCP for Travis AFB shall also be referred to the ALUC. Projects within the Travis Aero Club Compatibility Zones shall be referred to the Solano County ALUC as required by the Travis Aero Club Land Use Compatibility Plan.

Portions of the Specific Plan Area is located in areas addressed by the Travis Air Force Base (AFB) Land Use Compatibility Plan as Zone D. Incompatible land uses within Zone D include physical (i.e., objects greater than 200 feet tall), visual, and electronic forms of interference with the safety of aircraft operations. The City does not anticipate that the Specific Plan would include structures greater than 200 feet in height. However, each specific development project will be reviewed according to existing regulatory requirements designed to avoid conflicts with operations at Travis AFB.

Policy HS 5.3: If it is the determination of the ALUC that any of the mandatory projects referred to the ALUC are inconsistent with the LUCP for Travis AFB or the Land Use Compatibility Plan for Travis Aero Club, the City Council may hold a hearing to consider the project. The City Council may overrule the ALUC after the hearing by a two-thirds vote if it makes specific findings that the proposed action is consistent with the purposes of Airport Land Use Commission Law as contained in Article 3.5 of the Public Utilities Code.

This policy is not applicable because these mandatory projects are not the subject of the FTSSP.

Policy HS 5.4: The city should adopt an open land plan for the entire area within Compatibility Zones A and B north of Air Base Parkway, as defined in the Land Use Compatibility Plan for Travis Aero Club. Individual lots located within Compatibility Zone B south of Air Base Parkway shall include a minimum of 50 percent open land, as defined in the Land Use Compatibility Plan for Travis Aero Club.

This policy is not applicable because the FTSSP is outside this specified area.

Policy HS 5.5: The city will require, as a condition of approval of development within Compatibility Zone C of the Travis AFB LUCP, homebuyer notification regarding aircraft operational impacts on the property. Similarly, the city will require homebuyer notification of the proximity of Travis Aero Club and the characteristics of the airport's activity for all new residential development located within Compatibility Zone D of the Land Use Compatibility Plan for Travis Aero Club.

Portions of the Specific Plan Area is located in areas addressed by the Travis Air Force Base (AFB) Land Use Compatibility Plan as Zone D. All development will comply with this requirement.

Policy HS 5.6: Nonconforming development or reconstruction within the Travis AFB LUCP Area of Influence shall be consistent with the policies in the LUCP regarding such development or reconstruction. Nonconforming development within Aero Club Zone B that is partially or fully destroyed shall comply with the City's replacement requirements for nonconforming structures and uses as contained in the City's Zoning Ordinance.

The FTSSP will not result in new nonconforming structures. Reconstruction of existing nonconforming structures will comply with all ALUC requirements.

Policy HS 6.1: Objects and structures within Compatibility Zones A, B1, B2, C, D and the Height Overlay Zone for Travis AFB as shown in Exhibit HS-3 of the General Plan and objects and structures within the Compatibility Zones A, B, and C for the Travis Aero Club as shown in the Land Use Compatibility Plan for Travis Aero Club shall conform to FAR Part 77 height limits.

Portions of the Specific Plan Area is located in areas addressed by the Travis Air Force Base (AFB) Land Use Compatibility Plan as Zone D. Development facilitated by the FTSSP will be reviewed according to existing regulatory requirements designed to avoid conflicts with operations at Travis AFB.

Policy HS 7.1: Support the Solano County Hazardous Waste Management Plan (CHWMP) and the policies, objectives and programs contained therein to the extent that they are applicable to the City of Fairfield.

Policy HS 7.2: Annually review the Multi-Hazard Disaster Plan to ensure that the sections of the plan which address emergencies associated with the storage and transport of hazardous materials reflect current inter-agency response agreements and procedures.

A Hazardous Materials Assessment Report was prepared for the FTSSP Project. The FTSSP development will support the recommendations of the Hazardous Materials Assessment Report, including compatibility with the CHWMP where applicable. Project applicant(s) will prepare a Phase I ESA investigation for projects that were not addressed as a part of the Hazardous Materials Assessment Report.

Policy HS 7.3: Establish zoning standards for all industrial zoned areas, where hazardous waste treatment, transfer, storage and disposal facilities could be located, which are consistent with the adopted Siting Criteria outlined in the Solano CHWMP. (See Policy LU 13.2)

Planning Area 6, the planned employment center for the FTSSP area, will conditionally permit such uses and will include standards for developing such facilities.

Policy HS 7.4: Involve the Fire Department and Solano County Department of Environmental Health in the review process for all projects located on commercial and industrial designated properties where potential for hazardous materials use has been identified so procedures for hazardous waste handling, treatment, storage or disposal can be implemented. (See Policy LU 13.2)

Policy HS 7.6: Through the project review process, require risk assessments for all commercial and industrial uses that store, use and produce hazardous materials and are adjacent to residential areas and immobile populations such as schools, hospitals, convalescent homes, prisons etc. Determine an adequate buffer between these uses. (See Policy LU 13.2)

Implementation of the Specific Plan would involve the storage, use, and transport of hazardous materials during demolition, construction, and operation activities. The proposed Project will comply with all local, state, and federal regulations during construction and operation, and obtain required permits. In addition, industrial uses permitted in Planning 6 may include uses generating such wastes. These projects will comply with all relevant local and State regulations.

Policy HS 7.5: Continue to address potential concerns associated with the transport, storage, use and disposal of hazardous materials and waste through the environmental review process and minimize risk through the use of proper mitigation measures.

Implementation of the Specific Plan would involve the storage, use, and transport of hazardous materials during demolition, construction, and operation activities. Therefore, the proposed Project will comply with local, state, and federal regulations during construction and

operation, and obtain required permits. In addition, a licensed professional will investigate the extent to which soil and/or groundwater may have been contaminated, specifically on parcels not covered by the Hazardous Materials Assessment Report Northeast Fairfield Station Area, and require implementation of required measures, as necessary.

Policy HS 7.7: During environmental document preparation for and before approval of any project within 0.5 mile of the boundaries of Travis Air Force Base, the City shall consult with the Travis AFB Environmental Cleanup Program, EPA, California Department of Toxic Substances Control, and the San Francisco Bay Regional Water Quality Control Board regarding continuing base contamination and remediation efforts. No projects shall be approved where there is substantial evidence of existing contamination that would pose an unacceptable risk to the health of future occupants of the project.

The FTSSP EIR analyzed potential impacts related to hazardous materials and contamination within the Project area. After implementation of mitigation measures, potential impacts related to hazardous materials and contamination were reduced to a less-than-significant level. However, implementation of the Specific Plan would involve the storage, use, and transport of hazardous materials during demolition, construction, and operation activities. Therefore, the proposed Project will comply with local, state, and federal regulations during construction and operation, and obtain required permits.

Policy HS 8.1: Provide secure shelter facilities with adequate supplies for displaced individuals.

Implementation of the Specific Plan would result in the demolition of approximately seven residences that are currently located within the Specific Plan Area. Within any future redevelopment project areas within the Specific Plan Area, property owners would be compensated for their properties and relocated in accordance with the California Relocation Law, California Government Code Section 7260(B). However, the residences that could be removed as a result of the Specific Plan Area are not within areas currently contemplated as being within a future redevelopment project area by the City. Property owners will be justly accommodated in accordance with California Relocation Law.

Policy HS 8.2: Identify critical emergency facilities, including communication, medical, shelter, and transportation facilities, and human resources, and ensure their operation in the event of disaster.

Policy HS 8.3: Promote programs within the community that will improve emergency preparedness and public education, and prepare residents to respond to disaster situations.

Policy HS 8.4: Ensure that critical services will remain operable in the event of disaster. These services include water, communications, and utilities.



Policy HS 8.5: Annually review the Multi-hazard Disaster Plan to ensure that the plan provides the City with a comprehensive response plan for all disaster or emergency situations.

Development envisioned in the FTSSP will have limited impact on the City's emergency preparedness planning. Development impact fees will help pay for needed facilities and emergency response personnel and should result in compliance with the City's Emergency Risk Management Plan.

Policy HS 9.1: Ground transportation noise: The compatibility of proposed projects with existing and future noise levels due to ground transportation noise sources shall be evaluated by comparison to Table HS-1 where the existing or future noise level from ground transportation noise sources is determined to exceed the standards of Table HS-1. Noise levels in outdoor activity areas and interior spaces shall be mitigated to the levels shown in Table HS-1.

The FTSSP-related construction traffic to the local roadway network would not result in a substantial temporary increase in traffic noise levels or a temporary violation of applicable noise standards. Also, development permitted under the FTSSP should not result in long term noise levels which exceed the General Plan or the Fairfield Noise Ordinance. Please refer to Section 4.5, Noise, of the FTSSP EIR for additional information. Also, Mitigation Measure 4.11-5a:

"Construct Noise Barriers along Roadways that Expose Proposed Sensitive Receptors to Noise Levels Exceeding 60 dBA Ldn",
would ensure that mobile source noise would not exceed applicable standards at proposed sensitive receptors through the use of acoustical shielding.

Policy HS 9.2: Aircraft noise: All new land use proposals shall comply with the basic and supporting land use compatibility criteria of the Travis AFB Land Use Compatibility Plan (LUCP) and the Land Use Compatibility Plan for Travis Aero Club for aircraft-generated community noise. (See Policy LU 13.3)

Travis AFB runways are located approximately three miles south of the Specific Plan Area. No proposed Specific Plan sensitive receptors are located within the 60 dBA CNEL noise contour according to the most recent noise contours in the Solano County General Plan Public Health and Safety Element. Some proposed employment areas of the Specific Plan Area are within the 60-65 dBA CNEL noise contour and could be exposed to noise levels between 60-65 dBA CNEL. This level of noise is within acceptable noise limits. Therefore, no proposed sensitive receptors would be exposed to noise levels exceeding applicable standards from Travis AFB operations and no other airports are within two miles of the Specific Plan Area.

Policy HS 9.3: Non-transportation noise: Noise created by new non-transportation noise sources shall be mitigated so as not to exceed the

interior and exterior noise level standards of Table HS-2. Where proposed non-transportation noise sources are likely to produce noise levels exceeding the performance standards of Table HS-2, an acoustical analysis shall be required as part of the environmental review process so that noise mitigation may be included in the project design.

Mitigation Measure 4.11-4, Stationary Noise Source Reduction Measures and Design Criteria, in Section 4.5, Noise, of the FTSSP EIR will ensure that stationary noise sources associated with the proposed Project would be reduced to a less than significant at the nearest sensitive land uses. Planning and design of noise-generating features will be required to avoid exposing noise-sensitive receptors to levels of noise in excess of local standards.

Policy HS 9.4: Non-transportation noise: New development of noise sensitive land uses shall not be allowed where the noise level due to non-transportation noise sources will exceed the standards of Table HS-2. Where noise sensitive land uses are proposed in areas exposed to existing or projected exterior non-transportation noise levels exceeding the performance standards of Table HS-2, an acoustical analysis shall be required so that noise mitigation may be included in the project design.

Mitigation Measure 4.11-4, Stationary Noise Source Reduction Measures and Design Criteria, in Section 4.5, Noise, of the FTSSP EIR will ensure that stationary noise sources associated with the proposed Project would be reduced to a less than significant at the nearest sensitive land uses. Planning and design of noise-generating features will be required to avoid exposing noise-sensitive receptors to levels of noise in excess of local standards.

Policy HS 9.5: All acoustical analyses required by the Noise Component of the Health and Safety Element shall:

- *Be the responsibility of the applicant.*
- *Be prepared by a qualified person experienced in the fields of environmental noise assessment and architectural acoustics.*
- *Include representative noise level measurements with sufficient sampling periods and locations to adequately describe local conditions.*
- *Estimate existing and projected (20 years) noise levels in terms of Ldn and/or the standards of Table HS-2, and compare those levels to the policies of this Element.*
- *Recommend appropriate mitigation to achieve compliance with the adopted policies and standards of this Element. Where the noise source in question consists of intermittent single events, the report must address the effects of maximum noise levels in sleeping rooms in terms of possible sleep disturbance.*

- *Estimate noise exposure after the prescribed mitigation measures have been implemented.*
- *Describe a post-project assessment program which could be used to evaluate the effectiveness of the proposed mitigation measures.*

A noise analysis of the potential impacts resulting from the implementation of the FTSSP has been prepared as part of the FTSSP EIR. All development facilitated by the FTSSP will comply with this requirement.

Policy HS 9.6: The City shall utilize procedures for project review and issuance of building permits to ensure that noise mitigation measures identified in an acoustical analysis are implemented in the project design.

All development facilitated by the FTSSP will comply with this requirement.

Policy HS 9.7: The City shall require monitoring of compliance with the standards of the Noise Element after completion of projects where noise mitigation measures have been required.

All development facilitated by the FTSSP will comply with this requirement.

Policy HS 9.8: The Police Department shall actively enforce the California Vehicle Code sections relating to adequate vehicle mufflers and modified exhaust systems.

The FTSSP will not affect this Police Department responsibility.

Policy HS 9.9: The City shall purchase only newly acquired equipment and vehicles which comply with noise level performance standards based upon the best available noise reduction technology.

Existing City policy will continue to be implemented.

Policy HS 9.10: The City shall periodically review and update the Noise component of the Health and Safety Element to ensure that noise exposure information and policies are consistent with changing conditions within the community and with noise control regulations or policies enacted after the adoption of the Element.

This Project is not affected directly by this general policy.

Policy HS 9.11: The City shall require all development projects to mitigate noise impacts associated with construction activities.

Mitigation Measure 4.11-1, Construction Noise Mitigation, is included in the FTSSP EIR to reduce potential impacts from construction noise to less than significant.

Policy HS 9.12: The City shall enforce the State Noise Insulation Standards (California Code of Regulations, Title 24) and Chapter 35 of the Uniform Building Code (UBC).





The FTSSP development will comply with the applicable standards and codes.

A.5 Housing Element

Policy HO 1.1: Encourage multi-family housing at appropriate locations and densities.

The FTSSP includes mixed residential housing types, including single-family detached, townhomes, condominiums, apartments, and multi-family. Two main areas with multi-family housing include the Town Center District and Lake District. The Town Center District includes higher density residential with a mixed-use character, providing 1,645 high and medium density residential units. The Lake District includes three residential densities of low, medium, and high for a total of 1,306 dwelling units.

Policy HO 1.2: Encourage appropriate development of innovative projects that use manufactured homes, modular housing, and/or mobile homes.

The FTSSP does not specifically address modular housing, but such housing could be developed in the Project area subject to development review and meeting the Specific Plan's standards for development.

Policy HO 1.3: Encourage mixed-use development that encourages unique housing types, homeownership opportunities, and employment-generating uses.

The FTSSP specifically encourages mixed use development, concentrated in the Town Center and Lake communities. The Project facilitates housing near employment and opportunities for home-based businesses. The residential development will provide a range of housing type options, including single-family detached, townhomes, condominiums, apartments, and more. The proposed Project is expected to add approximately 4,000 jobs and 16,276 additional residential within the City of Fairfield.

Policy HO 1.4: Support pedestrian-and-transit-oriented housing development in appropriate locations.

Residential development in the FTSSP development will include an extensive network of pedestrian pathways that will enable residents to easily access public transportation, employment centers, the Lake Park, numerous neighborhood parks and other public amenities., In addition, the Town Center District, located adjacent to the Train Station, will be a Transit-Oriented Development (TOD) of a mixed-use character providing 1,645 high and medium density residential units.

Policy HO 2.1: Support changes in land use designations for commercial, industrial, and residential areas where conditions support quality higher density infill housing.

The FTSSP is a comprehensive change to the City's land use plan for the area surrounding the planned train station. Land uses permitted in the FTSSP are not infill per se but include higher density residential, employment/industrial, commercial, public/quasi-public, and open space. Land use configurations and Specific Plan threshold requirements are designed to support the long-term TOD viability and high-quality vision of FTSSP. Residential areas are structured around the Train Station to focus density in this area and maximize TOD potential.

Policy HO 2.2: Encourage property owners of adjoining parcels to consolidate or otherwise cooperatively develop their parcels.

The FTSSP was initiated with the participation of the property owners in the planning area, who are cooperatively funding the planning process and working with staff and the consultant team to complete the plan.

Policy HO 2.3: Encourage property owners of smaller parcels (less than two acres) to work with the City to develop affordable ownership housing where rental housing is infeasible.

This policy primarily addresses small scale infill development. However, the FTSSP includes smaller parcels suitable for residential development by smaller builders.

Policy HO 3.1: Assist developers interested in new affordable housing.

While the FTSSP does not specifically designate or fund affordable housing, the diverse housing types and higher densities in the Project area will facilitate the development of market rate housing which is more affordable. Existing State programs to assist in the development of new affordable housing would be available to developers interested in building in the FTSSP.

Policy HO 3.2: Support the use of State and federal programs and nonprofit funding sources as funding for affordable housing construction and rehabilitation.

Developers interested in building in the FTSSP area would be eligible to use these funding sources, depending on availability and the specifics of the Project.

Policy HO 3.3: Encourage homeownership for moderate and low-income homebuyers.

The development proposed in the FTSSP includes higher density housing in close proximity to employment, services, and transit. These factors can improve the affordability of the housing for moderate income households. City programs such as the Silent Second program would be potentially available for units purchased in the planning area.

Policy HO 3.4: Use Redevelopment Housing Set-Aside Funds to provide affordable housing.



This policy is not applicable because the FTSSP is not a redevelopment project area and the City concentrates its use of this funding source into rehabilitation of existing housing in project areas.

Policy HO 3.5: Implement State requirements regarding the location of manufactured homes.

Manufactured homes can be constructed in the Project area as elsewhere in the City, per the Zoning Ordinance and Building Code.

Policy HO 3.6: Support housing choice for very-low income households.

The high density housing constructed under the Specific Plan may be appropriate for some very low income households. Citywide funding sources would be available for applicable households within FTSSP area.

Policy HO 3.7: Maintain the affordability of existing housing.

The FTSSP involves the construction of new housing. However, if the City assists any project in the planning area, covenants or other guarantees would be implemented as part of the Project.

Policy HO 4.1: Locate affordable housing near employment, transportation, and facilities.

Policy HO 4.2: Discourage over-concentration of low-income housing in any neighborhood.

Policy HO 4.3: Encourage affordable housing to be spread throughout the community.

The FTSSP provides housing in close proximity to employment, transit, and services, improving overall affordability for lower and moderate income households. Affordable housing will be integrated into the current housing mix near employment, transportation, and community facilities and will be spread throughout the community.

Policy HO 5.1: Assist lower income households displaced by the removal of affordable units.

This policy is not applicable because it primarily involves the removal of older existing housing.

Policy HO 6.1: Enforce the City's Community Preservation Ordinance and building codes.

Code enforcement programs and building codes will be enforced in all neighborhoods, including the new projects constructed under the FTSSP.

Policy HO 6.2: Use available state and federal funds for rehabilitation assistance for low-income households.

This policy is less applicable during the immediate future as the policy primarily pertains to rehabilitation of existing older housing.

Policy HO 6.3: Continue to monitor housing conditions.

This policy is not applicable because it is a City-wide policy.

Policy HO 7.1: Support the efforts of Solano County and social service providers to ensure that there are adequate facilities to serve the needs of the homeless.

The Specific Plan does not directly address facilities for homeless persons. Services for the homeless would be available to persons in the area, although it is unlikely that new homeless facilities will be constructed in this area.

Policy HO 7.2: Support nondiscrimination in the sale, rental, and financing of housing.

This City-wide policy will continue to apply in the FTSSP area.

Policy HO 7.3: Address housing needs of single parents.

The diverse housing types and the availability of schools, parks, employment, and services, including child care, will support single parents. Social services will continue to be provided by County and City agencies. .

Policy HO 7.4: Support housing programs targeted to older adults, 55 years of age and above, taking into account lifestyle issues, health issues, and income.

The FTSSP will enable the construction of diverse housing types-including senior housing. The proximity to transit, recreation, and services may be highly suitable for seniors.

Policy HO 7.5: Support efforts to meet the housing needs of Travis Air Force Base personnel.

The FTSSP will provide several thousand housing units in close proximity to Travis Air Force Base.

Policy HO 7.6: Ensure that new residential developments include housing accessible to persons with disabilities and others with physical conditions that make it difficult for them to use conventional housing.

Residential developments within the FTSSP area will be subject to Americans with Disabilities Act (ADA) requirements where appropriate.

Policy HO 7.7: Encourage housing for larger families.

The FTSSP includes a mixture of housing types, including single-family detached, townhomes, condominiums, apartments, and multi-family, many of which will be suitable for larger households.

Policy HO 7.8: Support regional and County efforts to meet the needs of migrant and seasonal farm workers and their families.

This City-wide policy is not applicable to the FTSSP because the Project area is relatively distant from more active agricultural areas in



Solano County (e.g., Suisun Valley, Dixon). However, some housing may be suitable for full-time resident agricultural workers.

Policy HO 8.1: Implement state energy conservation standards.

The FTSSP will provide a sustainability framework for energy conservation. The City of Fairfield will be adopting the most stringent, eco-friendly state-wide building code in the United States, the new building code standards "CALGreen" will take effect in January 2011. CALGreen lays out specific constraints for newly constructed buildings and raises the bar of building energy performance for all future development. The FTSSP development will be built according to the CALGreen standards.

Policy HO 8.2: Encourage energy-conserving development patterns.

The FTSSP is intended to be developed in a mixed use, high density land use pattern that facilitates walking, bicycling, and transit use, with close proximity of diverse housing types to employment and services.

Policy HO 8.3: Assist low-income households in weatherproofing their homes.

This City-wide policy is more applicable to older neighborhoods in Fairfield which are not built to current weatherproofing standards. .

A.6 Land Use Element

Policy LU 1.1: Only allow development that is consistent with the Land Use Diagram and the Land Use Category definitions.

The permitted uses and development standards in the FTSSP supersedes those in the current Land Use Element and City of Fairfield Zoning Ordinance, unless otherwise stated in the FTSSP. If the FTSSP is silent on an issue, the standards in the existing City of Fairfield standards shall apply, as appropriate.

Policy LU 1.2: Provide a mixture of uses throughout the City that provide adequate housing, employment, shopping, and social and leisure activities for their respective populations. (See Policy CI 1.2)

The Project is broken into 10 Planning Areas with various land use categories within each Planning Areas, including residential, mixed commercial, community commercial, employment, community facilities/ public utilities/institutional, active open space/parks/recreation, and passive open space/conservation.

Policy LU 1.3: Achieve an ultimate citywide jobs-housing balance equal to the jobs-housing ratio for the nine-county Bay Area. (See Objective ED 6, Objective HO 4, Policy HO 4.1, and Program HO 4.1 A)

The proposed Project is expected to add approximately 4,000 jobs and 16,276 additional residents within the City of Fairfield. Although there is no guarantee that residents would work locally and vice versa, this



balance of new jobs and residents helps the City maintain an overall jobs/housing balance in the community.

Policy LU 2.1: Encourage the preservation of agricultural land surrounding the City and permanently preserve agriculture in the Suisun Valley. (See all policies and objectives in the Farmlands and Agriculture Element, Objective OS 1, Policies OS 1.1, OS 1.2, OS 1.3, OS 1.4 and OS 1.5, Programs OS 1.2 A, OS 1.2 B, OS 1.2 C, OS 1.4 A, and Rancho Solano North Master Plan Area Policy 6).

Implementation of the Specific Plan would not result in changes to the physical environment that could result in conflict with agricultural uses. Under the Specific Plan, on-site areas currently used for grazing would be designated as a Greenbelt/Conservation Area or could potentially used as a mitigation bank. The proposed Specific Plan would not place urban land uses adjacent to off-site agricultural operations. Therefore, the City does not anticipate compatibility conflicts between the proposed Project and on- or off-site agricultural operations.

Policy LU 2.2: Preserve and promote the Central Business District as one of the City's social and cultural centers and as an economically viable retail, professional office, and residential district. (See Policy CI 7.3 and Policy ED 3.3)

This policy applies to the City's existing traditional downtown. However, the FTSSP also proposes the creation of a new "town center" for the specific plan community. The Town Center District is considered the "downtown" area of the Fairfield Train Station Community. It integrates residential, commercial and recreational spaces in a central urban node that embodies the traditional town crafting, smart-growth and TOD principals of the plan. Occurring directly across from the Train Station, this District has the unique opportunity to establish a high-density residential and commerce corridor where business, entertainment, community festivals and daily life can coincide with traditional neighborhood design to reduce the need for vehicle trips. The design and integration of the land use has been carefully planned to function in support of the Train Station, to increase accessibility to transit, provide high-quality public spaces, decrease vehicle trips and increase quality of life.

Policy LU 2.3: Review and comment on all development proposals within the unincorporated areas of the City's sphere of influence and in neighboring cities.

Much of the urbanized area (on which City comments are typically addressed) in unincorporated Solano County is within the FTSSP. This Specific Plan will replace the older commercial and industrial land uses in the Peabody Road area.

Policy LU 2.4: Establish and maintain a greenbelt buffer around the City. (See Objective OS 2, Policies OS 2.1, OS 2.2, OS 2.3, OS 2.4, OS 2.5)



A major component of the FTSSP is open space with 30 percent of the plan area devoted to the preservation and conservation of natural lands. Approximately 870 acres of land (habitat conservation, mitigation bank, and vernal pool conservation area) is committed to natural lands preservation with an additional 822 acres of open space in the community as parks, paseos, and recreation network. Key components of the open space system include but are not limited to the 593-acre habitat conservation, the 203-acre mitigation bank, the 50-acre Great Park, and the 21-acre Lake Park. Some of these components, such as the habitat conservation, mitigation bank, and the Great Park, in addition to other open space elements, constitute the Vacaville-Fairfield-Solano Greenbelt. This Greenbelt has been reconfigured as a part of the specific plan process to better align with ecological conservation priorities and appropriate land use patterns.

Policy LU 3.1: What is urban shall be municipal, and what is rural shall be within the County. Any urban development requiring basic municipal services shall occur only within the incorporated City and within the urban limit line established by the General Plan. (See Policy OS 1.6)

The FTSSP will result in the annexation of a new urbanizing community near transit. The Project includes municipal facilities to address all of its public service needs and generate new capacity for residents. Section 9.0, Public Services, and Section 11.0, Utilities Plan, of the Specific Plan have detailed information about the proposed municipal services that will be provided to the Project site.

Policy LU 3.2: Where the urban limit line encompasses a master plan area, it may include land which will not ultimately be developed with urban uses. Once areawide plans are adopted for master plan areas, the urban limit line may be amended to exclude open space areas.

The Project various land use categories in the plan include both urban uses and open spaces. The open space and conservation lands in the Fairfield-Vacaville Greenbelt will remain outside the City Limits and can be excluded from the City's Urban Limit Line.

Policy LU 3.3: A voter initiative reaffirmed and readopted: (1) the "Travis Reserve" land use designation and the boundaries of the Travis Reserve shown on the General Plan Land Use Diagram; (2) the Urban Limit Line shown on the General Plan Land Use Diagram; (3) the General Plan text describing the uses permitted in the Travis Reserve land use designation; and (4) General Plan objective LU 3, Policies LU 3.1, LU 3.2, HS 9.2, and Program HS 9.2A, in effect as of October 11, 2002. These components of the General Plan are collectively referred to as the Travis Air Force Base and Fairfield Urban Boundary Policies and, through December 31, 2020, may be amended only by a vote of the people or as follows:

1. The City Council may amend the boundaries of the Urban Limit Line to exclude open space areas, provided that the amended

boundaries are within or coextensive with the limits of the Urban Limit Line in effect as of October 11, 2002.

2. The City Council may redesignate to a different land use designation that portion of the land currently designated as Travis Reserve that lies west of North Gate Road/Burgan Boulevard and north of Air Base Parkway/Travis Avenue.
3. The City Council may amend Program HS 9.2A to refer to a new 60 dB CNEL maximum mission contour for Travis Air Force Base (“New Contour”) adopted in either an Air Installation Compatibility Use Zone (“AICUZ”) or an Airport Land Use Plan for Travis Air Force Base, provided that the amendment specifies that in the event that the New Contour is set aside or otherwise ceases to be in effect, the 60 dB CNEL maximum mission contour established in the 1995 AICUZ for Travis Air Force Base shall apply for the purposes of Program HS 9.2A until such time as the New Contour is reinstated or the City Council amends Program HS 9.2A to refer to a different 60 dB CNEL maximum mission contour for Travis Air Force Base adopted in either an AICUZ or an Airport Land Use Plan for Travis Air Force Base.
4. The City Council may amend the Travis Air Force Base and Fairfield Urban Boundary Policies if it does so pursuant to a finding, based on substantial evidence, that the application of such policies to any specific property for which a development application has been submitted constitutes an unconstitutional taking of the landowner’s property; however, any such amendment shall be made only to the extent necessary to avoid such an unconstitutional taking.
5. The City Council may reorganize, renumber, or reorder the Travis Air Force Base and Fairfield Urban Boundary Policies, provided that the Travis Air Force Base and Fairfield Urban Boundary Policies remain in the General Plan.

The FTSSP directly implements this Policy, with its permanent preservation of land near Travis Air Force Base and the omission from the City Limits of the Fairfield-Vacaville Greenbelt, as configured.

Policy LU 4.1: Expand the city limits only in conformance with the Comprehensive Annexation Plan.

The Comprehensive Annexation Plan has been superseded by the Fairfield Municipal Services Plan. The MSP will be amended as part of this planning process to incorporate all of the land intended to be urbanized under the FTSSP.

Policy LU 4.2: Development proposals shall be reviewed for provision of public facilities and services and environmental impacts. (See Policies PF 1.1 and PF 1.2)

The FTSSP includes provisions for all public facilities and services, including impact fees, construction of new facilities, and fair share contributions to major infrastructure.



Policy LU 4.3: The content of all areawide plans shall be reviewed and approved by the Planning Commission prior to their preparation.

The Planning Commission will review and provide its recommendation to Council on the FTSSP.

Policy LU 4.4: Development east of Claybank Road and north of Airbase Parkway shall occur in a logical and orderly pattern, and avoid creating discontinuous islands of development which cannot be economically served with a full range of urban services and facilities.

The FTSSP is designed as a planned community with a structured land use plan anchored by community cores and multimodal circulation plan to meet the layered needs of the community. The Specific Plan utilizes the latest concepts in connectivity, diversity, integrated open space planning and a mix of uses to create a “framework” that allows for a fine-grained and varied development pattern suited for the needs of today’s active communities.

Policy LU 5.1: Cooperate with appropriate jurisdictions in the preparation of State-mandated regional plans, such as the Congestion Management Program, the Clean Air Plan, and the Source Reduction and Recycling Element. (See Policies CI 2.2, CI 12.1, CI 12.2, and PF 13.2)

The City of Fairfield has been working closely with Solano Garbage, Travis Air Force Base, Solano Transportation Agency, the County of Solano, and the City of Vacaville to ensure that the transportation infrastructure meets regional goals and objectives. The FTSSP incorporates a new regional train station that will help reduce commute traffic volumes. FTSSP residents and businesses will be served by Solano Garbage which remains committed to its efforts to reduce waste generation and land-filling.

Policy LU 5.2: Monitor proposals for regional government and review them for positive and negative impacts on the City.

The FTSSP does not directly address issues of regional government, although the Plan’s residents will be served by Solano County and the Fairfield-Suisun Sewer District.

Policy LU 5.3: Explore the establishment of a regional open space body, such as a new open space district, to be responsible for acquiring and managing permanent open space. (See Policy OS 11.5, Programs OS 11.5 A, OS 11.5 B, Policy OS 11.6, and Program OS 11.6 A)

While the Specific Plan does not implement this policy, the open spaces permanently preserved by the planning process would be suitable for incorporation into a regional open space body.

Policy LU 8.1: Residential development shall be consistent with the gross density ranges included in the Land Use Diagram. Lower

densities may be permitted only when the City Council makes all of the following findings:

- A. The development would be compatible with the surrounding neighborhood and would not have a detrimental effect on existing or future multi-family development.
- B. The development would be equal or superior to the higher density development with respect to site planning and to preservation of natural topography, mature trees, and other natural resource.
- C. The density reduction will not prevent the City from achieving its goals for low and moderate income housing as defined in the Housing Element. Higher densities may be permitted that are consistent with State density bonus requirements.

The FTSSP dedicates 503 acres of land to residential uses for the development of up to 6,800 dwelling units. The diverse neighborhoods include single-family detached, townhomes, condominiums, apartments, and mixed use development. This large supply of zoned residential land will help the City meet its Housing Element goals for new residential units.

Policy LU 9.1: Very low density residential development shall be encouraged in transition areas within the City's urban limit line. The intent is to provide a transition between agricultural or rural areas and urban development.

The FTSSP includes a Low Density Residential (LR) category with a range of 0-10 du/ac. The majority of the proposed LR homes are planned in the transition areas between urban development and rural areas and open space.

Policy LU 10.1: Where infill development is proposed, the character, scale and density of existing single-family residential neighborhoods should be preserved in the new development. (See Policy UD 3.3)

This policy is not applicable because it is a City directed policy. However, the Specific Plan a) utilizes timeless design and planning principles; b) creates a compact footprint to develop a sustainable land plan by transitioning from low density rural neighborhoods on the edge of the Project to higher intensity in the center; c) organizes neighborhoods into districts focused on district activity cores; d) develops a planning framework that maximizes vehicular and pedestrian connectivity; e) creates view corridors, landmarks, special civic streets and town features to articulate the visual landscape and community pattern f) creates a connection between buildings and landscape; g) utilizes "neighborhood crafting" techniques to elevate the character of every neighborhood; and h) includes visually engaging and diverse streets, neighborhoods and districts.

Policy LU 10.2: Large scale single-family residential development shall include a mixture of density and dwelling types, consistent with its land use designation and density range.





The FTSSP includes a variety of housing types and configurations in distinctive neighborhoods that may include single-family detached, townhomes, condominiums, apartments, and more.

Policy LU 10.3: Ancillary uses permitted in single-family residential neighborhoods, such as public facilities, day care centers, and churches, shall have minimum negative impact on the neighborhood.

Please see Chapter 4.0 of the FTSSP for permitted uses in single-family residential neighborhoods. In general, the design and planning principles used in the Specific Plan will allow for integration of ancillary uses into the public streetscapes. The City's design review and Conditional Use Permit process will ensure that new non-residential land uses in residential areas minimize negative impacts.

Policy LU 10.4: New single-family residences shall be constructed at a size and scale that is compatible with the size of the lot.

Please see Chapter 4.0 of the FTSSP for development standards of single-family residential neighborhoods.

Policy LU 11.1: Encourage the development of a wide variety of higher density multi-family residential uses. (See Policy HO 1.3)

The FTSSP includes mixed residential housing types, including single-family detached, townhomes, condominiums, apartments, and multi-family. Two main areas with multi-family housing include the Town Center District and Lake District. Located adjacent to the Train Station, the Town Center will be an animated, vibrant, Transit-Oriented Development (TOD) of a mixed-use character providing 1,645 high and medium density residential units. The Lake District is a major amenity for FTSSP as well as the community-at-large and offers a setting for trails, picnicking, day use areas, shopping and recreation. The Lake District includes the three residential densities of low, medium, and high for a total of 1,306 dwelling units.

Policy LU 11.2: Multi-family residential land uses shall be developed with a balance of open space, landscaping and recreational amenities and shall be accessible to commercial and recreational areas, and public transportation facilities.

The FTSSP incorporates multiple parks, paseos, and open spaces which will be available to residents. Two areas with multi-family housing include the Town Center District and Lake District. The Town Center District includes higher density residential with a mixed-use character, providing 1,645 high and medium density residential units. The Lake District includes the three residential densities of low, medium, and high for a total of 1,306 dwelling units. These districts integrate the housing with landscaping, open space, and access to facilities and amenities.

Policy LU 11.3: Mobile homes shall be considered multi-family residential land uses. High standards of development and maintenance with respect to the provision of recreation and open space, landscaping

and the exterior appearance of the units shall apply to their development. (See Program HO 1.3 C)

The FTSSP does not specifically address this issue. Single mobile homes on a permanent foundation may be developed as custom homes, subject to design review. Mobile home parks are listed as a land use in the City's zoning ordinance and would be conditionally permitted in the FTSSP.

Policy LU 12.1: Special residential land uses, such as senior housing, shall be distributed throughout the City to assure their accessibility to activity centers and shopping areas and to provide the option of continuing to reside in neighborhoods of mixed economic, ethnic and age groups. For projects located on the periphery of the City that house senior citizens or the mobility impaired, special transportation, such as vans, shall be required to be provided by the project.

The FTSSP provides for a broad range of housing types, integrated with services, amenities, open space, and employment. This mixed use housing may prove particularly attractive to senior citizens and others who cannot drive or with mobility impairment.

Policy LU 13.1: New development shall preserve and enhance, to the extent possible, the existing natural vegetation, landscape features, and open space.

The FTSSP dedicates 1,530 acres of land as Passive Open Space/Conservation. This land will be integrated with and surround the 6,797 dwelling units planned for the Specific Plan. Open space uses include agriculture, habitat conservation, mitigation banks, detention basins, and public open space uses. Passive Open Space/Conservation acreage is provided in the following Planning Areas: PA1, PA2, PA3, PA4, PA6, and PA7 and PA8.

Policy LU 13.2: Appropriate buffers shall be established between industrial and nonindustrial lands. (Policies HS 9.3, HS 9.4, HS 9.5 relate to noise impacts. Policies HS 7.4, 7.5, and 7.6 relate to hazardous materials. Objective HS 8 and Policies HS 8.2, HS 8.3, HS 8.4 and HS 8.5 relate to emergency preparedness. Policy OS 8.1 relates to air quality. Policies ED 2.5 and CI 1.1 relate to traffic.)

Planning Area 6 (PA6) is the employment hub for the FTSSP.

PA6, the major employment center for the FTSSP area, is separated from residential and mixed use areas by buffer zones. These buffer zones will reduce noise, air quality and visual impacts.

Policy LU 13.3: Proposed land uses shall be consistent with the land use compatibility criteria, maps, and policies of the Travis Air Force Base Land Use Compatibility Plan and the Land Use Compatibility Plan for the Travis Aero Club incorporated into this General Plan. (See Objective HS 5, Policies HS 5.1, HS 5.2, HS 5.3, HS 9.2 and ED 1.9)





Portions of the Specific Plan Area is located in areas addressed by the Travis Air Force Base (AFB) Land Use Compatibility Plan as Zone D. Incompatible land uses within Zone D include physical (i.e., objects greater than 200 feet tall), visual, and electronic forms of interference with the safety of aircraft operations. The City does not anticipate that the Specific Plan would include structures greater than 200 feet in height. However, each specific development project will be reviewed according to existing regulatory requirements designed to avoid conflicts with operations at Travis AFB.

Policy LU 13.4: Retail uses shall only be permitted in industrial areas as a secondary use to a permitted use, such as manufacturing. Retail and service uses which serve the employees of the area may also be permitted. (See Policy ED 2.3)

PA6 will include the FTSSP IL (Light Industrial/Employment) zoning designation designed to support the campus-style development of light industrial with business park, research and development (R&D), manufacturing, and service uses. PA6 is envisioned to include a rich mix of employers, including incubator new business, multi-tenant commercial, light industrial, and service retail. This mixture of land uses is consistent with the overall intent of the Policy.

Policy LU 13.5: Home occupations may be permitted in residential areas where the use is clearly incidental and secondary to the use of the residence for dwelling purposes and where the requirements of the Zoning Regulations for home occupations are met. (See Policy ED 1.4)

The city's regulations on Home Occupations will continue to apply in the Planning Area. Please see Chapter 4.0 of the FTSSP for development standards in residential areas.

Policy LU 14.1: Areas designated for industrial use on the General Plan Diagram should be held for such use to assure that there will be sufficient industrial land available to create an economic base in support of the costs of providing on-going public services. A slow pace of industrial development shall not be construed as justification for utilizing lands designated for industrial use for another type of urban use.

The FTSSP dedicates 286 acres of land to Industrial uses in two separate parcels for up to 4,600,000 square feet of potential development. The goal of industrial land uses is to provide for a campus-setting industrial and employment center with the potential to attract research and development, corporate head quarters, industrial services and administrative offices. This land use provision increase the jobs/housing balance within FTSSP and reduce the distance and frequency of vehicle trips between daily needs. The industrial land use is consistent with the IL zoning category of the Fairfield Municipal Code.

Policy LU 14.2: A variety of employment areas shall be provided in which industrial and commercial activities will contribute to the

continued economic welfare of the people of the City and to stable economic and tax bases for the City. (See Policy ED 1.1)

The FTSSP includes a broad range of commercial and industrial areas, including industrial land uses and the Planning Area 6 planned employment center.

Policy LU 14.3: Commercial areas shall be provided to accommodate the needs of the City's present and anticipated population. (See Objective ED 3)

The FTSSP dedicates 47 acres of land to commercial uses for up to 362,000 square feet of potential development. Commercial land uses reflect three service categories consistent with Commercial Zones of the Fairfield Municipal Code: Community Commercial, Mixed-Use Commercial, and Neighborhood Commercial.

Policy LU 14.4: New strip commercial development shall be discouraged. (See Policy ED 4.2)

No new strip development is proposed. New commercial areas are clustered within the two focal districts of the Fairfield Train Station Community – the Town Center District and the Lake Park District. Commercial areas are located along Main Street and iconic streets within these districts to provide a diverse range of businesses at a pedestrian-scale. Commercial uses may be vertically or horizontally mixed with compatible and well-designed residential units of various configurations within the commercial areas.

Policy LU 14.5: The 18-acre mixed use site located at the northeast corner of Airbase Parkway and Claybank Road shall include office, specialty services and retail uses. Prior to zoning the site, the property owner shall conduct a marketing study to identify uses and services for the site. The neighborhood shopping center to the north of the site shall be given priority for neighborhood-serving retail and services.

This policy applies to an area outside the scope of the FTSSP.

Policy LU 18.1: Utilize land within the existing city limits as efficiently as possible, allowing for a wider variety of housing types and densities within the same zone district, and economical use of public services and infrastructure.

The FTSSP includes a structured variety of land uses including, residential, employment/industrial, commercial, public/quasi-public, and open space. Land use configurations and Specific Plan threshold requirements are designed to support the long-term TOD viability and high-quality vision of FTSSP. Residential areas are structured around the Train Station to focus density in this area and maximize TOD potential. Nearly sixty percent of the Specific Plan area will be conserved as active or passive open space by the Project.

Policy LU 18.2: Provide incentives and support projects that are designed to encourage compact growth and higher densities while



providing amenities such as bike paths, parks and pedestrian parkways as densities increase.

Policy LU 18.3: Encourage pedestrian and/or transit-oriented projects at unit densities that make transit feasible. Evaluate planned land uses around transportation centers to assess the feasibility of a high-density residential project oriented to transit.

The FTSSP envisions a compact project providing a diverse mix of higher density housing, shopping, employment, cultural, and recreational opportunities to encourage walking, bicycling, and other alternative transportation trips. A bicycle and pedestrian network is a part of the proposed Project and includes an extensive network of pedestrian pathways and a cohesive bicycle path system that will enable residents to access the Project's public transportation facilities, the Lake Park, the numerous parks and other public amenities, consistent with the safety and sensitive resources protection priority. Land use configurations and Specific Plan threshold requirements are designed to support the long-term TOD viability and high-quality vision of FTSSP. Residential areas are structured around the Train Station to focus density in this area and maximize TOD potential.

Policy LU 18.4: Establish a program to reduce fees for infill development which meets a predetermined set of development criteria consistent with the City's goals for compact growth.

This policy is not applicable because the FTSSP is a new development area. Fees are specifically targeted to paying for the necessary infrastructure and services.

Policy LU 19.1: Reinforce the economic vitality of the core by maximizing the number of residents in close proximity to it, and encouraging development of new ownership housing opportunities in downtown.

Policy LU 19.1 applies to downtown Fairfield. However, the FTSSP includes a town center whose development objectives mirror this policy. Located adjacent to the Train Station, the Town Center will be an animated, vibrant, Transit-Oriented Development (TOD) of a mixed-use character providing 1,645 high and medium density residential units. The High Density Residential (HR) category provides a range of 20-50 du/ac with a target density of 28du/ac with a potential for 2,421 dwelling units, centering nearly 3,000 homes within a half-mile radius of the Train Station.

Policy LU 19.2: Encourage the development of live-work spaces in the CD and CM districts in downtown Fairfield.

Policy LU 19.1 applies to downtown Fairfield. However, the FTSSP includes a town center whose development objectives mirror this policy. The Town Center District and the Lake District include Community Commercial (CC), Community Mixed Use (CM) and

Neighborhood Commercial (CN) uses in order to promote live-work lifestyles.

Policy LU 19.3: Allow street parking spaces adjacent to properties to be counted toward meeting the parking requirements for new residential and mixed-use projects near the downtown but outside the P-1 Parking Overlay District.

This policy applies specifically to areas near downtown Fairfield. Please see Chapter 4.0 of the FTSSP and the City of Fairfield Municipal Code for parking development standards for residential and mixed-use areas.

A.7 Open Space, Conservation and Recreation Element

Policy OS 1.1: Preserve agricultural enterprises by supporting right-to-farm policies, and separating and buffering agricultural lands from new development areas. (See Policy AG 1.1)

Under the proposed Project, on-site areas currently used for grazing would be designated as a Greenbelt/Conservation Area. The Specific Plan also includes establishment of on-site and off-site areas for habitat conservation and mitigation. The proposed Project would not place urban land uses adjacent to off-site agricultural operations.

Policy OS 1.2: Provide economic support for permanent commitment to agricultural uses. (See Policy AG 1.2)

The FTSSP will provide a funding mechanism for the permanent preservation of the Vacaville-Fairfield Greenbelt and significant resource conservation lands, including critical habitat.

Policy OS 1.3: Maintain current agricultural zoning in the area east of Travis AFB, including the Jepson Prairie. (See Policy AG 1.3)

The proposed Project site is not located east of Travis Air Force Base.

Policy OS 1.4: Permanently preserve productive agricultural lands within the Suisun Valley by continuing to direct new urban development away from the Suisun Valley. (See Policy LU 2.1, AG 1.4 and ED 8.3)

The project site is not located within the Suisun Valley.

Policy OS 1.5: Strongly reaffirm the City of Fairfield's commitment to the basic goals of the 1974 "Solano Irrigation District Agreement," actively participate in any local measures that promote the preservation of agriculture adjacent to Fairfield, and encourage extension of the agreement past its present expiration date. (See Policy AG 1.5)

The Project site is not located in Suisun Valley and is not affected by the Agreement.

Policy OS 1.6: What is urban shall be municipal, and what is rural shall be within the County. Any urban development requiring basic municipal services shall occur only within the incorporated City and within the



urban limit line established by the General Plan. (See Objective LU 3, AG 1.6 and Policy LU 3.1)

All urban development permitted by the FTSSP will be annexed into the City of Fairfield. Section 11.0, Utilities Plan, of the Specific Plan has detailed information about the proposed municipal services that will be provided to the Project site.

Policy OS 2.1: All future Master Plans shall include adequate provisions for incorporating open space buffers. View corridors, watersheds, and prominent ridges shall be protected, and development on unstable soils shall be discouraged. (See Objective UD 5)

The FTSSP will create a compact, efficient community with a minimal footprint. Nearly 60 percent of the Specific Plan area will be conserved as active or passive open space by the Project. The Specific Plan preserves those portions of the Project site with view corridors, watersheds, and prominent ridges as open space in order to conserve soils and vistas and does not propose development on ridgelines. The Specific Plan will designate open space existing flood plain areas, areas containing natural and sensitive resources, along with a variety of agricultural and recreational uses and open space buffers.

Policy OS 2.2: Establish and maintain open space buffer zones between Vallejo and Fairfield along I-80 and between Fairfield and Benicia along I-680, identified as Open Space Planning Area 1 on the Open Space Planning Area map.

The FTSSP is not within the boundaries of this Planning Area.

Policy OS 2.3: All land uses and activities in Open Space Planning Area 1, also known as the Benicia/Fairfield/Vallejo Buffer as shown in Exhibit OS-1, shall conform to the policies contained in the Tri-City and County Cooperative Plan for Agriculture and Open Space Preservation.

The FTSSP is not within the boundaries of this Planning area.

Policy OS 2.4: Preserve open space and agricultural lands in Green Valley.

This policy is not applicable because the FTSSP lies outside Green Valley.

Policy OS 2.5: Establish a permanent open space buffer between Fairfield and Vacaville in conjunction with the Vacaville-Fairfield-Solano Greenbelt Authority.

The FTSSP establishes a boundary and funding mechanism for permanently preserving a greenbelt between the two cities. Overall, the Specific Plan would result in a net increase of 400 acres of Greenbelt above the original greenbelt concept. Approximately 148 acres would be removed from the Greenbelt east of Vanden Road and 115 acres of land would be removed from the Greenbelt west of Vanden Road. Within the Vanden Road corridor, about 663 acres

would be added to the Greenbelt. In addition, 1,647 acres would be added to the Greenbelt along the area identified as the “urban buffer” in the vicinity of the Travis AFB runway.

Policy OS 2.6: Maximize open space preserved to enhance City identity and preserve surrounding natural environments. (See Objective UD 5)

Nearly 60 percent of the Specific Plan area will be conserved as active or passive open space by the Project. Key natural habitats are preserved and protected, and the mitigation measures in the Project EIR will replace or mitigate those natural areas impacted by urban development in the Project area.

Policy OS 3.1: Open Space dedications shall provide a plan for funding to insure that financing for interim maintenance is provided.

The Community Facilities District shall include funding for open space acquisition and maintenance at a rate equivalent to City of Fairfield CFD #2004. These funds:

- *shall be used to acquire and maintain open space lands in PA7*
- *may be used to acquire and maintain open space lands within the Vacaville-Fairfield-Solano Greenbelt.*
- *shall not be used to acquire or maintain habitat conservation lands which are needed to satisfy the requirements of US Fish and Wildlife Service, California Department of Fish and Game, 404 Permit requirements of the US Army Corps of Engineer or similar permits.*
- *shall not be used to maintain the Great Park or similar public parks within the Specific Plan area*
- *may be used to satisfy the requirements of US Fish and Wildlife Service, California Department of Fish and Game, 404 Permit requirements of the US Army Corps of Engineer or similar permits, when City is permittee for City improvements.*

Policy OS 3.2: Ownership and management responsibility of public open space shall be assigned to the agency/organization best suited to meeting this responsibility. Adequate security of open space shall be provided to insure that applicable laws and regulations are enforced.

Ownership and management of open space lands in the FTSSP will be vested in the City of Fairfield, Solano County, or a private designee such as the Solano Land Trust. In some cases, lands may remain under private ownership with natural values preserved through conservation easements or other mechanisms. If a regional park agency is established, the recreational and lands with significant natural amenities may be transferred to such an agency at that time. A Management Plan will be prepared to ensure that security and other public safety issues are addressed.

Policy OS 3.3: Residents, property owners, and users shall contribute to maintaining public open spaces.

The Project will establish a Community Facilities District with funding for open space maintenance. .

Policy OS 4.1: New development shall fund its share of open space.

The Community Facilities District shall include funding for open space acquisition and maintenance at a rate equivalent to City of Fairfield CFD #2004.

Policy OS 4.2: Require new development in northeastern Fairfield to dedicate land or pay an in-lieu fee for the acquisition of open space land within or in the vicinity of the Vacaville-Fairfield-Solano Greenbelt.

The FTSSP establishes a boundary and funding mechanism for permanently preserving a greenbelt between the two cities.

Policy OS 4.3: All future areawide plans shall include appropriate mechanisms for acquisition of open space.

This Plan includes a comprehensive program for the acquisition of open space.

Policy OS 4.4: An equitable balance shall be sought between development density and open space to be preserved.

The FTSSP will create a compact, efficient community with a minimal footprint. FTSSP land uses include, residential, employment/industrial, commercial, public/quasi-public, and open space. Nearly sixty percent of the Specific Plan area will be conserved as active or passive open space by the Project. This land will be integrated with and surround the 6,800 dwelling units planned for the Specific Plan.

Policy OS 4.5: Promote clustering of housing units to preserve the hillsides, ridges, and a maximum amount of open space. (See Policy LU 13.1 and Policy UD 5.3)

The FTSSP will create a compact, efficient community with a minimal footprint. FTSSP land uses include, residential, employment/industrial, commercial, public/quasi-public, and open space. Nearly sixty percent of the Specific Plan area will be conserved as active or passive open space by the Project. This land will be integrated with and surround the 6,797 dwelling units planned for the Specific Plan.

Policy OS 4.6: Utilize development projects as a means for permanently preserving open space by encouraging mechanisms such as land pooling and transfer of development rights within Open Space Planning Areas for the preservation of significant open space features and lands.

The development permitted in the FTSSP will fund the acquisition of significant open space resources in the vicinity through development impact fees and the establishment of a Community Facilities District. The plan clusters development into a compact, mixed use form that

allows for significant new housing, shopping, offices, and employment generating uses while preserving the greenbelt and significant natural resources.

Policy OS 5.1: A study shall be conducted by staff to identify, analyze, and establish open space funding mechanisms.

The FTSSP specifically studied and addressed open space funding mechanisms for the specific plan area.

Policy OS 5.2: Through one or more of the following programs, generate funds for acquisition and management of open space.

The FTSSP generates funds for acquisition and management of open space in the planning area.

Policy OS 5.3: Place appropriate funding mechanisms on the local ballot.

The funding mechanisms established for the FTSSP will not require a ballot measure.

Policy OS 6.1: Preserve the views of hills and other vistas surrounding Fairfield. (See Policy UD 5.2 and Policy HS 4.7)

The FTSSP will preserve Cement Hill and a major greenbelt between Fairfield and Vacaville. Nearly 60 percent of the Specific Plan area will be conserved as active or passive open space by the Project. The Specific Plan preserves those portions of the Project site with view corridors, watersheds, and prominent ridges as open space in order to conserve soils and vistas and does not propose development on ridgelines. The Specific Plan will designate open space existing flood plain areas, areas containing natural and sensitive resources, along with a variety of agricultural and recreational uses and open space buffers.

Policy OS 6.2: Continue to administer adopted preservation ordinances for trees, streams and hillside development. (See Policy UD 5.3)

The FTSSP establishes specific policies for the preservation of hillsides, streams, wetlands, and trees. New trees planted as a result of development in the area will be subject to the Fairfield Tree Preservation Ordinance.

Policy OS 6.3: Require master planning approaches to regulate future developments within designated Open Space Planning Areas.

The FTSSP impacts the Vacaville-Fairfield Greenbelt Open Space Planning Area. As described elsewhere in this document, the FTSSP is a master plan which will regulate all future development in the planning area. Nearly sixty percent of the Specific Plan area will be conserved as active or passive open space by the Project.

Policy OS 6.4: All grading shall be integrated and compatible with adjacent areas so as to create a natural topographical appearance and avoid abrupt changes in slope.





A natural topographical appearance is an integral part of the design of FTSSP. The Specific Plan directs development away from hillsides and grazing lands with grades over 30 percent. Any grading will be minimized the extent feasible while minimizing hazards and visual impacts and maximizing safety.

Policy OS 6.5: New development within a Scenic Vista Area or its viewshed shall comply with the policies and guidelines in the Scenic Vistas and Roadways Plan. (See Objective UD 5)

This policy is not applicable because there are no designated Scenic Roadways within or adjacent to the Project site and the Project site itself is not within a Scenic Vista Area designated by the City of Fairfield's Scenic Vistas and Roadways Plan.

Policy OS 6.6: Significant natural features shall be preserved wherever development is allowed in a Scenic Vista Area, and views of these significant features shall not be obstructed.

Significant natural features and open space will be preserved by the FTSSP.

Policy OS 6.7: New development along a Scenic Roadway shall comply with the policies and guidelines in the Scenic Vistas and Roadways Plan.

This policy is not applicable because there are no designated Scenic Roadways within or adjacent to the Project site and the Project site itself is not within a Scenic Vista Area designated by the City of Fairfield's Scenic Vistas and Roadways Plan.

Policy OS 6.8: Preserve the character of Nelson Hill through establishment of open space/recreation in conjunction with development projects. (See LU Nelson Hill Policies 4, 5, 6, and 7)

This policy is not applicable because Nelson Hill is not a part of the FTSSP.

Policy OS 6.9: Maintain the scenic vistas of the rolling hills that are seen from the urban areas of Fairfield.

The Specific Plan preserves those portions of the Project site with view corridors, watersheds, and prominent ridges as open space in order to conserve soils and vistas and does not propose development on ridgelines. The Specific Plan will designate open space existing flood plain areas, areas containing natural and sensitive resources, along with a variety of agricultural and recreational uses and open space buffers.

Policy OS 6.10: The Cordelia Commons development (generally located to the west of Nelson Hill, north of Cordelia Road and east of Dan Wilson Creek) shall include public open space on Lookout Hill and preserve the marsh area and significant cultural resources. (See LU Nelson Hill Policy 11)

This policy is not applicable because Cordelia Commons is not a part of the FTSSP.

Policy OS 6.11: Permanently preserve open space above the 200 foot contour on Cement Hill because of its dominant visual significance to the City. If the City ultimately acquires this open space, it shall develop publicly accessible trails and vista points on Cement Hill to take advantage of the outstanding views of Fairfield and the Valley.

This policy is not applicable because this portion of Cement Hill is not a part of the FTSSP.

Policy OS 7.1: Establish policies to protect indigenous wildlife and their habitats. Support the Solano County Water Agency Habitat Conservation Plan (HCP) preparation effort and, if the HCP is approved and adopted, implement its requirements.

It is assumed that the SMHCP will be finalized and adopted before the Specific Plan is implemented. It is the City's desire that mitigation for Project impacts on biological resources be mitigated through participation in the SMHCP, by implementing all measures described for the respective species in the SMHCP. In addition, the FTSSP EIR includes mitigation measures and policies for wildlife habitat. Preserving over 60 percent of the Project area will help maintain the most significant open space and habitat lands as permanent open space.

Policy OS 7.2: Work with the County to preserve Green Valley Falls as a nature reserve. Plan for limited public access in the environmentally sensitive areas of the Green Valley canyon.

Green Valley Falls is not within the boundaries of the FTSSP.

Policy OS 7.3: Work with the County to ensure that development projects within the Rockville Hills/Stoneridge Open Space Planning Area to achieve minimal alteration of existing land contours and natural drainage channels.

Rockville Hills is not within the boundaries of the FTSSP.

Policy OS 7.4: Provide for the permanent protection of wildlife habitat areas.

The Project shall not conflict with local policies or ordinances protecting biological resources, such as the City of Fairfield General Plan and City ordinances. The Specific Plan includes establishment of approximately 869 acres of open space that would preserve most of the highest quality habitats in the Specific Plan area, including areas identified in the draft SMHCP as medium- and high-value conservation areas. The Noonan Ranch Conservation Bank has already been established on 203 acres in the southern portion of the Specific Plan area, east of Vanden Road, in a SMHCP high-value conservation area. The Noonan Ranch Conservation Bank preserves 3.96 acres of seasonal drainage, 0.15 acre of freshwater marsh, and 23.85 acres of



vernal pools in a grassland matrix and supports vernal pool fairy shrimp, California tiger salamander, and a large population (between 12.4 and 17.2 million plants estimated in 2009) of Contra Costa goldfields. Another 450 acres of high-value conservation area would be designated adjacent to the established Noonan Ranch Conservation Bank as part of the Specific Plan area. These open space conservation areas preserve the majority of the vernal pools and other seasonal wetlands in the Specific Plan area, including the large playa-type vernal pools and wetlands known to be occupied by Contra Costa goldfields.

Policy OS 7.5: Identify and protect vernal pools located in the entire General Plan Area. For vernal pools located in the Technology Park and Travis Reserve, establish a habitat reserve similar to the Jepson Prairie Nature Preserve.

The Specific Plan includes establishment of approximately 1,522 acres of open space that would preserve most of the highest quality habitats in the Specific Plan area, including areas identified in the draft SMHCP as medium- and high-value conservation areas. In addition, the following mitigation measures provided in the FTSSP EIR identify and protect vernal pools in the area.

- *Mitigation Measure 4.4-1a: Secure Clean Water Act Section 404 Permit and Implement All Permit Conditions; Ensure No Net Loss of Functions and Values of Wetlands, Other Waters of the United States, and Waters of the State, in Section 4.4, Biological Resources, of the FTSSP EIR would ensure no net loss of function and acreage of wetlands, other waters of the United States, and waters of the state.*
- *Mitigation Measure 4.4-2a: Secure Take Authorization for Federally Listed Vernal Pool Invertebrates and Implement All Permit Conditions; Preserve and Restore Wetland and Adjacent Upland Habitat Consistent with the SMHCP Conservation Strategy, in Section 4.4, Biological Resources, of the FTSSP EIR would ensure that substantial habitat for vernal pool invertebrates would be preserved in the Specific Plan Area and vicinity consistent with the conservation strategy in the proposed SMHCP and that habitat lost as a result of project implementation would be replaced through restoration of degraded vernal pool and seasonal wetland habitat.*
- *Mitigation Measure 4.4-3a: Secure take Authorization for Federally Listed Contra Costa Goldfields and Implement All Permit Conditions, Implement Contra Costa Goldfields Core Population Development Criteria Consistent with the SMHCP, Establish New Populations of Contra Costa Goldfields, in Section 4.4, Biological Resources, of the FTSSP EIR would ensure that occupied habitat is avoided to the extent feasible, that the majority of occupied habitat present is preserved in perpetuity, and that populations removed as a result of project implementation are replaced through establishment of new populations.*

Policy OS 7.6: Avoid the potential negative impacts of increased human activity on sensitive habitat areas when establishing new recreational facilities or programs.

The Specific Plan includes establishment of approximately 1,522 acres of open space that would preserve most of the highest quality habitats in the Specific Plan area, including areas identified in the draft SMHCP as medium- and high-value conservation areas. In addition Mitigation Measure 4.4-4: Map Riparian Habitat; Implement Avoidance and Mitigation Measures, Secure and Implement Section 1602 Streambed Alteration Agreement, in Section 4.4, Biological Resources, of the FTSSP EIR, requires development of a mitigation and monitoring plan, ensuring adequate compensation for the loss of riparian habitat and implementation of the plan as a condition of the streambed alteration permit.

Policy OS 7.7: Promote the preservation of existing mature trees and encourage the planting of appropriate shade trees in new developments. (See Policy UD 6.1 and Program UD 6.1 A)

Section 12.0, Landscape Plan, of the FTSSP includes tree planting discussion in the Specific Plan Area.

Policy OS 7.8: For each proposed development site within oak woodland or oak savannah biological communities, the City will require qualified biologists to identify and map all sensitive biological resources, including special-status species (as defined above) and their habitats; using methods and protocols developed or acceptable by the U.S. Fish and Wildlife Service and California Department of Fish and Game, make recommendations for avoiding sensitive biological resources to the extent feasible; and flag or fence sensitive resources to be avoided. This requirement shall apply prior to City consideration of any development proposal for the site.

The FTSSP area has little oak woodland or oak savannah. Mitigation Measure 4.4-6a: Avoid removal of oak trees to the extent feasible, compensate for the unavoidable loss of oak trees and obtain a tree removal permit, in Section 4.4, Biological Resources, of the FTSSP EIR, requires project applicant to identify and avoid native oak trees to the extent feasible and compensate for unavoidable removal of oak trees as a condition of the tree removal permit.

Policy OS 7.9: Promote the protection of special status plant populations. Programs OS 7.9 A – 7.9 C should be applied to the mitigation of special-status plant species, but should be superseded when strategies in the City’s Habitat Conservation Plan are developed and approved.

The Specific Plan has been designed to provide consistency with the anticipated conservation strategy of the proposed SMHCP. Measures have been incorporated into the project design specifically to meet the particular goals of the current draft of the SMHCP, including preserving areas identified in the draft SMHCP as medium and high value

conservation areas, and designing development to minimize and avoid impacts on species covered in the SMHCP and their habitats. In addition, all of the mitigation measures provided in Section 4.4, Biological Resources, of the FTSSP EIR are consistent with the mitigation requirements presented in the conservation strategy section of the current draft SMHCP.

Policy OS 7.10: Promote the prevention of the spread of noxious weeds.

Mitigation Measure 4.4-6a: Identify and map noxious weed infestations, avoid infested areas to the extent feasible, Section 4.4, Biological Resources, of the FTSSP EIR, requires project applicant of all phases to identify and avoid noxious weed infestations or clean weed propagules from equipment.

Policy OS 8.1: Mitigate air pollution from fixed and vehicular sources as required by state and regional air quality plans and programs. (See Policy LU 13.2 and Objective CI 12)

The FTSSP development will comply with all available and feasible mitigation measures, including the Basic Construction Mitigation Measures and Mitigation Measure 4.3-1: Operational Criteria Air Pollutant and Precursor Emissions in Section 4.3, Air Quality, of the FTSSP EIR.

Policy OS 8.2: Work with surrounding communities to develop regional solutions to air quality issues. (See Objective LU 5, Policy LU 5.1, and Policy CI 12.1)

The compact, mixed use, and transit oriented development in the FTSSP will potentially contribute to a solution to regional air quality issues. If population growth is channeled into more efficient development which reduces the use of private vehicles, this development will reduce total air pollutants. The FTSSP has been thus designated a Potential Priority Development Area by the Association of Bay Area Governments. This policy is not applicable because it is a City directed policy.

Policy OS 8.3: Encourage more efficient use of private vehicles and increased use of mass transit and alternative transportation modes. (See Objectives CI 6 and CI 7 and Policy CI 12.4)

The FTSSP development is designed to utilize the new Fairfield-Vacaville Multi-Modal Train Station, which will include a new Amtrak Capitol Corridor passenger rail station, bus transfer facility, and park-and-ride lot to improve accessibility to the Capitol Corridor intercity train service. In addition, the network of pedestrian and bicycle routes and the efficient, mixed use land use patterns envisioned in the plan will help reduce private vehicle use.

Policy OS 8.4: Encourage the retrofitting of existing buildings to be energy efficient. (See Objective HO 8)

The FTSSP addresses new development, which will be built according to the latest energy efficiency requirements and “green” building standards. Chapter 8.0, Sustainability and Climate Change, responds to climate change, by addressing construction related activities, vehicle miles traveled, water and energy use and open space preservation and landscaping. The City will adopt the mandatory provisions of CALGreen in the local building code, therefore, making it critical that future FTSSP buildings comply with CALGreen standards. It is anticipated that new structures built in compliance with the green building code will achieve at least a 15 percent reduction in energy usage when compared to the State’s mandatory energy efficiency standards.

Policy OS 8.5: Require water conservation and energy efficiency techniques to be incorporated into the design of all development projects. (See Policy UD 6.2 and Policy PF 4.8)

The FTSSP provides applicable policies in Chapter 8.0, Sustainability and Climate Change, which addresses water use. The City will adopt the mandatory provisions of CALGreen in the local building code, therefore, making it critical that future FTSSP buildings comply with CALGreen standards. The 2010 CALGreen standard calls for the reduction of indoor potable water by at least 20 percent from current code standards through the use of low and ultra low flow fixtures. It also requires buildings to have more efficient controllers for exterior irrigation system.

Policy OS 8.6: Support recycling activities throughout the City. (See Policy PF 13.1 and Program PF 13.1 A)

The FTSSP provides applicable policies in Chapter 8.0, Sustainability and Climate Change, to respond to climate change, decrease greenhouse gas emissions and enhance air quality through addressing construction related activities, vehicle miles traveled, water and energy use and open space preservation and landscaping. The City will adopt the mandatory provisions of CALGreen in the local building code, therefore, making it critical that future FTSSP buildings comply with CALGreen standards. The CALGreen code mandates that all new buildings must divert waste to recycling or salvage a minimum of 50 percent of nonhazardous construction waste and demolition debris generated onsite. This does not represent a significant change from the current standard since the voluntary tier one standard of a 65 percent diversion rate is considered to be achievable in most areas of California. Future construction management in the FTSSP area should aim to achieve this 65 percent diversion target. The development shall also provide proper recycling facilities, such as trash cans, for recyclable and non-recyclables.

Policy OS 8.7: Encourage the use of recycled water where feasible.





The FTSSP does not directly address the use of recycled water; however, water saving techniques in landscaping and use of native plants is proposed and encouraged.

Policy OS 9.1: Promote restoration and establish permanent mechanisms to protect wetlands and riparian corridors.

Policy OS 9.2: Manage all seasonal creeks and other drainage courses so as to protect and enhance the Suisun Marsh. (See Policy PF 8.4 A)

Policy OS 9.3: Secure wetlands designation for appropriate lands in the Fairfield/Vacaville Greenbelt and for lands in the Travis AFB/Jepson Prairie Planning Area.

The Specific Plan includes establishment of approximately 1,522 acres of open space that would preserve most of the highest quality habitats in the Specific Plan area, including areas identified in the draft SMHCP as medium- and high-value conservation areas. While the Project area does not directly impact the Suisun Marsh, the policies and programs contained in the EIR will address indirect and regional impacts on the marsh and its watersheds.

The following mitigation measures provided in the FTSSP EIR identify and protect vernal pools in the area.

- *Mitigation Measure 4.4-1a: Secure Clean Water Act Section 404 Permit and Implement All Permit Conditions; Ensure No Net Loss of Functions and Values of Wetlands, Other Waters of the United States, and Waters of the State, in Section 4.4, Biological Resources, of the FTSSP EIR would ensure no net loss of function and acreage of wetlands, other waters of the United States, and waters of the state.*
- *Mitigation Measure 4.4-2a: Secure Take Authorization for Federally Listed Vernal Pool Invertebrates and Implement All Permit Conditions; Preserve and Restore Wetland and Adjacent Upland Habitat Consistent with the SMHCP Conservation Strategy, in Section 4.4, Biological Resources, of the FTSSP EIR would ensure that substantial habitat for vernal pool invertebrates would be preserved in the Specific Plan Area and vicinity consistent with the conservation strategy in the proposed SMHCP and that habitat lost as a result of project implementation would be replaced through restoration of degraded vernal pool and seasonal wetland habitat.*
- *Mitigation Measure 4.4-3a: Secure take Authorization for Federally Listed Contra Costa Goldfields and Implement All Permit Conditions, Implement Contra Costa Goldfields Core Population Development Criteria Consistent with the SMHCP, Establish New Populations of Contra Costa Goldfields, in Section 4.4, Biological Resources, of the FTSSP EIR would ensure that occupied habitat is avoided to the extent feasible, that the majority of occupied habitat present is preserved in perpetuity, and that populations removed as*

a result of project implementation are replaced through establishment of new populations.

Policy OS 9.4: Allow no development on the east side of I-680 between Parish Road and the Cordelia historic area.

Policy OS 9.5: Support acquisition of key parcels on the periphery of the Suisun Marsh to ensure the integrity of the entire marsh.

Policy OS 9.6: Continue to endorse the integrity of the Suisun Marsh Secondary Management Zone.

Policy OS 9.7: Promote only low intensity recreational activities which are compatible with the marsh environment adjacent to Suisun Marsh.

The FTSSP does not address the I-680 corridor or the Suisun Marsh.

Policy OS 9.8: Preserve natural water courses through requirements of land dedication and open space improvement imposed during the land development process.

As a goal of the Specific Plan, conservation easements shall be placed on all land designated as Open Space Conservation or Open Space Recreation. In some cases, the placement of easements shall be done in conjunction with development entitlements and, in other cases, the City or a nonprofit organization may acquire such easements.

Policy OS 9.9: The proponents of new development projects along important freshwater marsh, riparian, or open water habitat areas that are not already covered by the City's Creekside Protection Ordinance shall provide an assessment of the habitat. Based on the assessment, an enhancement or restoration plan would be required to be prepared by a qualified person(s) experienced in the development and implementation of riparian restoration and enhancement plans.

Mitigation Measure 4.4-6c: Assess riparian, marsh, and stream habitat, develop and implement an enhancement or restoration plan for riparian and marsh habitat, implement Mitigation Measures 4.4-1 and 4.4-4, in Section 4.4, Biological Resources, of the FTSSP EIR, requires project applicant to restore and enhance these habitat on a no net loss basis.

Policy OS 9.10: Avoid or reduce the loss of riparian habitat to the extent feasible for each development site.

Mitigation Measure 4.4-4: Map Riparian Habitat; Implement Avoidance and Mitigation Measures, Secure and Implement Section 1602 Streambed Alteration Agreement, in Section 4.4, Biological Resources, of the FTSSP EIR, requires development of a mitigation and monitoring plan, ensuring adequate compensation for the loss of riparian habitat and implementation of the plan as a condition of the streambed alteration permit.

Policy OS 10.1: Conserve valuable resources by promoting educational activities and encouraging conservation efforts by citizens, property owners, and builders.

Residents of the FTSSP will participate in City and School District educational activities.

Policy OS 10.2: Assign to the Open Space Commission the responsibility for the official inventory of historical and archeological sites.

The Planning Commission has assumed this responsibility and archaeological resources were addressed in the Project EIR.

Policy OS 10.3: Consult with the California Archaeological Inventory Northwest Information Center at Sonoma State University on any project that could have an impact on cultural resources.

In the preparation of the FTSSP Environmental Impact Report (EIR), a record search from the Northwest Information Center at Sonoma State University was conducted on March 29, 2010.

Policy OS 10.4: Avoid impacts on cultural resources when archeological studies reveal the presence of cultural resources at a development site. If avoidance is infeasible, require site testing by a qualified archeologist to determine the significance of the resources, and implement recommended mitigation measures.

Policy OS 10.5: Halt construction at a development site if cultural resources are encountered unexpectedly during construction and require consultation with a qualified archeologist to determine the significance of the resources.

Policy OS 10.7: Require archeological studies by a qualified archeologist (as defined by the Secretary of the Interior's standards) in areas of archeological significance prior to approval of development projects.

The FTSSP EIR includes Mitigation Measure 4.5-2 which states, "If an inadvertent discovery of cultural materials (e.g. unusual amounts of shell, animal bone, glass, ceramics, structure/building remains, etc.) is made during project-related construction activities, ground disturbances in the area of the find shall be halted and a qualified professional archaeologist will be notified regarding the discovery. The archaeologist shall determine whether the resource is potentially significant per the CRHR and develop appropriate mitigation to protect the integrity of the resource and ensure that no additional resources are impacted. Mitigation could include, but not necessarily be limited to preservation in-place, archival research, subsurface testing, or contiguous block unit excavation and data recovery."

Policy OS 10.6: Promote a museum or education center to build awareness of our area's history and resources.

The FTSSP will not address this issue.

Policy OS 10.8: Prepare and maintain an inventory of historic structures within the General Plan Area per the City's adopted Historic Preservation Ordinance and, where appropriate, promote the inclusion

of these structures on the California Register of Historic Resources and the National Register of Historic Places. Work with property owners in seeking registration of historical structures as State Historic Landmarks or listing on the National Register of Historic Places.

This Plan does not directly address this City-wide policy.

Policy OS 10.9: Promote the preservation and restoration of historical sites and structures within the General Plan Area that are significant to the City's or the region's cultural or historic background. (See Policy UD 3.4)

No cultural resources or historical sites were documented within or in the immediate vicinity of the Specific Plan Area. Any historical sites and structures that are significant to the City's or the region's cultural or historic background would be preserved in accordance with California State law.

Policy OS 11.1: Secure public access to open space to the maximum extent feasible.

Nearly sixty percent of the Specific Plan area will be conserved as active or passive open space by the Project. This land will be integrated with and surround the 6,797 dwelling units planned for the Specific Plan. A comprehensive bicycle and pedestrian pathway network shall be established through the Specific Plan area.

Policy OS 11.2: Promote trail systems through the open spaces, and pedestrian/bicycle links throughout the City, per the adopted Master Trails Plan. (See Objective CI 9, Policies CI 9.1 and CI 9.2, and Program CI 9.2 A)

The City's Linear Park, a multi-use trail system, will be extended from Peabody Road, through the Specific Plan area to Vacaville city limits. An additional multi-use trail will be extended from the Linear Park to Center school, including a pedestrian overcrossing of Vanden Road and Union Pacific railroad, along an abandoned railroad right-of-way.

Policy OS 11.3: Require recreation and trails programs as a condition of approval for future development projects. Plans should provide access to trail heads located on adjacent public lands.

A comprehensive bicycle and pedestrian pathway network will be established through the Specific Plan area.

Policy OS 11.4: Actively participate with other governmental entities (cities, county, state, and federal) or agencies in the acquisition, management, and use of recreational/open space lands and facilities of mutual interest. (See Policy LU 5.3 and Program LU 5.3 A)

Policy OS 11.5: Where a project involves potential open space, natural resource reserves, or recreational lands of interest to more than one entity, the City shall work cooperatively with the other involved agencies. (See Policy LU 5.3 and Program LU 5.3 A)

The planning process for the FTSSP has involved the City of Vacaville, the County of Solano, and Solano Transportation Authority. Open space issues have been outlined and discussed in cooperation with these agencies.

Policy OS 11.6: Maintain water courses within the General Plan Area as components of the recreation corridor system. Public access within sensitive habitat areas of waterway shall be considered individually to ensure protection of the habitat resource.

While there are no significant stream corridors or other water courses, the FTSSP includes wetlands adjacent to the linear park. There will be public access to some open space areas and habitat resources.

Policy OS 11.7: Preserve public access corridors for walkers, bicyclists, and equestrians along future floodway improvement projects whenever feasible.

A comprehensive bicycle and pedestrian pathway network shall be established through the Specific Plan area.

Policy OS 11.8: Utility easement corridors shall be designated for recreational open space unless an acceptable trail alternative is included in a development plan.

Some utilities will be co-located with streets and public open space corridors, consistent with this Specific Plan.

Policy OS 12.1: Develop park lands at the local and community levels to meet the recreational needs of current and future Fairfield residents.

The FTSSP includes a park system that is intended to provide residents and visitors a meaningful public space experience while promoting active participation and healthy lifestyles. The parks and recreational areas will serve as focal points within the Specific Plan area by offering safe opportunities for people to gather, recreate, exercise, rest, and play. The FTSSP proposes a parks and recreation program that includes recreation centers as well as linear parks, pocket parks, neighborhood parks, a Lake Park, and a community park (Great Park).

Policy OS 12.2: Update and adopt a Parks, Open Space, Leisure, and Cultural Services Plan that identifies potential programs and recreation facility improvements for the Fairfield community. The city should examine funding sources to enable the city to meet its parkland-per-population standards by buildout of the general plan and provide for reestablishment and renovation of aging park facilities.

The FTSSP itself addresses the park and recreational needs of future residents as well as providing for facilities that benefit the entire community. The City has reviewed and adopted plans for its open space and recreation services on an ongoing basis, and any future updates will incorporate the parks addressed in the FTSSP.

Policy OS 12.3: Establish neighborhood parks of approximately five to seven acres at a ratio of 1.5 acres of usable park land per 1,000 residents throughout the City. Neighborhood parks are intended to serve a 1/2 mile radius area.

The FTSSP will meet this standard in new development constructed under the Specific Plan. The FTSSP proposes a parks and recreation program that includes recreation centers as well as linear parks, pocket parks, neighborhood parks, a Lake Park, and a community park (Great Park). One public neighborhood park is proposed for the Specific Plan area in the southern portion of the Project site south of the train station at approximately 5 to 6 acres. It may be programmed for active field sports, sport courts, picnic areas, play structures, and tot lots. The Lake Park is a 24-acre public park proposed to be centrally located within the FTSSP in PA 4. The amenities may include, but are not limited to, promenade, parking, lake trail, seating plaza, benches, great meadow, amphitheater, iconic architecture, themed gazebo, and fitness stations.

Policy OS 12.4: Establish community parks of approximately 40 acres at a ratio of two acres usable park lands per 1,000 residents throughout the City. Community parks are intended to serve a two mile radius.

The Great Park is an approximately 50-acre public community park including active and passive recreation opportunities for the community. The amenities will provide activities for all ages and may include soccer and baseball fields, basketball courts, shade structures, picnic areas, activity lawn, skate park, jogging trails, trailhead with interpretive signage, orchard, amphitheater, children's play areas, and a concession/restroom/storage building.

Policy OS 12.5: Establish regional parks to meet regional open space and recreation needs.

While the FTSSP does not include a regional park, the specific plan provides for significant public open space that may be suitable for trails and other passive recreational; uses. .

Policy OS 12.6: Develop a new recreational and outdoor cultural complex for softball, soccer, picnicking, and outdoor cultural events.

The proposed 176 acres of public park land includes soccer and baseball fields, skate park, trails, picnic areas, and lake.

Policy OS 12.7: Preserve and develop the entire Linear Park system, from Solano Community College into the Fairfield Train Station Specific Plan area, as illustrated on the Land Use Diagram, exclusively as a recreation facility in order to provide an alternative non-motorized vehicular linkage among recreation and parks, schools, shopping, industry, residential, and other community activity centers.

The 42-acre public Linear Park is proposed as a major open space linkage and activity spine connecting residential neighborhoods to open space recreation areas.



A.8 Public Facilities and Services Element



Policy PF 1.1: New development shall be phased according to the capacity of public facilities and services to serve new development. (See Policy LU 4.2)

The FTSSP establishes through a development agreement and mitigation measures in the EIR specific standards for the phasing and construction of public infrastructure. Prior to development of each subdivision, apartment complex, or commercial building, the availability of infrastructure will be confirmed.

Policy PF 1.2: Encourage the concurrent (as opposed to piecemeal) annexation of adjacent unincorporated properties in order to facilitate the formation of assessment districts, Mello-Roos districts, and other financial mechanisms which will provide public facilities in an efficient and effective manner.

The Development Agreement for the Project will include a that ensures that all new development within the Specific Plan area will pay all capital costs for Public Improvements needed by the development in accordance with standards and policies adopted by City and responsible agencies; new residential development within Specific Plan shall generate tax revenue to City equal to or greater than the cost of Municipal Services; sufficient Backbone Infrastructure will be constructed in the first phase of development so that City may provide services and facilities for residents and businesses within Specific Plan which meet or exceed adopted standards and policies; development of Specific Plan will improve, not diminish City's overall financial situation; and the overall property tax burden for future residents will be consistent with industry practices.

Policy PF 2.1: New development shall be responsible for the public costs attached to each development project, which include, but are not limited to, the acquisition of permanent open space, the provision of adequate school facilities, and the provision of streets, street lighting, sidewalks, landscaping, storm drains, and other infrastructure needs.

Policy PF 2.2: New development shall be responsible for paying a financial contribution to mitigate the effect of the development on the provision of such public services as police and fire protection, public education, water, and sewer.

Policy PF 2.3: Construction permits shall not be granted until the developer provides for the installation and/or financing of needed public facilities.

The financial plan and Development Agreement will outline how new development within Specific Plan will pay all capital costs for Public Improvements needed by the development in accordance with standards and policies adopted by City and responsible agencies.

Policy PF 3.1: Upgrade facilities and services that experience deterioration or obsolescence.

New facilities constructed to serve the development in the FTSSP will meet all current standards. In the future, these facilities will be maintained by the City or other responsible agencies per City or agency standards. Existing facilities which require upgrades in order to serve the Project area will be funded and constructed as outlined in the Financing Plan and Development Agreement.

Policy PF 4.1: The City shall condition approval of new development projects on the availability of adequate water supply and infrastructure to serve the new development.

The FTSSP addresses water supply and infrastructure, and the Project Development Agreement and Financing Agreement establish mechanisms to ensure that necessary water and infrastructure is available. The City's overall Capital Improvements Plan addresses overall system capacity and delivery infrastructure. As elsewhere in the City, applicants for development projects will submit utility improvement plans for review and approval by FSSD and the City of Fairfield Department of Public Works to ensure that adequate service and applicable codes are met.

Policy PF 4.2: The City shall work with other urban water agencies to secure additional water supplies for new development so that the local affordable agricultural water supply is not reduced.

Policy PF 4.3: The City shall acquire water supplies to serve all foreseeable needs in the General Plan with a minimum 90 percent reliability (e.g. water supplies may be deficient in no more than 10 percent of the years).

Policy PF 4.4: The City shall extend water service to new areas in accordance with the capital phasing of the City's Water Service Master Plan.

Policy PF 4.5: The City should attempt to obtain a minimum 5 day supply of more localized raw water storage than the Solano Project at Lake Berryessa (nearly 30 miles away) in order to provide additional water supply reliability.

The City undertakes comprehensive planning for its water supply which addresses long term supplies, storage, cooperative agreements with other agencies, and the purchase of water rights. The City continues to seek water supply to support the growth envisioned in the General Plan. Water supply was addressed in the FTSSP and the EIR and the water needed to support development of this project will not impact agricultural water supplies.

Policy PF 4.6: The City should encourage development of a nonpotable water system to serve large irrigation and industrial process users in order to extend potable water supplies and reduce maximum day demands on the potable system.



The City will adopt the mandatory provisions of CALGreen in the local building code, therefore, making it critical that future FTSSP buildings comply with CALGreen standards. The 2010 CALGreen standard calls for the reduction of indoor potable water by at least 20 percent from current code standards through the use of low and ultra low flow fixtures. It also requires buildings to have more efficient controllers for exterior irrigation system.

Policy PF 4.7: The City shall encourage and actively promote long-term water conservation according to the water management programs established in the City's Urban Water Management Plan.

As a key sustainability indicator, reducing overall water usage of the Project is an invaluable advancement for FTSSP. The FTSSP provides applicable policies in Chapter 8.0, Sustainability and Climate Change.

Policy PF 4.8: New development shall include water conservation features and drought resistant landscaping. (See Policy OS 8.5)

As a key sustainability indicator, reducing the overall water usage of the Project is an invaluable advancement for FTSSP. The FTSSP provides applicable policies in Chapter 8.0, Sustainability and Climate Change. Water conservation features and drought tolerant plant will be used for landscaping, as described in Section 12.0 Landscape Plan.

Policy PF 5.1: Treatment facilities shall meet or exceed current standards set by federal, state, or local regulatory agencies.

All public improvements dedicated to City shall conform to the Fairfield Municipal Code, the City's Standard Specifications, and where applicable the standards of the FSSD and SID unless a deviation from those standards is approved by the City in writing.

Policy PF 5.2: Treatment capacity should be maintained at 1 maximum day of demand plus 20 percent.

The combined capacity of the City's water treatment plants exceeds the projected demand at buildout of the Fairfield General Plan.

Policy PF 5.3: The City should have a goal to maintain a treated water storage capacity of 2 maximum days of demand.

The FTSSP does not impact this Policy.

Policy PF 5.4: Treated water storage reservoirs should be distributed throughout the service area to optimize system reliability during emergencies.

The City owns and operates a water system that includes 11 water storage reservoirs, which have a total storage capacity of more than 76 million gallons; it is in the process of developing more storage reservoirs to meet the City's treated water storage needs for the next 10 to 20 years. The City also owns and maintains 15 pump stations with a total capacity more than 30 mgd and 350 miles of water

pipelines. The FTSSP will include a nearby water storage reservoir that will serve the Project area during emergencies.

Policy PF 5.5: Treated water piping systems should be interconnected (“looped”) wherever economically feasible to facilitate the reliable delivery of water anywhere in the City.

Prior to or concurrently with the initial subdivision map within the Specific Plan, subdivision map applicants shall submit a Master Water System Plan to the City for its review and approval. The Master Water System Plan shall be substantially in accordance with the Conceptual Water System shown in Figure 11-1 of Chapter 11.0, Utilities, and shall include the entire Specific Plan area. Elements of the Master Water System Plan shall include:

- *water demand by land use,*
- *pipe diameters, and*
- *location and specifications for all reservoirs, booster pump(s), and hydropneumatic tank(s) location and specifications for all water storage facilities.*

Policy PF 5.6: Pipes should be sized to provide minimum operating pressure of 45 pounds per square inch (psi) under normal conditions and 20 psi under emergency conditions (e.g. fires). (See Program HS 4.5 A)

Prior to or concurrently with the initial subdivision map within the Specific Plan, the subdivision map applicant shall submit a Master Water System Plan to the City for its review and approval. The Master Water System Plan shall be substantially in accordance with the Conceptual Water System shown in Figure 11-1 of Chapter 11.0, Utilities, and shall include the entire Specific Plan area. Elements of the Master Water System Plan shall include:

- *water demand by land use,*
- *pipe diameters, and*
- *location and specifications for all reservoirs, booster pump(s), and hydropneumatic tank(s) location and specifications for all water storage facilities.*

Policy PF 6.1: Continue to provide sewer services and operate major public facilities through the Fairfield-Suisun Sewer District (FSSD).

Prior to or concurrently with the initial subdivision map within the Specific Plan, the subdivision map applicant shall submit a Master Sewer System Plan to the City for its review and approval. The Master Sewer System Plan shall be substantially in accordance with the Conceptual Sewer System shown in Figure 11-2 of Chapter 11.0, Utilities, and shall include the entire Specific Plan area. Elements of the Master Sewer System Plan shall include:

- *wastewater generation factors by land use,*



- *all flow information, and*
- *pipe diameters location and specifications for all lift stations.*

Policy PF 6.2: Support the Fairfield-Suisun Sewer District's efforts to continue to provide sewer system maintenance for all trunk lines (12" diameter and larger), pump stations and treatment plant facilities. The City of Fairfield will provide maintenance for all sewer trunk lines less than 12" in diameter.

Prior to or concurrently with the initial subdivision map within the Specific Plan, the subdivision map applicant shall submit a Master Sewer System Plan to the City for its review and approval. The Master Sewer System Plan shall be substantially in accordance with the Conceptual Sewer System shown in Figure 11-2 of Chapter 11.0, Utilities, and shall include the entire Specific Plan area. Elements of the Master Sewer System Plan shall include:

- *wastewater generation factors by land use,*
- *all flow information, and*
- *pipe diameters location and specifications for all lift stations.*
- *sewer lines smaller than 12 inches must comply with City's Standard Specifications. Sewer lines 12 inches or larger must comply with the standards of the FSSD.*

Policy PF 6.3: Closely coordinate with the FSSD regarding new development projections to allow FSSD to continue to construct sewerage capital improvements in a planned and orderly manner consistent with the levels of General Plan growth and recommendations in the updated "Sewer System Capacity Study."

Prior to issuance of a will-serve letter by FSSD, the applicant shall submit utility improvement plans for review and approval by FSSD and the City of Fairfield Department of Public Works to ensure that adequate service and applicable codes are met. Prior to the approval of each development phase of the Specific Plan, the project applicant shall submit to the FSSD for review of the finalized land use plan for each development phase. The FSSD shall submit feedback on the planning land uses and may require pretreatment facilities for land uses that may accommodate uses that could result in wastewater discharges with additional chemicals or corrosive materials not originally accounted for in the Specific Plan. The FSSD may also require additional individual facilities for land uses expected to result in excessive wastewater discharges that could potentially impeded the FSSD from providing adequate service for existing or other planned development.

Policy PF 6.4: Support the Fairfield-Suisun Sewer District's coordination with Travis Air Force Base and Anheuser Busch brewery on the sewage generation requirements during the General Plan buildout.

This policy does not apply to the FTSSP.

Policy PF 6.5: Require new development to be responsible for construction of all sanitary sewer lines serving such development (including oversizing of sewers if requested by Fairfield-Suisun Sewer District or the City; the costs of oversizing shall be borne by the beneficiary of the oversizing).

The Conceptual Sewer System addresses this issue (Figure 11-2). Individual subdivisions will submit master utility plans for each development to the City for its review and approval. The Master Sewer System Plan shall be substantially in accordance with the Conceptual Sewer System and shall include the entire Specific Plan area. Elements of the Master Sewer System Plan shall include:

- *wastewater generation factors by land use,*
- *all flow information, and*
- *pipe diameters location and specifications for all lift stations.*

Policy PF 6.6: Continue to support the Fairfield-Suisun Sewer District Act which requires annexation in order to receive Fairfield-Suisun Sewer District sewer service.

- *The FTSSP envisions annexation prior to any development.*

Policy PF 7.1: Promote water conservation. (See Policy PF 4.8)

The FTSSP will manage onsite water resources to reduce the demand for water, including re-using and recycling waste water and storm water, and minimizing impacts on natural surface and subsurface water flows and quality. A Sustainability Program, found in Chapter 8.0, focuses on the conservation and re-use of water, storm water and wastewater. In addition, FTSSP property owners shall work with the City to encourage development of a nonpotable water system to serve large irrigation and industrial process users in order to extend potable water supplies and reduce maximum day demands on the potable system.

Policy PF 7.2: The City shall promote the use of reclaimed water with priority to recreational, industrial and agriculture users and irrigation for landscaping along roadways.

The FTSSP will manage onsite water resources to reduce the demand for water, including re-using and recycling waste water and storm water, and minimizing impacts on natural surface and subsurface water flows and quality. A Sustainability Program, found in Chapter 8.0, focuses on the conservation and re-use of water, storm water and wastewater. In addition, FTSSP property owners shall work with the City to encourage development of a nonpotable water system to serve large irrigation and industrial process users in order to extend potable water supplies and reduce maximum day demands on the potable system.



Policy PF 8.1: The City shall condition approval of development projects on the provision of adequate storm drainage improvements. (See Policy HS 3.2)

Policy PF 8.2: The City shall extend storm drains to new areas in accordance with the phasing of a storm drainage master plan.

The FTSSP incorporates a Conceptual Storm Drain Reuse Facilities Diagram in Figure 11-4. All project applicants shall submit master utilities plans consistent with this diagram. A Master Storm Drain Reuse Plan shall be submitted with the Area Plan for PA4. Elements of the Plans shall include:

- *size and location of conveyance facilities,*
- *location and specifications for pump stations, and*
- *conceptual design for the reuse basin; the minimum size of the reuse basin shall be 30 acre-feet.*

Policy PF 8.3: Complete flood control and storm drainage improvement projects on the Capital Improvements list to permit timely development under the General Plan and to remove identified flood hazards.

FTSSP projects will be incorporated into the City's Capital Improvements Plan.

Policy PF 8.4: Improve long-term drainage management in the watersheds affecting the City of Fairfield by establishing cooperative relationships with other agencies or jurisdictions for the purpose of planning and coordinating drainage programs and policies on a regional basis. (See Policy HS 3.1)

Drainages which impact the FTSSP area will be addressed as elements in the City's overall drainage management system. This management will be funded by development fees and infrastructure investment outlined in the Specific Plan and any Development Agreements. There are only limited regional impacts in this planning area.

Policy PF 8.5: Detention basins should be considered for multiple use (recreation, parking, etc.) particularly larger basins, providing the basic detention function is not lost or impaired, and maintenance and liability issues are satisfactorily resolved.

The Lake Park shall be designed primarily as an aesthetic and recreational facility with a limited, secondary role in storm water detention. The Area Plan for PA4 shall include the following:

- *Lake Management Plan shall be prepared by a qualified expert and approved by City.*
- *The Lake Park shall be designed with a maximum of 1 foot of free-board for storm water detention.*

Policy PF 9.1: Natural and manmade channels, detention basins, and other drainage facilities shall be maintained to ensure that their full use and carrying capacity is not impaired.

Policy PF 9.2: Continue to require new development to discharge storm runoff at volumes no greater than the capacity of any portion of the existing downstream system by utilizing detention or retention or other approved methods, unless the project is providing drainage pursuant to an adopted drainage plan

Pursuant to City of Fairfield standards, nine storm water detention basins would be constructed such that the post development peak flows are 90 percent of the pre-development peak flows in a 100-year event.

Policy PF 9.3: All drainage improvements shall comply with the City of Fairfield Standard Specifications and Details, Engineering Design Standards (Section 4 - Storm Drainage).

Development in the FTSSP will comply with the City of Fairfield Standard Specifications and Details, Engineering Design Standards when conducting drainage improvements.

Policy PF 9.4: Utilize a variety of means for floodplain management, including structural improvements, land use and zoning controls, erosion control techniques, set backs, levees, floodways, detention and retention of storm runoff, and other measures as circumstances dictate. Priority will be given to those measures which provide results in a cost-effective way while complying with the Goals of this General Plan, the Federal Emergency Management Agency (FEMA) requirements, and aesthetic, environmental, and recreational considerations.

The Specific Plan will provide wastewater systems to serve planned uses within the Specific Plan area, in compliance with local and state requirements. The Specific Plan goals and policies and the Utilities Plan include provisions for a range of “sustainable” features that will protect floodplain and groundwater resources. The proposed Project will comply with the General Plan, FEMA requirements, as well as Executive Order 11988 (Floodplain Management) which addresses floodplain issues related to public safety, conservation, and economics. In addition, detailed hydraulics analysis shall be performed prior to road construction over Union Creek and other crossings in the 100-year floodplain to determine and include the appropriate culvert sizes and locations such that adequate hydraulic conveyances for the 100-year flood are maintained.

Policy PF 10.1: Protect the viability of existing health care facilities by preventing the encroachment of incompatible land uses and structures such as noise-sensitive uses and structures that obstruct emergency helicopter flight paths.

Policy PF 10.2: The City shall ensure that the Zoning Ordinance provides for adequate lands for a variety of health care facilities.



Policy PF 10.3: The City should encourage additional health care capacity as needed by the increasing population in the Fairfield area.

Policy PF 10.4: The Fire Department shall exercise its licensing authority in a manner that ensures that there are no unnecessary barriers to the timely response of health care providers to medical emergencies.

These policies are not applicable because the FTSSP does not include development of health care facilities.

Policy PF 11.1: Maintain the Fairfield Civic Center as the City government center.

Policy PF 11.2: Continue to promote the use of the Fairfield Civic Center for various educational, cultural, and civic programs by the Fairfield community.

The Fairfield Civic Center will continue to serve as the City government center and will continue to support various programs, which will be available to residents of the FTSSP. In addition, the FTSSP dedicates 134 acres of land to Community Facilities/Public Utilities/Institutional uses throughout the Project site. These facilities may include: elementary school, public buildings consisting of a library and community center, quasi-public facilities consisting of day care centers and private recreational clubs, and fire station and fire training center. Residents of the Specific Plan Area will be able to use these facilities for daily needs that do not require a trip downtown.

Policy PF 11.3: Community centers, including cultural facilities, should be located on highly visible and easily accessible locations.

The FTSSP dedicates 134 acres of land to Community Facilities/Public Utilities/Institutional uses throughout the Project site. These facilities may include: elementary school, public buildings consisting of a library and community center, quasi-public facilities consisting of day care centers and private recreational clubs, and fire station and fire training center.

Policy PF 11.4: The clustering of public and quasi-public uses such as schools, parks, libraries, child care facilities, and community activity centers is encouraged.

The land dedicated to Community Facilities/Public Utilities/Institutional uses permits the clustering of users and facilities in the planning area.

Policy PF 11.5: Support the Fairfield Center for the Creative Arts in providing a diversity of performing and visual arts programs.

Residents of the Specific Plan will be able to utilize the programs at the Fairfield Center. In addition, the Specific Plan proposes numerous facilities that will enhance the City's recreational and cultural resources, including building and maintaining a new 50-acre Great Park with comprehensive recreational facilities; a Lake Park near the Town Center that will feature a lake and pedestrian promenades; a

Linear Park--a major open space linkage and activity spine; community-level pocket parks that will provide flexible play and picnic areas. In addition, private recreation centers are included that will provide a central amenity for the neighborhoods in PA1, PA4 and PA5. They will act as a social activity nexus and will vary in size, program, and character. Recreation centers of a larger scale may include meeting rooms, multi-purpose rooms, or special event areas.

Policy PF 11.6: Promote the use of local public schools by school and community creative arts groups.

Schools located in the FTSSP area will be available to local residents per the policies of the Travis Unified School District.

Policy PF 12.1: The City shall designate adequate, appropriately located land for utility uses.

The FTSSP designates utility corridors and sites for major utility infrastructure. These sites will be coordinated with appropriate private, local and regional agencies.

Policy PF 12.2: The City shall continue to circulate development proposals to local utility providers, including Pacific Gas and Electric, Pacific Bell, and local cable television providers, for their review and comment and to ensure that they can and will provide service to development.

Development proposals enabled under this plan will follow this policy.

Policy PF 12.3: The City shall continue to work with local utility providers to allow them adequate time to prepare plans for servicing new planned growth.

Local utility agencies have participated in the FTSSP process. The FTSSP EIR addresses specific and cumulative impacts on local utilities and proposes measures that mitigate growth.

Policy PF 13.1: Reduce the amount of waste disposed of at the landfill by reducing 25 percent of the solid waste stream by the year 1995 and 50 percent by the year 2000 as mandated by State law. (See Policy OS 8.6)

Implementation of the proposed Project would result in an increase in the amount of solid waste generated within the Specific Plan Area and increase the demand for landfill capacity. Potrero Hills Landfill expansion is expected to be completed prior to the beginning of construction activities within the Specific Plan Area. Since the expansion will double the landfill's capacity prior to the first solid waste collection services within the Specific Plan Area, the landfill would be able to accommodate the additional solid waste that would be generated. In addition, the Specific Plan will incorporate methods that would reduce solid waste generation and promote source reduction, recycling, and composting, all of which would help to lengthen the life of the landfill.

Policy PF 13.2: Continue to participate as member of the County Local Task Force For Integrated Waste Management by providing input on county-wide waste management issues. (See Objective LU 5, Policy LU 5.1)

The Specific Plan does not address participation in this Task Force.

Policy PF 14.1: Reduce the amount of household hazardous waste improperly disposed of by City residents by providing a convenient means of disposing of these materials.

Development constructed under the FTSSP is in compliance with the City's hazardous waste policies, including the Household Hazardous Waste Disposal program, which allows City residents to properly dispose of household hazardous waste. In addition to consistency with City policies, projects developed under the Specific Plan will be regulated by the Hazardous Materials Release Response Plans and Inventory Act of 1985, Hazardous Waste Control Act, Emergency Services Act, Safe Drinking Water and Toxic Enforcement Act of 1986 (Proposition 65), Underground Storage Tank Program, California Integrated Waste Management Act and the State Unified Program, as described above.

Policy PF 15.1: Provide enough staffing and fire stations to ensure that at least 80 percent of the residential dwelling units in any response area are located within five minutes maximum travel time of a station. Where the number of dwelling units within five minutes travel time of any response area falls below 80 percent, the City shall strive to take the appropriate steps (e.g., construct a new fire station) to ensure that the above standard is maintained, within its budgetary constraints. In addition, fire stations shall be located to ensure that all target hazards are within five minutes travel time from a fire station where feasible.

The FTSSP and the EIR analyzed and addressed fire response issues. FTSSP will provide fire services to meet the needs of its residents. The closest fire station to the Project site is Station 39. According to the needs assessment, Station 39 should be relocated to better serve the Specific Plan area by complying with Policy PF 15.1. Relocation would not adversely affect other areas currently served by Station 39. In addition, project proponents will identify and dedicate land for the construction of a new Fire Training Center that will house fire protection and emergency services and provide training for the fire department.

Policy PF 15.2: Require grouping of residential units to provide for more localized and effective fire protection measures such as consolidation of fuel buildup abatement, fire break maintenance, firefighting equipment access, and water service provision.

The FTSSP clusters development into mixed use villages that facilitate fire protection.

Policy PF 16.1: Maintain an average emergency response time of under 5 minutes and an average non-emergency response time of under 20 minutes.

According to the needs assessment, a total of six stations will be needed at buildout of the General Plan to maintain fire response times of less than 5 minutes. Station 39 would be relocated to better serve the Specific Plan area by complying with Policy PF 15.1. Relocation would not adversely affect other areas currently served by Station 39.

Policy PF 17.1: Continue to collect fees from new development to fund its share of County-provided facilities and services (e.g. library, health, welfare, and justice system).

New development within FTSSP is responsible for paying a financial contribution to mitigate the effect of the development on the provision of such public services as police and fire protection and public education. Please see Chapter 14, Implementation and Administration of the Specific Plan for detail.

Policy PF 18.1: All new residential development shall be required to annex to a Mello-Roos Community Facilities District or to otherwise financially contribute for the purpose of acquiring land or development rights to preserve open space buffers between Fairfield and its neighboring cities and to preserve open spaces as defined in the Open Space and Conservation Element. Developers shall be given the option of delaying development until sufficient open space is acquired to mitigate proposed residential development. (See Policy OS 5.2)

The City shall adopt a comprehensive Financial Plan concurrently with adoption of Specific Plan (see Chapter 14.0 Implementation and Administration for more information). The Financial Plan shall include the following:

- *Infrastructure Financing: The method(s) by which new development within the Specific Plan will pay for all capital costs associated with the construction of those public improvements identified by the Specific Plan and in accordance with standards and policies adopted by City.*
- *Municipal Services: The method(s) by which new development within the Specific Plan will pay for the cost of increased demand for municipal services.*

Policy PF 19.1: Explore the possible relocation of existing City, County, and various utility corporation yards to a joint-use corporation yard.

The corporation yard issue is not relevant to the Specific Plan..

Policy PF 20.1: Adopt mechanisms to ensure that the rate of growth and the rate of school construction are compatible. (See Program ED 7.8 A)





The City has worked with Travis Unified School District to determine available capacity to serve the Project's elementary student projections.

Policy PF 20.3: Cooperate with the Fairfield-Suisun Unified School District and the Travis Unified School District in the assessment and collection of school impact fees on new development.

Development which occurs in the Specific Plan area will pay relevant impact fees outlined in the Plan and subsequent development agreements.

Policy PF 20.4: Work with the City of Suisun City, the City of Vacaville, Solano County, the Fairfield-Suisun Unified School District, and the Travis Unified School District to devise a permanent means of financing school construction within the Districts to accommodate the residential, industrial, and commercial growth policies of the of the Cities and County. The permanent means of financing school construction must recognize that adequate school facilities are in the best interests of the public and private sectors and that only through the cooperation of all parties can a permanent solution be devised. The solution must address (a) its cost effectiveness; (b) the timeliness of funding; (c) its flexibility; (d) cash flow needs of the school district involved; and (e) political acceptability.

The City shall adopt a comprehensive Financial Plan concurrently with adoption of Specific Plan (see Chapter 14.0 Implementation and Administration for more information). The Financial Plan shall include the following:

- *Infrastructure Financing: The method(s) by which new development within the Specific Plan will pay for all capital costs associated with the construction of those public improvements identified by the Specific Plan and in accordance with standards and policies adopted by City.*
- *Municipal Services: The method(s) by which new development within the Specific Plan will pay for the cost of increased demand for municipal services.*

Policy PF 21.1: Elementary schools should be planned as an integral part of neighborhoods, located away from major arterial streets and linked with recreational parks and community corridors serving residential neighborhoods, and situated so as to reduce or eliminate the need for pupil transportation.

According to TUSD's 1997 Facilities Plan, there is a need for three additional elementary schools, one additional middle school, and one additional high school to accommodate development in Fairfield and Vacaville. The TUSD is proposing an elementary school site within the Specific Plan area. The City of Fairfield will work with the TUSD to determine available capacity to serve the Project's student projections.

Appropriate funding and financing mechanisms will be pursued to construct the new elementary school to serve the project in PA2. This can include the use of State New Construction funding to build new and expand existing schools, as well as local revenue sources such as Mello Roos tax and school mitigation impact fees, and developer fees.

Policy PF 21.2: Middle schools and high schools should be located with respect to the student populations which they will serve, with direct access provided from collector or major arterial streets.

This policy is not applicable because the proposed Project does not include development of middle schools and high schools.

Policy PF 21.3: Middle schools are generally considered compatible with most residential and park uses but are not generally considered compatible with retail commercial and general industrial uses.

This policy is not applicable because the proposed Project does not include development of middle schools.

Policy PF 21.4: High schools are generally considered compatible with most residential, park, service commercial, office, and light industrial uses, but are not generally considered compatible with retail commercial and general industrial uses.

This policy is not applicable because the proposed Project does not include development of high schools.

Policy PF 21.5: Continuation high schools and technical schools shall be considered compatible uses in industrial parks in order to encourage vocational training and provide opportunities for gaining firsthand experience in technical skills.

This policy is not applicable because the proposed Project does not include development of continuation high schools and technical schools.

Policy PF 21.6: Wherever possible, school sites should be integrated with recreation parks and community recreation corridors to maximize the benefits of recreation experience as part of the education process and to permit pedestrian and bicycle access with a minimum of interruption by the street system.

The TUSD is proposing an elementary school site within the Specific Plan area. The City of Fairfield will work with the TUSD to determine available capacity to serve the Project's student projections. The school contains a park and will also be situated in PA2 along the Linear Park and proposed bike and pedestrian trails to maximize recreational and access opportunities for the students.

Policy PF 22.1: Notify the appropriate school district(s) affected by newly proposed developments as soon in the review process as possible to allow ample time for the assessment of impacts on school facility needs.



The City will work with Travis Unified School District to determine available capacity to serve the Project's elementary student projections.

Policy PF 22.2: Work with the school districts in developing long-range land use plans to allow adequate time for the preparation of school facility plans for servicing new planned growth.

The City will work with Travis Unified School District to determine available capacity to serve the Project's elementary student projections.

Policy PF 22.3: Encourage the school districts to explore means for maximizing the service potential of all existing and planned school facilities, including year-round enrollments and increased school capacities.

The City will work with Travis Unified School District to determine available capacity to serve the Project's elementary student projections.

Policy PF 22.4: Use Exhibit PF-5 as a guide for locating new schools.

The City will work with Travis Unified School District to determine available capacity to serve the Project's elementary student projections.

Policy PF 22.5: The City shall continue to work with the school districts to designate additional potential sites for new school facilities as the future need for them becomes known.

The City will work with Travis Unified School District to determine available capacity to serve the Project's student projections.

Policy PF 22.6: Facilitate cooperative arrangements between the Fairfield-Suisun Unified School District, the Travis Unified School District, and/or the City to share facilities.

TUSD does not anticipate a need to share facilities serving the FTSSP.

Policy PF 23.1: Cooperate with Solano Community College, Solano County, and the cities within Solano County in the provision of adequate classroom space and facilities to serve the area.

This policy is not applicable because the FTSSP does not include post-secondary educational facilities and programs.

Policy PF 23.2: Encourage shared use of facilities between Solano College, the City, the Fairfield-Suisun Unified School District, and the Travis Unified School District.

This policy is not applicable because the FTSSP does not include development of post-secondary educational facilities and programs.

Policy PF 23.3: Work with adjacent communities to encourage a wide variety of vocational, technical, and post-secondary educational opportunities.

This policy is not applicable because the FTSSP does not include development of post-secondary educational facilities and programs.

A.9 Travis Air Force Base Protection Element

Policy TP 1.1: Future development shall not conflict with the viability of Travis AFB. (See Policy LU 13.3 and Policy ED 1.9)

Portions of the Specific Plan Area is located in areas addressed by the Travis Air Force Base (AFB) Land Use Compatibility Plan as Zone D. Incompatible land uses within Zone D include physical (i.e., objects greater than 200 feet tall), visual, and electronic forms of interference with the safety of aircraft operations. The City does not anticipate that the Specific Plan would include structures greater than 200 feet in height. However, the proposed Project will be reviewed according to existing regulatory requirements designed to avoid conflicts with operations at Travis AFB.

Policy TP 1.2: Continue to notify the Air Force of proposed development activities in the vicinity of Travis AFB.

This ongoing city policy will continue during implementation of the FTSSP. Travis was consulted during preparation of the Plan and will continue to be consulted as individual subdivisions and commercial projects are reviewed by the City.

Policy TP 1.3: Proposed land uses shall be consistent with the land use compatibility policies and criteria of the 2002 Land Use Compatibility Plan (LUCP) for Travis Air Force Base. (See Policy LU 13.3, and Policies HS 5.1, HS 5.2, HS 5.3, and HS 9.2)

Policy TP 1.4: Projects within the LUCP Compatibility Zones A, B1, B2, C, D or the Height Review Overlay Zone as shown on Exhibit HS-3 of the General Plan shall be referred to the Solano County Airport Land Use Commission (ALUC) as required by the Travis AFB Land Use Compatibility Plan (LUCP).

Portions of the Specific Plan Area is located in areas addressed by the Travis Air Force Base (AFB) Land Use Compatibility Plan as Zone D. Incompatible land uses within Zone D include physical (i.e., objects greater than 200 feet tall), visual, and electronic forms of interference with the safety of aircraft operations. The City does not anticipate that the Specific Plan would include structures greater than 200 feet in height. However, Project will be reviewed according to existing regulatory requirements designed to avoid conflicts with operations at Travis AFB.

Policy TP 1.5: Maintain current agricultural zoning in the area east of Northgate Road, including the Jepson Prairie, for land that is not within the boundaries of the Base. (See Policy OS 1.3, Policy OS 9.3, and Program OS 9.3 A,)



The area east of Northgate Road lies outside the purview of the FTSSP.

Policy TP 1.6: Continue to support the role of Travis Air Force Base as a significant contributor to the economic base of the community. (See Policy ED 1.9)

The land use plan was designed to protect the viability of Travis Air Force Base. Implementation of the Plan will provide diverse housing opportunities for base personnel in close proximity to their employer. The industrial and commercial areas will provide opportunities for companies interested in working with the Air Force.

Policy TP 1.7: Two hundred units of housing shall be exempt from any special funding mechanism established to pay for new infrastructure to serve the northeast area of the City and shall provide preference for affordable housing for military personnel stationed at Travis Air Force Base.

The City of Fairfield will work with Travis and interested private developer(s) if the City receives a proposal to develop housing to serve Travis personnel

Policy TP 2.1: All new land use proposals shall comply with the noise and overflight policies of the 2002 LUCP for Travis Air Force Base. (See Policy HS 9.2 and Program HS 9.2A and Policy LU 12.3)

Portions of the Specific Plan Area is located in areas addressed by the Travis Air Force Base (AFB) Land Use Compatibility Plan as Zone D. The proposed Project will be reviewed according to existing regulatory requirements designed to avoid conflicts with operations at Travis AFB.

Policy TP 2.2: No new residential zoning will be applied to land in Compatibility Zones A, B1, B2, or C and no new schools will be permitted in these areas. (See Policy HS 9.2 and Program HS 9.2 A)

The Land Use Plan for the FTSSP does not designate any land uses which violate this policy.

Policy TP 2.3: For all new residential development east of Claybank Road, require homebuyer-renter notification of the aircraft noise associated with Travis AFB.

This policy applies to residential development both outside the FTSSP and within the boundaries of the Specific Plan. However, other than existing housing in the southeastern corner of the Specific Plan Area formerly associated with Travis AFB, no Specific Plan sensitive receptors are located within the 60 dBA CNEL noise contour according to the most recent noise contours in the Solano County General Plan Public Health and Safety Element.

Policy TP 3.1: No new development shall be permitted in the Compatibility Zones A, B1, and B2. Development shall be limited in Compatibility Zone C, per the 2002 LUCP.

Portions of the Specific Plan Area is located in areas addressed by the Travis Air Force Base (AFB) Land Use Compatibility Plan as Zone D.

Policy TP 4.1: Objects and structures within the Compatibility Zones C, D, and the Height Review Overlay Zone as shown in Exhibit HS-3 shall conform to FAR Part 77 height limits. (See Policy HS 6.1)

Portions of the Specific Plan Area is located in areas addressed by the Travis Air Force Base (AFB) Land Use Compatibility Plan as Zone D. Incompatible land uses within Zone D include physical (i.e., objects greater than 200 feet tall), visual, and electronic forms of interference with the safety of aircraft operations. The City does not anticipate that the Specific Plan would include structures greater than 200 feet in height. However, Project will be reviewed according to existing regulatory requirements designed to avoid conflicts with operations at Travis AFB.

Policy TP 4.2: Development proposals shall be reviewed for hazards to aircraft in flight, including: uses that release into the air any substance such as steam, dust and smoke which would impair pilot visibility; uses that produce light emissions, glare or distracting lights which could interfere with pilot vision or be mistaken for airfield lighting; sources of electrical emissions which would interfere with aircraft communications or navigation; and uses which would attract birds or waterfowl to the extent that they would pose a danger to aircraft operation in the vicinity of the Base.

The proposed Project will be reviewed according to existing regulatory requirements designed to avoid conflicts with operations at Travis AFB.

Policy TP 5.1: Establish a reserve of land to be set aside for the expansion of Travis AFB or a change in its mission.

The Specific Plan does not impact the existing Travis Reserve area east of Travis AFB.

Policy TP 5.2: Establish mechanisms to facilitate the exchange of information between the City of Fairfield and the Air Force as follows:

- *Early notification by the City to the Air Force of development projects east of Claybank Road;*
- *Early notification by the Air Force to the City of changes in aircraft flight patterns;*
- *An ongoing consultation mechanism between the City and the Air Force on issues of mutual concern.*

This policy is will continue to apply to all development proposals within the FTSSP.

Policy TP 5.3: The projected need for additional utilities and other municipal services by Travis AFB should be considered in the development of new infrastructure plans.

The FTSSP did not address Travis infrastructure needs. The City will work with the Air Force as more information becomes available as to expansion plans.

Policy TP 5.4: The City will cooperate with Travis Air Force Base officials in identifying strategies to meet the housing needs of military personnel. (See Policy HO 7.5 and Program HO 7.5 A)

Housing in the FTSSP may be suitable for many military personnel. The City will work with the Air Force and any private developer interested in providing affordable military housing. .

A.10 Urban Design Element



Policy UD 1.1: Create entryways to the City that achieve a sense of arriving into the City. These entryways shall incorporate signage, landscaping, architectural features, and combinations of land uses that enhance the image of the City. (See Policy CI 2.9)

The Train Station Community Gateway at the cross streets of Peabody Road and Vanden Road; the Hillside Community Gateway at the cross streets of Peabody Road and Cannon Road; and the Gateway on Vanden Road at the Great Park are intended to provide a sense of arrival to the City and serve as a gateway to the new development.

Policy UD 1.2: Enhance core areas of more intensive and higher image development to function as centers in key areas of the City (i.e. Downtown and the Fairfield Gateway in Central Fairfield, the Fairfield-Vacaville Train Station, and the I-80/I-680 Interchange in Cordelia). (See Policy LU 2.2)

The FTSSP includes two key core areas. The Town Center is planned as the walkable mixed-use hub of social, entertainment, shopping, and business activity for the FTSSP. Design and development standards in the Town Center will encourage higher intensity uses that attract residents from throughout the Project area.

The Lake District is envisioned as a one of a kind community in the City which will draw hundreds of users a day from surrounding neighborhoods as well as the City-at-large. Planned as a vibrant, mixed-use district organized around a 12-acre manmade lake, it will be accessible to everyone in the community and offer a wide variety of neighborhood and recreational activities including walking; picnicking; day use areas; and small, non-powered craft boating.

Policy UD 1.3: Connect areas of the city by concerted streetscape development along major arterials and pedestrian pathways.

Ten basic street types and greenbelt recreation/pedestrian pathways network are proposed to be interlaced throughout Fairfield. The FTSSP provides multiple modes of transportation including train, bus, park and ride, bicycle, and walking to further reduce single occupancy trips both internal and external to the Project. The streetscape development provides clear and convenient linkage between the

station, parking, residential, and business uses. Please refer to Section 10.0, Circulation Plan, of the FTSSP for detailed information on streetscape development and pedestrian pathways.

Policy UD 1.4: Develop positive, high quality edges along Interstate 80, major arterials, and the city limits. (See Policy CI 2.9)

The major arterials in the FTSSP will be designed as integrated landscapes that are diverse yet consistent with the overall scale of the community and surrounding grasslands and agricultural areas.

Policy UD 1.5: Continue to support the Art in Public Places Program.

Iconic public works of art will be used to enhance urban design and culture in the Specific Plan area.

Policy UD 2.1: New development should be compatible with the City's overall development profile.

The FTSSP envisions a mixture of individual neighborhoods, commercial and employment districts, and open spaces integrated into the existing City of Fairfield to the south and west. Unique to this development is a new Town Center district of higher density housing; mixed uses, and a network of boulevards, walkable blocks and small parks. Greenways, trails, boulevards and other pedestrian-friendly elements tie the community together as well as providing linkages to central Fairfield.

Policy UD 2.2: Encourage variety in the use of complementary colors, textures, forms, styles, structures, and/or materials.

A variety of colors, styles and materials are proposed for the FTSSP. The Specific Plan includes comprehensive development standards and landscape guidelines.

Policy UD 2.3: Allow the careful use of contrast where it would be appropriate for providing focus and interest to an area.

The Town Center and the Lake Park communities proposed in the FTSSP provide focal points which contrast with lower density surrounding uses and natural areas.

Policy UD 2.4: Reinforce key patterns that positively characterize an area through the use of common design features.

Proposed community design guidelines will result in residential and commercial districts which reflect the City of Fairfield's suburban aesthetic. In addition, the Town Center and Lake Park mixed use communities will provide focal points that contrast with surrounding neighborhoods and refer to the traditional town centers found in Fairfield and other nearby inland California cities.

Policy UD 3.1: Include guidelines in the Urban Design Plan to enhance the City's small-scale identity,



such as creation of a sense of neighborhood in residential areas. (See Policy HO 9.2, Objective LU 10, Objective ED 2)

Major and Minor Neighborhood Entries are proposed in FTSSP to provide a sense of arrival and provide a signature element within each neighborhood. Please refer to Section 12.0, Landscape Plan, of the FTSSP.

Policy UD 3.2: Promote pedestrian and bicycle orientation through separated sidewalks, bicycle paths, interior walkways, planting of canopy trees adjacent to pedestrian paths, etc. (See Objective CI 9, Policy CI 9.1, Policy CI 9.2, Policy CI 9.5, Policy CI 9.8, Program CI 9.1 A, Program CI 9.2 A, Objective CI 10, Policy CI 10.1, Policy CI 10.2, Policy CI 10.3, Policy CI 10.4, Policy CI 10.5, Policy CI 10.6, and Policy ED 4.3)

The FTSSP includes landscaping and design standards for pedestrian and bicycle circulation designed to encourage alternative modes of transportation. Pedestrian/bicycle trail overcrossings and under crossings are proposed at major street crossings. Please refer to Section 12.0, Landscape Plan, of the FTSSP for additional information.

Policy UD 3.3: Require new development to respect the scale and character of nearby structures and minimize or mitigate abrupt and excessive differences. (See Policy ED 4.3)

The FTSSP includes design goals that are consistent with City standards and design criteria and ensures that high standards of siting and design of the development are achieved. Specifically, the Specific Plan requires that the Project will conserve Fairfield's rural character and culture while creating the capacity to significantly address the region's major infrastructure and other public service needs; create a high standard in neighborhood design and implement it consistently throughout the Specific Plan; create neighborhood design that integrates architecture, Site planning, and landscape; and provide for uniform design and development guidelines that allow for creativity and diversity, while at the same time ensure long-term, quality maintenance standards.

Policy UD 3.4: Encourage private preservation of buildings which have historic significance and/or architectural merit. (See Policy OS 10.1 and OS 10.9)

This policy is not applicable because there are no significant identified historic buildings in the planning area.

Policy UD 4.1: Adhere to the design guidelines of the Fairfield Urban Design Plan.

The FTSSP includes design goals that are consistent with the Fairfield Urban Design Plan, and ensures that high standards of siting and design of the development are achieved.

Policy UD 4.2: All aspects of development, including, but not limited to, grading, site planning, signage, fencing, landscaping, screening, lighting, color scheme, size, bulk, height, etc., must be integrated and relate to their surroundings in a complementary manner.

The FTSSP includes design goals that are consistent with City standards and design criteria and ensures that high standards of siting and design of the development are achieved.

Policy UD 4.3: Require the use of water features, sculptures, or other elements to help define the entrance to large projects.

The FTSSP incorporates design elements such as water features (the Lake Park) as well as the Train Station Community Gateway at the cross streets of Peabody Road and Vanden Road; the Hillside Community Gateway at the cross streets of Peabody Road and Cannon Road; and the Gateway on Vanden Road at the Great Park, which are all intended to provide a sense of arrival to the City and serve as a gateway to the new development.

Policy UD 4.4: Encourage rehabilitation or upgrade of aging residential neighborhoods, commercial centers, and industrial areas.

The FTSSP area is largely undeveloped, with limited older industrial areas along Peabody Road. This policy thus has limited applicability to the FTSSP. The Specific Plan does not anticipate preservation over the long term of existing industrial uses along Peabody Road.

Policy UD 4.5: Screen negative views through site planning, architectural, and landscape devices.

The design standards and buffer areas are established to ensure that visually incompatible land uses are properly screened. At the same time, the Specific Plan provides for mixed use development that will use careful building design and site planning to minimize conflicts between land uses. .

Policy UD 4.6: Include regulations in the Urban Design Plan which prevent a cluttered appearance in the front of the buildings.

The design standards minimize visual clutter while permitting a diverse mix of uses-particularly in the Town Center and community cores.

Policy UD 4.7: Include regulations in the Urban Design Plan to limit excessive repetition of the same residential building elevations within a subdivision.

This design standard for the community as well as City of Fairfield development standards will help avoid excessive repetition of building elevations.

Policy UD 5.1: Development should be designed to provide continuity with features of the surrounding area.



The Landscape Plan (Chapter 12.0) is a major component of the overall FTSSP development and will guide and maintain Project design themes, emphasizing community continuity.

Policy UD 5.2: Restrict development from significantly encroaching on public views of ridgelines, agricultural areas, the Cement Hill Range, and the Suisun Marsh. (See Policy OS 6.1)

The Specific Plan preserves those portions of the Project site with view corridors, watersheds, and prominent ridges as open space in order to conserve soils and vistas and does not propose development on ridgelines.

Policy UD 5.3: Develop hillside areas in a manner which respects their topography and maintains their visual integrity. (See Policies OS 6.1, OS 6.2, and OS 6.3 and Programs OS 6.3 A, OS 6.4 A, and OS 6.4 B)

There will be minimal hillside development within the boundaries of the FTSSP. Nearly 60 percent of the Specific Plan area will be conserved as active or passive open space by the Project. The Specific Plan preserves those portions of the Project site with view corridors, watersheds, and prominent ridges as open space in order to conserve soils and vistas and does not propose development on ridgelines.

Policy UD 6.1: Preserve existing significant trees and extensively plant new trees where appropriate. (See Policy OS 7.7)

Section 12.0, Landscape Plan, of the FTSSP includes tree planting discussion in the Specific Plan Area.

Policy UD 6.2: Landscape materials should consist of drought resistant plant varieties complementary to the area. (See Policy PF 4.8 and Policy OS 8.5)

Plants will be selected based upon their reduced demand for water, fertilizers, pesticides and maintenance as well as their potential to provide habitat value for residents and potential fauna. The FTSSP provides applicable policies in Chapter 8.0, Sustainability and Climate Change, to respond to climate change, decrease greenhouse gas emissions and enhance air quality through addressing construction related activities, vehicle miles traveled, water and energy use and open space preservation and landscaping. The City will adopt the mandatory provisions of CALGreen in the local building code, therefore, making it critical that future FTSSP buildings comply with CALGreen standards. It is anticipated that new structures built in compliance with the green building code will achieve at least a 15 percent reduction in energy usage when compared to the State's mandatory energy efficiency standards. The 2010 CALGreen standard calls for the reduction of indoor potable water by at least 20 percent from current code standards through the use of low and ultra low flow fixtures. It also requires buildings to have more efficient controllers for exterior irrigation system.

Policy UD 6.3: Landscaping shall be continuously maintained in good condition in accordance with the City's Property Maintenance Ordinance.

Commercial developments will execute standard City of Fairfield Maintenance Agreements. Subdivisions will annex into lighting and landscaping districts or use similar mechanisms to facilitate high quality maintenance,

Policy UD 7.1: Develop guidelines for new community commercial centers that encourage:

- *A minimum FAR of .5;*
- *60 percent of arterial frontage and 100 percent of street front have building within ten feet of the property line; and*
- *20 percent of the square footage be housing.*

The Town Center District and the Lake District are commercial districts which will include housing, urban footprint building types, and high quality design. Chapter 4.0, Land Use, of the FTSSP provides development standards for commercial uses.

Policy UD 7.2: Evaluate the City's current standards for parking and recommend measures to reduce the amount of land area devoted to parking on new projects.

Chapter 4.0, Land Use, of the FTSSP provides development standards for parking, including shared parking.

Policy UD 7.3: Encourage shared use of parking facilities and promote planning for land uses that can utilize the same parking area at different times.

The FTSSP encourages use of shared parking within PA2 and PA4. Development of a parking management plan is required to clearly define and achieve projected shared parking as part of the Area Plan submittal. Shared parking studies and subsequent parking reductions shall conform to the Shared Parking policies of the CM zone adopted concurrently with this Specific Plan.

Policy UD 8.1: Encourage and facilitate the development of identified vacant, contiguous properties zoned for medium density housing with a prototypical "neo-traditional" type subdivision of single-family detached and attached homes.

The FTSSP includes mixed residential housing types, including single-family detached, townhomes, condominiums, apartments, and multi-family with three density categories: Low, Medium, and High.

Policy UD 8.2: Encourage the use of traffic circles, hardscape treatments at crossroads and other traffic-calming devices in the street layout and land plans for new residential subdivisions, in order to make new neighborhoods friendlier to the pedestrian.





The Project will implement numerous measures to promote the City goal of reducing traffic while making the community more pedestrian-friendly. These measures include traffic-calming devices in the street layout as well as the implementation of a Bicycle and Pedestrian Network. Please see Chapter 10.0 Transportation Plan for more information.



APPENDIX B: FAIRFIELD GENERAL PLAN AMENDMENT



APPENDIX B GENERAL PLAN AMENDMENT

July 26, 2011



Implementation of the Fairfield Train Station Specific Plan (FTSSP) requires the amendment of the City of Fairfield General Plan. Five of the ten elements included in the City of Fairfield General Plan have been amended, as follows:

1. Economic Development Element
2. Land Use Element
3. Open Space, Conservation, and Recreation Element
4. Public Facilities and Services Element
5. Urban Design Element

Economic Development Element

Industrial Development

Fairfield's industrial sector is diverse and vigorous. Several manufacturing, light industry, back office, distribution, and research and development firms are located in Fairfield. Specific business and industrial parks were developed to meet the needs of particular businesses. Tolenas Industrial Park, a 300-acre park located along Air Base Parkway, is zoned for general manufacturing.

Solano Business Park and Busch Corporate Center are light industrial parks. Located along Highway 12, these parks contain light manufacturing facilities, professional offices, and distribution facilities. Other, smaller light industrial parks are located along Air Base Parkway (e.g., Travis Industrial Park) and in Cordelia (e.g., Fairfield-Cordelia Industrial Park). Located in Cordelia adjacent to Solano Community College, Fairfield Corporate Commons is planned for research and development and office uses. Its development currently is targeted toward office uses.

The Train Station Specific Plan identifies two "employment centers" in the eastern sections of the planning area. These two areas will primarily see development of light manufacturing, office, warehousing, and similar uses. However, the Train Station Specific Plan also requires designation of 25% of these areas for "General Industrial" uses which would benefit from rail access. The employment centers will also include a small neighborhood serving commercial/services center.

Future Conditions

Background

Under the policies contained in this element and other General Plan elements, Fairfield will continue to experience significant, yet controllable, levels of economic growth. As implemented, the plan policies will provide the City with the means to continue to diversify its economic base, while recognizing and protecting the important role of Travis Air Force Base.

Employment

Fairfield's one-third share of all Solano County jobs is expected to continue through 2020. Future job creation in the County is projected to come primarily from the retail, manufacturing, services, and construction sectors of the economy. In Fairfield, the relocation or expansion of small and medium-sized firms into the City accounts for a significant portion of job growth. At general plan buildout, Fairfield is projected to have approximately 77,000 jobs. Centers of employment are located in a number of locations through the city, and no one area is expected to receive a majority of new jobs.

Industrial

With well over 10 million square feet of existing occupied industrial space and over 1,400 acres of vacant land planned for industrial and business development, Fairfield offers both short and long-term opportunities for quality business development.

With accessibility to Highway 12 and rail service, the Tolenas industrial park is ideal for additional heavy industrial users. There will continue to be demand for these industrial sites. However, large amounts of land currently designated for industrial uses in this area will make the supply of land exceed demand in the through the end of the decade.

Several hundred acres of vacant industrial acreage are located in the Solano Business Park, Busch Corporate Center, and Cordelia, providing ample available land for light industrial growth beyond the year 2000. These areas will continue to attract light industrial and back office uses and will see the greatest absorption over the next ten years.

At least ~~800~~ 300 acres of unincorporated land east of ~~Peabody Road~~ Vanden Road is planned for annexation and development in conjunction with the Fairfield Train Station Specific Plan for employment-generating uses, including rail served industry, expected to be designated for development of an industrial park dedicated for research, development and high technology businesses.





Land Use Element

Policies for Specific Plan and Master Plan Areas

Following are descriptions and specific policies for [the Train Station Specific Plan Area](#) and the [two](#) Master Plan Areas. As described in the section on future land use, Master Plan Areas are special study areas which have the potential for urban development, but are presently constrained by a variety of existing factors. In addition, there are specific policies governing development in the Cordelia area.

Fairfield Train Station Specific Plan Area

The Fairfield Train Station Specific Plan Area encompasses 2,972 acres of land to the north of Cement Hill Road and the east of Peabody Road. Over 60% of the land is dedicated to greenbelt and conservation open space and recreational lands.

The purpose of the plan is to create a community framework based in sustainable urban form, traditional neighborhood planning principles, transit-oriented design, and an integrated open space and transportation concept.

The plan envisions the development of a variety of housing types with up to 6,800 dwelling units on 503 residential acres. Approximately 3,000 of these dwelling units will be within ½ mile of the planned train station. Complementing the residential development will be two major employment centers, commercial and mixed use development, parks and open space, and public schools and a library.

All development in the planning area shall comply with the adopted Fairfield Train Station Specific Plan. This Plan contains development standards, design guidelines, policies for public facilities and services, and directives for administration and implementation. Future land uses are shown on Figure 1-2 of the Specific Plan.

Peabody-Walters Master Plan

The Peabody-Walters Master Plan was adopted on September 6, 1994 for the area east of Claybank Road and north of Air Base Parkway. The Master Plan establishes specific policies and standards for this area. Future uses for land within the Master Plan area are identified on the General Plan Land Use Diagram.

Policies for the Cordelia Area

The following policies were included in the Cordelia Area Specific Plan, which was repealed in 1999. These policies apply to land that was formerly addressed in that plan.

Open Space/Agriculture

1. The location of open space corridors may vary from those depicted on the diagram except when the location is fixed by such features as an earthquake fault zone, PG&E power line, natural topographic or vegetative feature, along the Urban Limit Line, or divides land uses. The width and location of the open space corridor over the earthquake fault zone shall be determined by geotechnical studies.
2. The minimum size of a residential lot abutting land designated as Intensive Agriculture shall be 1/4 acre.
3. The use of the extensive agricultural land which lies between I-680 and Suisun Marsh shall be limited to those agricultural operations which are harmonious with the adjoining marsh and wetland area. Examples of acceptable uses include grain and hay crop production and irrigated and non-irrigated pasture.
4. There shall be an open space corridor with a minimum width of 50 feet along the Urban Limit Line when abutting areas designated Intensive Agricultural by the plan.
5. When development occurs adjacent to areas designated as Extensive or Intensive Agriculture, fencing shall be installed which is designed to discourage trespassing onto such agricultural properties.
6. Ridge lines and riparian areas shall be retained in open space uses.
7. The environmental quality of Suisun Marsh shall be protected and enhanced through a combination of land use regulation and construction of public facilities. Activities within and adjacent to the marsh shall generally be limited to the following: nature-oriented recreation, hunting, fishing, wildlife habitat, low-speed boating, and only those specific types of agricultural operations

vacant. However, there is an area of heavy industrial development along Huntington Drive. In addition, various manufacturing and service commercial uses are located along Peabody Road and Cement Hill Road in the unincorporated County.

Future Land Use – The Livable City Concept

Through the Land Use Diagram, Goals, Objectives, Policies, and Programs, the Land Use Element is a plan for future development of the City to the year 2020. This plan is a response to the unique issues, opportunities and constraints that face the community. Some of the key issues which the element addresses are amount and rate of growth, distribution and location of future land uses, and extent of future City boundaries. These issues, and others, are resolved in the context of a variety of opportunities and constraints, such as availability of vacant land, availability of public services and facilities, the existing pattern of land uses, and the natural environment.

Past strategies for future development have emphasized annexation of undeveloped land and establishment of unique identities for the western, central and eastern areas of the city. The experience of other cities, however, has demonstrated that this type of growth pattern plays an important role in causing urban sprawl. Symptoms of sprawl include the loss of productive farmland, increased parking and infrastructure requirements, increased traffic, and a growing inability to access basic services without the use of an automobile.

With this in mind, the Land Use Element is based on a future scenario known as the "Livable City" concept. This concept envisions the city developing in a manner that promotes a more compact and efficient land use pattern, and places less emphasis on development that necessitates use of the automobile. In particular, the Land Use Element incorporates the following concepts:

- Fairfield will remain an important center in Solano County for government, business, and commerce. The downtown area will become a stronger center for the entire City.
- There will be a strong commitment toward protection of agricultural areas outside the Urban Limit Line and to the separation from other urban areas in the County.
- Future development will largely occur within the existing City limits. Limited development will be proposed outside the City limits as part of future annexations, primarily to achieve certain related objectives that are difficult to achieve within the existing city limits. Examples would include the establishment of a large technology-related industrial center and the development of a regional open space park north of the City. An example would be establishment of a transit-oriented community adjacent to the

future Fairfield-Vacaville train station and a rail-served industrial park east of Vanden Road in conjunction with the Fairfield Train Station Specific Plan adopted in 2011.

- Incentives will be provided for concentrated development of infill areas within the existing City boundaries. These incentives will include modifications to development regulations and city fees.
- There will be a greater emphasis than in the past on pedestrian-oriented development (POD) and transit-oriented development (TOD).
- The existing separation of the western, central and eastern areas of the city will become more connected, with emphasis on a common city identity, and citywide diversity in development. However, areas that will remain remote from central Fairfield and downtown, such as Cordelia, would have high quality governmental services, recreation, shopping and employment.
- There will be a citywide balance of jobs and housing, with an emphasis on diversity in jobs and housing options. The desired citywide ratio should be consistent with the desired overall ratio for the nine-county Bay Area, established by recent policy decisions of the Association of Bay Area Governments.
- The City will provide high quality services and infrastructure in accordance with adopted standards.

The Land Use Diagram includes an "Urban Limit Line" which represents the ultimate limit of the City. Policies in the Land Use Element direct that urban development be confined within this Urban Limit Line. The Urban Limit Line spells out a commitment on the part of the City of Fairfield to respect the integrity of the surrounding non-urban areas. This intent is an integral part of the Livable City concept, which envisions Fairfield being surrounded by a greenbelt buffer of open space, clearly separated from the other cities of Solano County.

The Livable City concept, as represented by the Land Use Diagram, contains a range of land use categories, which are discussed below. The diagram also includes one Specific Plan Area and two ~~three~~ Master Development Plan areas, namely the Train Station Specific Plan, Nelson Hill in Cordelia, and Rancho Solano. ~~These include Nelson Hill, located in the Cordelia area, Rancho Solano North, located west of Interstate 80,~~ Master Plan areas are special study areas surrounding the community which have the potential to become urban areas, but are presently constrained by a variety of existing factors. Distinct circumstances must occur in order for master plan area development to be triggered. A General Plan Amendment and the adoption of an Areawide Plan would be required for these

areas before development would occur. The Land Use Element includes specific policies for each of the master plan and Specific Plan areas.

On September 6, 1994, a Master Plan was adopted for Peabody-Waters. The Master Plan includes specific policies governing this area. The Master Plan also establishes land uses designations for this area. The General Plan land use diagram includes the land use designations established by the Master Plan.

Table LU-1, which summarizes future housing, population and employment for the City, includes estimates for the Master Plan areas. These estimates have been developed in order to determine the appropriate level of citywide public facilities. Once the master, specific or areawide plans are adopted, these numbers may be revised and should not be considered a maximum or minimum.

High Density Residential: 15.0 - 22.0 dwelling units/gross acre

This category is intended for multi-family apartment and condominium development. This designation is located in close proximity to major arterial streets, commercial and recreational facilities, and employment centers.

Very High Density Residential: 22.0 - 32.0 dwelling units/gross acre

This category includes multi-family apartment and condominium development in high-intensity areas. Development of greater than 32 dwelling units/gross acre may be permitted for senior congregate care housing projects. When making a determination regarding the number of dwelling units in a senior congregate care housing project, a dwelling unit shall be considered a living unit including bathroom and living areas, but need not include kitchen facilities.

Train Station Specific Plan Residential Land Uses

The Train Station Specific Plan establishes three specific residential land use categories:

- Residential Low-Train Station (0-10 dwelling units per gross acre)
- Residential Medium-Train Station (10-20 dwelling units per gross acre)
- Residential High-Train Station (20+ dwelling units per acre).

Except for the specific density ranges, these three categories correspond generally with the general land use categories described above.

However, the Train Station Specific Plan also emphasizes mixed use development, in particular the integration of residential and commercial uses in a traditional urban environment centered on the planned train station.

Commercial and Industrial

The descriptions of land uses permitted in each commercial and industrial category are not exclusive. Other uses are permitted that are consistent with the Zoning Ordinance. The commercial and industrial categories include both a range of Floor Area Ratios (FAR) and typical FAR's for each land use. The FAR's are intended to be used as a guide for estimating the future development intensity. The Zoning Ordinance governs the actual FAR's required by the City.

Neighborhood Commercial: 0.2 - 0.275 FAR

This designation includes convenience commercial and neighborhood shopping centers which provide for a range of necessary day-to-day retail goods and services. Typically, a neighborhood shopping center would include a supermarket as the principal anchor tenant. Other tenants offer a variety of goods and services such as drugstores, banks, restaurants, and service stations. A typical FAR for this category is 0.225.

Community Commercial: 0.2 - 1.5 FAR

This category includes a variety of commercial areas that serve a larger market area than a neighborhood center but less than a regional center. The designation includes shopping centers that serve a larger market than the immediate neighborhood. Typically, there are one or more anchor tenants such as a supermarket, discount or off-price department store, hardware/home improvement store or combined drug/variety/garden store. There would also be a variety of tenants offering consumer goods and personal services. A typical FAR for this type of use is 0.225.

Specialty shopping centers that may be smaller in size than other community shopping centers but that serve a community-level market are also included in this category. These centers do not have a supermarket, but provide specialized goods and services. A typical FAR for this type of use is 0.225.

This category also includes strip commercial shopping areas along North Texas Street and Texas Street west of Pennsylvania Avenue. These areas are characterized by retail commercial and office uses, usually developed as freestanding units. A typical FAR for this type of use is 0.225.

The City's Central Business District (CBD) is also included in the Community Commercial category. However, this area has its own unique character as a pedestrian-oriented, concentrated area of retail, service and office uses. Multi-family residential may also be permitted in the CBD. The City Center Redevelopment Plan and the Zoning Ordinance govern the permitted uses in this area. Higher intensity development should be encouraged in the CBD than in the other types of Community Commercial development. A typical FAR in the CBD is 0.5. However, FAR's in the CBD may be as high as 1.5.

The Train Station Specific Plan identifies several parcels suitable for community-serving commercial development. In addition, the Specific Plan identifies mixed use commercial areas which will integrate housing and commercial uses. The Specific Plan includes design standards which emphasize mixed use and pedestrian-oriented building orientation, scale, and amenities.

Limited Industrial: 0.25 - 0.5 FAR

This designation provides for light and medium industrial type activity. Uses typical of this category may include but are not limited to research & development, warehouse, wholesale distribution, manufacturing, assembling, fabrication, office uses, and support uses. Not permitted within this category are uses which require unscreened open air storage of large quantities of raw, semi-refined, or finished products. Retail uses are discouraged in this category but may be permitted as a secondary use to a permitted use. This land use is appropriate as a buffer between General Industrial areas and non-industrial areas. The typical FAR for this category is 0.35.

The Train Station Specific Plan identifies two "employment centers" (Planning Area 6) designated for Limited Industrial, employment-generating uses. Up to 25% of the area should be developed for heavier, "General Industrial" uses, particularly those facilities which require rail service. In addition, the employment centers will also include development of neighborhood-serving community nodes with small scale retail and services for employment center workers and nearby residents.

General Industrial: 0.3 - 0.5 FAR

This designation allows for a wide range of industrial activities which includes manufacturing, assembling, fabrication, and other similar uses. The intent of this category is to provide a location for heavy industrial uses. Areas developed under this designation should be located with direct access to major roads, freeways, or rail lines. The typical FAR for this category is 0.40.


As noted above, the Planning Area 6 allows for limited General Industrial development.

Travis Reserve

This designation applies to certain unincorporated land located north and east of Travis Air Force Base. Land in the Travis Reserve is set aside for future expansion of Travis Air Force Base only. If the status of the base changes, the construction of a non-military airport and support uses may be permitted in the Travis Reserve. No residential uses will be permitted in the Travis Reserve. Until a military or airport use is proposed for land with the Travis Reserve designation, the City supports its continued use for agriculture and grazing.

Public Facilities

This includes a wide range of uses that can be defined as institutional responses to basic human needs, such as health, education, safety, and recreation. Examples of typical public facilities include schools, water treatment plants, and fire stations. Public facilities, including waste management, are described in detail in the Public



Southwest corner of Pennsylvania Avenue and Texas Street:
Office, commercial and residential, up to 32 units per acre

Sunset and Travis:
Neighborhood Commercial and Office

Northeast Corner of Airbase Parkway and Claybank Road:
Office, Specialty Services and Retail (See Policy LU 14.5 for more detail)

North of Highway 12 and West of Pennsylvania Avenue:
Limited Industrial, Service Commercial, and Office

Peabody Walters Master Plan: Sites designated for mixed use in the Peabody-Walters Master Plan Area shall be developed in accordance with the Master Plan

Area South of Travis Blvd. between Clay Street and Wilson Street:
Mixed residential and commercial

South side of Tabor Avenue, east of Kidder:
Mixed residential and commercial

Mission Village Shopping Center site, west of North Texas Street, north of Atlantic Avenue: Office, commercial, and high density residential

Any future areas which receive the mixed use designation shall include an integrated mixture of residential and employment-generating uses.

Open Space, Conservation, and Recreation Element

Preserve open space and agricultural lands in Green Valley.

Policy OS 2.5

Establish a permanent open space buffer between Fairfield and Vacaville in conjunction with the Vacaville-Fairfield-Solano Greenbelt Authority, consistent with the Fairfield Vacaville Greenbelt Study Committee Report and Recommendations of May 9, 1991.

Program OS 2.5 A

Establish and maintain an ongoing cooperative open space implementation program with the City of Vacaville.

Policy OS 2.6

Maximize open space preserved to enhance City identity and preserve surrounding natural environments. (See Objective UD 5)

Objective OS 3

Maximize public open space with minimum management. (See Objective LU 7)

Policy OS 3.1

Open Space dedications shall provide a plan for funding to insure that financing for interim maintenance is provided.

Policy OS 3.2

Ownership and management responsibility of public open space shall be assigned to the agency/organization best suited to meeting this responsibility. Adequate security of open space shall be provided to insure that applicable laws and regulations are enforced.

Program OS 3.2 A

Implement the City's adopted Open Space Acquisition and Management Plan.

Policy OS 3.3

Residents, property owners, and users shall contribute to maintaining public open spaces.

Objective OS 4

Maximize open space through appropriate acquisition mechanisms. (See Objective LU 7)

Policy OS 4.1

New development shall fund its share of open space.

Policy OS 4.2

Require new development in northeastern Fairfield to dedicate land pay an in-lieu fee for the acquisition of open space land within or in the vicinity of the Vacaville-Fairfield-Solano Greenbelt. dedication of open space lands for development projects in the Fairfield/Vacaville Greenbelt Open Space Planning Area in the northeastern area of Fairfield.

Policy OS 4.3

All future areawide plans shall include appropriate mechanisms for acquisition of open space.

Policy OS 4.4

An equitable balance shall be sought between development density and open space to be preserved.

Policy OS 4.5

Promote clustering of housing units to preserve the hillsides, ridges, and a maximum amount of open space. (See Policy LU 13.1 and Policy UD 5.3)

Policy OS 4.6

Utilize development projects as a means for permanently preserving open space by encouraging mechanisms such as land pooling and transfer of development rights within Open Space Planning Areas for the preservation of significant open space features and lands.

Program OS 4.6 A

Require dedication of appropriate open space land or development rights as a condition of approval for proposed development projects.

Program OS 4.6 B

Establish a method for permanent preservation of open space acquired through a land pooling program. Such designations shall be registered on the property title and shall be maintained should the property be sold.

Objective OS 5

Generate funds within the community for acquisition and management of open space lands.

Policy OS 5.1

A study shall be conducted by staff to identify, analyze, and establish open space funding mechanisms.

Policy OS 5.2

Through one or more of the following programs, generate funds for acquisition and management of open space.

Program OS 5.2 A

Policy OS 11.7

Preserve public access corridors for walkers, bicyclists, and equestrians along future floodway improvement projects whenever feasible.

Policy OS 11.8

Utility easement corridors shall be designated for recreational open space unless an acceptable trail alternative is included in a development plan.

Objective OS 12

Develop park areas to serve the needs of all residents.

Policy OS 12.1

Develop park lands at the local and community levels to meet the recreational needs of current and future Fairfield residents.

Policy OS 12.2

Update and adopt a Parks, Open Space, Leisure, and Cultural Services Plan that identifies potential programs and recreation facility improvements for the Fairfield community. The city should examine funding sources to enable the city to meet its parkland-per-population standards by buildout of the general plan and provide for reestablishment and renovation of aging park facilities.

Program OS 12.2 A

Annually review the Parks, Open Space, Leisure, and Cultural Services Plan in conjunction with the capital improvements program and the annual budget cycle to identify projects for funding and implementation. Annually assess progress in attaining parkland-per-population standards and use that information to adjust funding and/or parkland dedication standards, if necessary.

Policy OS 12.3

Establish neighborhood parks of approximately five to seven acres at a ratio of 1.5 acres of usable park land per 1,000 residents throughout the City. Neighborhood parks are intended to serve a 1/2 mile radius area.

For the Fairfield Train Station Specific Plan, the land portion of the "Lake Park" shall be considered a neighborhood park for the purposes of satisfying the General Plan neighborhood park land ratio.

Policy OS 12.4

Establish community parks of approximately 40 acres at a ratio of two acres usable park lands per 1,000 residents throughout the City. Community parks are intended to serve a two mile radius.

Policy OS 12.5

Establish regional parks to meet regional open space and recreation needs.

Program OS 12.5 A

Develop a regional park in the Rancho Solano North Master Plan Area similar to the Rockville Hills Park. (See Rancho Solano North Policy 11)

Policy OS 12.6

Develop a new recreational and outdoor cultural complex for softball, soccer, picnicking, and outdoor cultural events.

Policy OS 12.7

Preserve and develop the entire Linear Park system, from Solano Community College into the ~~Peabody-Walters Master Plan~~ Fairfield Train Station Specific Plan area, as illustrated on the Land Use Diagram, exclusively as a recreation facility in order to provide an alternative non-motorized vehicular linkage among recreation and parks, schools, shopping, industry, residential, and other community activity centers.

Program OS 12.7 A

Grant a conservation easement to an eligible non-profit corporation to protect the Linear Park from non-recreational uses.

OSPA 9 - Fairfield-Vacaville Greenbelt

This area includes the hills surrounding Lagoon Valley and a strip of land between the cities of Fairfield and Vacaville ~~as defined through action of the Vacaville-Fairfield-Solano Greenbelt Authority. Pursued jointly by the cities of Fairfield and Vacaville, a permanent open space buffer shall be established within the OSPA. The open space buffer shall be consistent with the Fairfield-Vacaville Study Committee Report and Recommendations of May 9, 1991 (included as an appendix to the General Plan).~~

OSPA 10 - Travis AFB/Jepson Prairie

This OSPA encompasses approximately 26 square miles, bounded by Travis Air Force Base, the proposed Fairfield-Vacaville greenbelt, State Route 113, Jepson Prairie's eastern and southern borders, and the Suisun Marsh Preservation Area. Most of the land is devoted to livestock grazing. This is essentially the same area recommended for establishment of a major ecological park in the 1982 Jepson Prairie Feasibility Study. The land is primarily flat to rolling grasslands containing numerous vernal pools and wetlands. It represents some the state's most significant examples of Central Valley Prairie.

OSPA 11 - Suisun Marsh

Located immediately south of Fairfield and Suisun City, Suisun Marsh is the largest continuous estuarine wetland in the United States. Occupying nearly 85,000 acres, the marsh is a vital wintering and breeding area for waterfowl. Strict federal and state regulations protect the wetlands and prohibit any new development or improvements which could degrade its quality. The Grizzly Island Wildlife Area and other State Department of Fish and Game lands are open to the public.

Open Space Commission

The Open Space Commission is charged with recommending to the Planning Commission, the Community Services Commission, the City Council, and other appropriate agencies the means by which to designate, preserve and protect agricultural, ecological, recreational and scenic lands in Fairfield and surrounding areas and to implement the General Plan Open Space and Conservation Element. A primary duty of the Open Space Commission is to review proposed development within the eleven Open Space Planning Areas for open space approval. The commission is comprised of five standing members and two advisory members.

Open Space Acquisition Mechanisms

The Open Space portion of this Element includes policies and programs regarding open space acquisition. Some methods for acquiring permanent open space are described below.

Pooled Development Rights Program



Public Facilities and Services Element

Integrated Waste Management

Solid waste management, including waste disposal and curbside recycling is handled by a local franchised hauler under contract with the City. Solid wastes are currently taken to Potrero Hills Landfill and recyclables processed at an interim facility in Fairfield.

All solid waste management in Solano County is governed by the County Integrated Waste Management Plan. However, the Integrated Waste Management Act of 1989 (AB 939) resulted in many changes to solid waste management in California. Among other things, it mandated that each City and County in the State develop a Source Reduction and Recycling Element (SRRE) for inclusion in the County Integrated Waste Management Plan. The SRRE, which is not a General Plan element, promotes source reduction (including reuse), recycling and composting, and transformation (combustion) as means of managing solid waste. The City's SRRE complies with the Integrated Waste Management Act of 1990's requirement that each local jurisdiction in the State divert from disposal 25 percent of its waste stream by 1995 and 50 percent by 2000 or risk substantial fines.

Fire Suppression

Fire suppression services are provided by the City's Fire Department from five fire stations located throughout the city. A new fire station is planned on the east side of North Texas Street, south of Dickson Hill Road. In addition, the station on Magellan Road will be replaced with a station on Pennsylvania Avenue in the Civic Center. The Union Avenue station will be converted into an administrative facility. Two new fire stations are planned in Cordelia, which would replace the existing temporary station. A total of six stations will be needed to accommodate buildout of the General Plan in order to maintain fire response times of under five minutes. In order to provide a 5-minute response time to 80% of the dwelling units in its response area, the existing fire station at Huntington Drive/Walters Road is proposed to be relocated to the east in conjunction with the development of the Fairfield Train Station Specific Plan.

Law Enforcement

Police protection services are provided by the City's Police Department located in the Fairfield Civic Center. In order to maintain adequate police protection for the City's growing population, additional police staff, vehicles and equipment will have to be added to the force as needed. As a general rule, the ratio of sworn officers to population should be in the range of 1.13-1.20 per one thousand residents. Given the growth projections at General Plan buildout, the following is an approximation of required total staff and vehicles:

Urban Design Element

Policy UD 3.4

Encourage private preservation of buildings which have historic significance and/or architectural merit. (See Policy OS 10.1 and OS 10.9)

Objective UD 4

Ensure high standards of quality in development.

Policy UD 4.1

Adhere to the design guidelines of the Fairfield Urban Design Plan.

Program UD 4.1 A

Adopt, and regularly update the Fairfield Urban Design Plan.

Program UD 4.1 B

Regularly review and update the design guidelines and manuals that have been prepared for specific areas of the City (i.e., Cordelia Area Design Compatibility Guidelines, [Fairfield Train Station Specific Plan](#), Downtown Design Guidelines, Interim Design Manual for the Fairfield Regional Center Project, and the Fairfield Gateway Design Manual).

Policy UD 4.2

All aspects of development, including, but not limited to, grading, site planning, signage, fencing, landscaping, screening, lighting, color scheme, size, bulk, height, etc., must be integrated and relate to their surroundings in a complementary manner.

Policy UD 4.3

Require the use of water features, sculptures, or other elements to help define the entrance to large projects.

Policy UD 4.4

Encourage rehabilitation or upgrade of aging residential neighborhoods, commercial centers, and industrial areas.

Program UD 4.4 A

Adopt design plans and rehabilitation programs for the commercial corridors along Texas Street between Pennsylvania Avenue and the I-80 interchange and along North Texas Street between Union Avenue and Air Base Parkway. Include in the program for North Texas Street, a review of land uses in the vicinity of the Travis Blvd./North Texas intersection, including the former Ellsworth Court Mobile Home Park. (See Program ED 9.1 A)

Policy UD 4.5

Screen negative views through site planning, architectural, and landscape devices.

- Cordelia Area Design Compatibility Guidelines, March 1982. Contains guidelines intended to implement the design policies of the Cordelia Area Specific Plan on commercial and industrial development in the Cordelia area.
- Interim Design Manual for the Fairfield Regional Center Project, June 1977. Provides design guidelines for all development within the Fairfield Regional Center Redevelopment Area.
- Fairfield Gateway Design Manual, September 1981. Sets specific design and performance requirements for the Fairfield Gateway portion of the Fairfield Regional Center Redevelopment Area.
- Fairfield Train Station Specific Plan, April 2011. Establishes specific design guidelines for development to implement the land use and economic development goals and objectives of the Specific Plan.

The above documents have been incorporated into the Fairfield Urban Design Plan (February 1990), together with the appropriate policies from the City's General Plan. A revised Urban Design Plan is currently in production.

The Urban Design Plan sets design standards for public and private development reflecting Fairfield's desire to achieve high quality in all development. It coalesces public and private development towards common city design goals, defines what each development project must contribute toward this image, and encourages high standards of planning and development.

Thirteen districts are delineated based on a combination of visual, physical, and land use characteristics. Urban design goals and objectives are established for each of the districts. Further, the Plan's design guidelines address site plans, community plans, single and multiple family residential, hillside residential, shopping center, commercial, office, and industrial development, landscaping, parking, signs, and the integration of development with the surrounding areas.

These guidelines expound upon the general objectives and policies contained in the Urban Design Element and are complementary to it.



APPENDIX C: GLOSSARY



APPENDIX C GLOSSARY

July 26, 2011



Abutting: having a common boundary except that parcels having no common boundary other than a common corner shall not be considered abutting

Adjusted gross acreage (AGA): the number of acres remaining after subtracting non-development and/or utility acres; for the purposes of this document, acres for neighborhood parks and local streets are included in the adjusted gross acreage

Alley loaded: residential dwelling units with automobile access via alleys and primary entries and walkways facing the street.

Alley: a public or private access serving as secondary access to abutting property and used to serve as garage access and trash collection.

Architectural features: towers, gables, spires, chimneys, flagpoles and other architectural elements that are not habitable structures

Association of Bay Area Governments (ABAG): the regional planning agency for the counties, cities and towns of the San Francisco Bay region.

Attached buildings/housing types: two (2) or more residential units or structures that are physically connected with a wall, roof, deck, floor, bearing or support structures, trellises, architectural features or any other feature, fixture or device that exceeds thirty (30) inches in height above the finished grade.

Berm: a mounded landscape feature for screening and drainage purposes

Boulevard: the iconic roadway within FTSSP connecting the Town Center District to the Lake District

Building orientation: refers to the placement of building on lots, in respect to streets and the environment. For example, in general buildings

are perpendicular or radian on the street, regardless of the shape of the lot.

Building Type: A singular structure containing all similar units having a building massing that sets it apart from the others. Buildings consisting of all garages or garages with storage/office/carriage units above will not be considered as a separate building type.

California Environmental Quality Act (CEQA): California Public Resources Code § 21000 et seq., codifying the statewide policy of environmental protection and governing code for the preparation of the Project Environmental Impact Report (EIR)

Cannon Station LLC: the limited liability company representing the majority land ownership of property within the Project boundary.

Capitol Corridor system: the Amtrak passenger train line route running between the San Francisco Bay Area and Sacramento, roughly parallel to Interstate 80

Class I bike path: bicycle pathway intended for the exclusive use of bicycles and physically separated by distance or a vertical barrier from vehicular traffic

Class II bike path: bicycle pathway that shares the right-of-way with a roadway or walkway; either visually signaled with a bikeway pictograph and a continuous stripe on the pavement or separated by a curb or other low barrier.

Class III bike path: roadways for bicycle use signaled by signs only and may not include additional pavement width for cyclists

Classic Romantic Neighborhoods: neighborhoods that are characterized by relaxed yet connected curvilinear street system organized around a central open space feature such as a linear park and/or drainage feature.

Collector: intermediate roadways designed to handle traffic between arterial streets and local streets

Commercial: businesses operated or conducted on a frequent basis for the purpose of financial gain; non-residential development

Community Commercial: land use that provides a broad range of retail and services at a community wide and/or regional level

Community Facility District: an area established by the local government agency in which the real property owners within the district are assessed a special property tax on real estate, in addition to the normal property tax, as a means of obtaining community funding for said area.

Connector: low volume streets providing access off of collectors into residential neighborhoods

Court street: *see Alley*



Courtyard (building): a public or private open area generally surrounded by walls serving as an accessway to building entries or for recreation.

Courtyard (vehicular): a public or private access serving as the primary garage access around which residential units are configured; unit front entries may also be accessed from this area.

Density: a. Gross residential density: the number of dwelling units per gross residential acre; b. Adjusted gross density: the number of dwelling units per adjusted gross acre (*see definition of adjusted gross acre*)

Design Review Board (DRB):

Detached residential and/or building: two (2) or more residences, units or structures that are structurally independent and freestanding and not connected by walls, roofs, floors, bearing or support structures, trellises, architectural features or any other feature, fixture or device that exceeds thirty (30) inches in height above the finished grade.

Developer:

Development Agreements:

Development standards: the textual and numeric requirements governing the site planning and building of structures within the applicable land use/zoning designation

Development: any type of construction, including necessary grading, together with the land upon which the buildings or structures are constructed

Director: the Director of Community Development for the City of Fairfield

District: one or more neighborhoods generally emphasizing a special predominant use emphasizing a special predominant use and is typically organized around district core that functions as the central gathering place and activities center. A district can be physically defined by natural features such as a lake, park, drainage corridor or manmade features such as open space elements, the train station, rail line or local/regional transportation corridors.

DRB: *see Design Review Board (DRB)*

Driveway: a vehicular passageway for the exclusive use of the occupants of a property and their guests

Driveway: paved area a minimum of 10 feet wide by 18 feet deep leading to a garage; may be used as a parking space

Dwelling unit: one or more rooms in a structure, including a kitchen of any size, designed for occupancy by one (1) person or family for living and sleeping purposes



Dwelling units per acre (du/ac.): the number of residential units of a parcel divided by the parcel acreage

Dwelling, high density (HR): a maximum of thirty (30) dwelling units on an acre

Dwelling, low density (LR): a maximum of ten (10) dwelling units on an acre

Dwelling, medium density (MR): a maximum of twenty (20) dwelling units on an acre

Dwelling, standard single-family detached: a residential unit with street-front orientation detached from other dwelling units and situated on the lot of record

Easement: a recorded right or interest in the land of another which entitles the holder thereof to some use, privilege or benefit in, on, over or under said land

Fairfield Municipal Code: refers to the standards and regulations of Section 25 of the Fairfield Municipal Code adopted January 2010.

Fairfield Train Station Specific Plan (FTSSP): also referred to as FTSSP, the community or Project; is a high-quality TOD development located in the north east region of the City of Fairfield including 2,972 acres of land between Fairfield and Vacaville.

Farmers market: commercial sales of agricultural produce and other related product in an outdoor setting

FFD: Fairfield Fire Department

Flats: residential unit generally in an apartment configuration; the individual unit is of a single-level design within the building

Garage face: the plane of the garage door, used as a reference for the "Garage to Garage" setback requirements.

Garage: a building, or a portion of a building, used primarily for the parking of vehicles

General Plan: the governing planning document for the City of Fairfield; adopted City of Fairfield General Plan

GFA: gross floor area

Great Park: a fifty (50) acre sports field complex owned and operated by the City of Fairfield.

Greenbelt: an open space area generally linear in nature of largely undeveloped, wild, landscaped, or park land surrounding or passing through neighborhood or commercial areas

Greenfield communities: an area of land such as the majority of FTSSP that have not been previously developed



Habitat Conservation: land reserved within the Project boundary to conserve and protect the current state of the natural land condition.

High Density Residential (HR): Land use designation that provides for attached housing opportunities at a density from 20.0 du/ac. up to 30.0 du/ac. with a target density of 28 du/ac. The FTSSP land use designation includes and modifies City of Fairfield zoning districts RH and RVH.

High visibility edge: a parcel edge that is visible from other parcels within the Project and deemed high-visibility by this document

Horizontal mixed-use: a development configuration combining two (2) or more uses in a single building or development, generally combining residential or office uses next to commercial to located a greater number of residents and services within proximity to each other

Housing types: a range of residential unit configurations that are limited only by the standards of this document, purposely a limited number of housing types are defined by the Project to encourage the incorporation of newly developed/designed housing types

HR: see *High Density Residential (HR)*

Institutional: land use designation that provides a variety of public and private services and support to residents, employers and visitors such as community centers, recreational facilities, schools, churches, hospitals, fire and police stations, etc.

Lake District: the Specific Plan development area defined by Planning Area 2 (PA4) designed to provide a community activity node centered around the drainage/water feature lake with residential and neighborhood commercial uses

Land owners: all persons owning real property included in the FTSSP area boundary and participating in the formulation, passage and development of this document

Landscaping: the planting of trees, shrubs, grass or groundcover to improve the appearance of an area

Light Industrial: land use designation consistent with the City of Fairfield municipal code that includes small to large scale light manufacturing and may include office space

Linear Park: contiguous open space network within FTSSP totaling 54 acres as delineated in Figure 9-4

Livable area: for the purposes of measuring setbacks, enclosed living space within the walls of the residence; non-porch space

Live/work: a building configuration of detached or attached buildings that include a commercial land use on the ground floor with a residential unit above with direct internal access between the uses;





designed for a resident to live above their own work space; not all units are required to have a work space in a live/work configuration

Lot: any area identified as a lot or parcel on a recorded final map, parcel map or record of survey recorded pursuant to an approved division of land, certificate of compliance or lot line adjustment. A lot is not necessarily a building site.

Low Density Residential (LR): Land use designation that provides for detached and attached housing opportunities at a density from 0.0 du/ac. up to 10.0 du/ac. with a target density of 7 du/ac. The FTSSP land use designation includes and modifies City of Fairfield zoning districts RVL, RL and RLM.

LR: see *Low Density Residential (LR)*

Main Street: an iconic roadway designed for use within the Town Center District with on-street parallel or angled parking permitted

Medium Density Residential (MR): Land use designation that provides for detached and attached housing opportunities at a density from 10.0 du/ac. up to 20.0 du/ac. with a target density of 14 du/ac. The FTSSP land use designation includes and modifies City of Fairfield zoning districts RM and RH.

Memorandum of Understanding (MOU):

Mixed-use: a development configuration combining two (2) or more land uses into a single parcel or development area in a compatible supportive design

MR: see *Medium Density Residential (MR)*

Multi-modal network: a network of pathways, streets and accessway focused on making a community well connected by encouraging residents to walk, bike or utilize public transit to their daily activities.

Negative Declarations: a Negative Declaration (ND) is prepared for a project when it is determined from an Initial Study that the project will not have a significant effect. Or, a Mitigated Negative Declaration (MND) is prepared if it is determined the project will have potentially significant effects, but revisions in the project or mitigation measures will avoid or reduce the effects to a point where no significant effects would occur (CEQA Guidelines, Section 15070).

Neighborhood Commercial (CN): land use designation for commercial uses and vertical mixed-use that offer small scale services and retail that meet the daily needs of the residents

Neighborhood: groups of residential units that include a range of housing types and price levels in a development pattern that links them by a general design or aesthetic continuity

North Bay Regional Water Treatment Plant: water treatment facility located within the Project boundary jointly operated by the cities of Fairfield and Vacaville.

Open space: any parcel or area of land or water that is essentially unimproved and devoted to a passive recreation use providing relief in the urban/residential structure of the environment

Parcel: a distinct, continuous portion of land established in compliance with the building site requirements of the Zoning Code and intended for development by this document.

Parking lot: an area usually divided into individual spaces intended for parking of vehicles

Parkway: *see Greenbelt*

Paseo: a walkway or promenade

Pathways, pedestrian and bicycle: a trail or way designed for and used by pedestrians, cyclist and non-motorized bicycles

Planning Area: a planning area is a geographic area within the Specific Plan reflecting a specific set of land uses and characteristics to best serve the needs of the Project.

Pocket park: passive parks intended for flexible play areas for the local neighborhood, generally programmed for half (0.5) to three (3) acres

Project Description: the scope of the FTSSP project as defined by the associated EIR

Project: *see Fairfield Train Station Specific Plan*

Promenade: walkway or pathway within the Lake District for pedestrian and bicycle circulation

Property owners: *see Land Owners*

Public agency: for the purposes of this Specific Plan, City of Fairfield is the public agency

Residential project: development of any residential building or community features, proposed or real, that area outside the proposed scope of development for FTSSP that impact the findings and/or declarations of the project EIR.

Retail: the selling of goods, wares or merchandise directly to the ultimate consumer

Roof massing: the composition and balance of roof forms

Sensitive edges: development parcels that have side-on and rear elevations oriented toward parcel boundaries that are visible from other parcels across open space areas or corridors that may require architectural treatment such that views from public places, trails and linear parks are pleasant and add value to the Project



Specific Plan: for the purposes of FTSSP and all related documentation, this shall refer to the Fairfield Train Station document.

Streetscape: an environment composed of streets; usually oriented to create unique experiences and public space opportunities allowing the public and residents to visit, explore, shop and play throughout the district.

Structures: anything constructed or erected that requires location on or in the ground including swimming pools but excluding driveways, uncovered patios or parking spaces

Sub-Core Service Area: a node within the industrial area providing services such as coffee, food, laundry to local workers.

Sustainability: meeting the needs of the present without compromising the ability of future generations to meet their own needs

Thematic Street(s): a street textually identified by this Specific Plan requiring an additional level of design treatment to support the theme and consistency of the FTSSP community

TOD: see *Transit Oriented Development (TOD)*

Town Center District: the Specific Plan development area defined by Planning Area 2 (PA2) designed to provide residential density and commercial land uses to support high-volume use of public transit at the Train Station and support the TOD nature of FTSSP

Traditional neighborhood planning: a comprehensive planning system that includes a variety of housing types and land uses in a defined area served by a network of paths, streets and lanes suitable for pedestrians as well as vehicles; public and private spaces have equal importance, creating a balanced community that serves a wide range of home and business owners enhancing community identity and value

Train Station: the land parcel located at the south east corner of Peabody Road and Vanden Road slated for development of a transit stop as part of the Capitol Corridor system.

Transit Oriented Development (TOD): a mixed-use area designed to maximize access to public transportation by incorporating residential and commercial development features to encourage transit ridership.

Travis Air Force Base (Travis AFB): United States Air Force air base under the operational control of the Air Mobility Command (AMC), located east of the central business district of Fairfield, in Solano County.

TUSD: Travis Unified School District

Use: the purpose for which land buildings are occupied, arranged, designed or intended or for which either land or buildings are or may be occupied or maintained



Vertical mixed-use: a development configuration combining two (2) or more uses in a single building, generally combining residential or office uses above commercial, to reinforce the form of traditional urban commercial districts.

View corridor: maximize view premium (lake, mountain, green belt, park, open space) that may be used to guide the orientation of the development program

Wall offsets: building wall planes at different setbacks, separated by at least twelve (12) inches, to create variation along the streetscape

Zero lot line: the construction of a structure on any of the boundary lines of a lot. A structure is usually built on the front line or side line

Zoning Code: see *Fairfield Municipal Code*





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